



*Imagine Brea*

THE CITY OF  
BREA

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GENERAL  
PLAN



BREA  
GENERAL PLAN



THE CITY OF  
BREA

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# GENERAL PLAN

Adopted August, 19, 2003

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# Acknowledgements

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# TABLE OF CONTENTS



<b>Chapter 1: Introduction</b> .....	1-1
What Is the General Plan?.....	1-1
How We Imagine Brea .....	1-5
Organization of the General Plan.....	1-11
Using the General Plan .....	1-14
<b>Chapter 2: Community Development</b> .....	2-1
Vision .....	2-1
Land Use .....	2-3
The Land Use Plan.....	2-7
Circulation.....	2-39
Infrastructure .....	2-65
Urban Design .....	2-78
Economic Development .....	2-118
Growth Management.....	2-128
<b>Chapter 3: Housing</b> .....	3-1
Introduction.....	3-1
Housing Needs Assessment.....	3-8
Summary of Housing Needs .....	3-39
Housing Constraints.....	3-42
Housing Resources .....	3-59
Housing Plan .....	3-71
<b>Chapter 4: Community Resources</b> .....	4-1
Vision .....	4-1
Parks and Open Space.....	4-3
Trails .....	4-17
Wildlife Habitat .....	4-27
Scenic Resources .....	4-36
Water Conservation and Quality.....	4-43
Air Quality.....	4-47
Historic Resources .....	4-50

**Chapter 5: Community Services** ..... 5-1

Vision ..... 5-1

Human Services..... 5-2

Recreation Programs..... 5-8

Education Services..... 5-13

Library Services..... 5-16

Cultural Arts..... 5-18

**Chapter 6: Public Safety**..... 6-1

Vision ..... 6-1

Emergency Services and Safety ..... 6-2

Hazards Management..... 6-12

Wildland Fires ..... 6-17

Flood Concerns ..... 6-23

Geologic and Seismic Considerations..... 6-28

Noise ..... 6-36

**Glossary**.....G-1

**Appendices**

- A: Implementation Guide
- B: Urban Design Principles

# List of Tables

<b>Table</b>	<b>Page</b>
I-1	General Plan Chapters and State Mandated Elements . 1-13
CD-1	Land Use Distribution ..... 2-4
CD-2	Allowed Density Based on Average Slope ..... 2-21
CD-3	Land Use Buildout..... 2-37
CD-4	Level of Service Definition ..... 2-45
CD-5	Bicycle Network Classification ..... 2-56
CD-6	Total Water Demand by Land Use..... 2-67
CD-7	Ultimate Wastewater Flows by Drainage Region ..... 2-72
CD-8	Share of Total Businesses and Employment (2001) .... 2-120
CD-9	Employment Projection 1990 to 2020 ..... 2-121
HE-1	Population Growth Trends – 1980s and 1990s ..... 3-9
HE-2	Age Distribution – 1980 and 1990..... 3-11
HE-3	Race and Ethnicity – 1990 ..... 3-12
HE-4	Occupation of Residents – 1990..... 3-13
HE-5	Jobs-Housing Ratio – 1994..... 3-14
HE-6	Average Household Size – 1999 ..... 3-16
HE-7	Income Distribution – 1990..... 3-18
HE-8	Households by Income Groups -1990..... 3-18
HE-9	Housing Overpayment Profile..... 3-19
HE-10	Special Needs Groups – 1990..... 3-21
HE-11	Disability Status of Non-Institutional Persons..... 3-23
HE-12	Inventory of Homeless Services and Facilities – North Orange County ..... 3-25
HE-13	Housing Growth: 1980s and 1990s ..... 3-27
HE-14	Comparative Housing Unit Mix – 1990 and 1999..... 3-28
HE-15	Neighborhood Enhancement Areas..... 3-32
HE-16	Prices of Single-Family Homes – June 1998 to May 1999..... 3-34
HE-17	Prices of Multi-Family Homes/Condominiums – June 1998 to May 1999..... 3-34
HE-18	Rental Rates – June 2000..... 3-35
HE-19	Affordable Homeownership Costs by Income Category Orange County – 1999 ..... 3-36
HE-20	Affordable Rents by Income Category Orange County – 1999..... 3-36
HE-21	City-Assisted Multi-family Rental Housing Projects ..... 3-38
HE-22	Summary of Existing and Projected Housing Needs..... 3-41
HE-23	Disposition of Home Purchase Loans..... 3-44
HE-24	Disposition of Home Improvement Loans..... 3-45
HE-25	Residential Land Use Designations..... 3-46

## List of Tables (continued)

<b>Table</b>	<b>Page</b>
HE-26 Residential Development Standards.....	3-48
HE-27 Parking Requirements.....	3-49
HE-28 Housing Types by Residential Zone Category.....	3-51
HE-29 Development Deposits .....	3-53
HE-30 Traffic Impact Fee Schedule by Residential Land Uses .....	3-54
HE-31 Residential Development Potential .....	3-61
HE-32 1998-2005 Regional Housing Growth Needs Compared To Units Constructed Between 1998-1999 .....	3-63
HE-33 Developments Providing Affordable Units Under the Affordable Housing Ordinance.....	3-74
HE-34 Affordable Housing Projects Supported Through Land Disposition (1989-1999).....	3-76
HE-35 Affordable Housing Projects Supported Through Direct Financial Assistance or Development Incentives (1989-1999).....	3-77
HE-36 1989-1994 RHNA, 1989 Housing Element Objectives, and Units Constructed during 1989-1997 .....	3-83
HE-37 Housing Implementation Program Summary .....	3-97
CR-1 Park and Recreation Facilities .....	4-5
CR-2 City of Brea Historic Resources Register .....	4-55
PS-1 Magnitude and Intensity Scales of Earthquakes .....	6-30
PS-2 Magnitude and Intensity of Maximum Credible Earthquake for Faults Potentially Impacting Brea .....	6-29
PS-3 State Criteria for Minimizing Adverse Noise Effects on Humans .....	6-39

# List of Figures

<b>Figure</b>	<b>Page</b>
I-1 Planning Area .....	1-3
I-2 Brea’s Planning Area and Regional Location.....	1-5
I-3 Organization of the General Plan.....	1-12
CD-1 Examples of Floor Area Ratios (FAR).....	2-8
CD-2 Land Use Policy Map.....	2-9
CD-3 Example of Unit Transfer .....	2-22
CD-4 Example of a Recalculation of Average Slope .....	2-23
CD-5 Focus Areas .....	2-27
CD-6 Typical Arterial Roadway Cross Sections .....	2-43
CD-7 Typical Collector Roadway Cross Sections.....	2-44
CD-8 Master Plan of Roadways .....	2-47
CD-9 Typical Bikeway Cross Sections.....	2-55
CD-10 Bike Plan .....	2-57
CD-11 Water System Improvements .....	2-69
CD-12 Sewer System Improvements .....	2-73
CD-13 Urban Design District Map .....	2-84
CD-14 Urban Design Plan Map.....	2-87
HE-1 City of Brea .....	3-2
HE-2 Population Growth – 1980s and 199s.....	3-9
HE-3 Age Distribution – 1980 and 1990.....	3-11
HE-4 Household Composition – 1990 .....	3-15
HE-5 Median Household Incomes – 1989 .....	3-17
HE-6 Household Composition by Income Groups – 1989....	3-19
HE-7 Overpaying Households by Tenure and Income – 1990 .....	3-20
HE-8 Housing Growth – 1980s and 1990s.....	3-27
HE-9 Age Distribution of the Housing Stock – 1999.....	3-29
HE-10 Neighborhood Enhancement Areas.....	3-31
HE-11 Median Sales Prices – 1999 .....	3-33
HE-12 Future Residential Sites.....	3-60
CR-1 Parks, Recreation, and Open Space Plan .....	4-7
CR-2 Trails Plan.....	4-21
CR-3 Regional Wildlife Corridors.....	4-30
CR-4 Scenic Resources .....	4-37
CR-5 Water Consumption and Projected Demand in Brea ..	4-44
CR-6 Historic Resources Map .....	4-53

## List of Figures (continued)

<b>Figure</b>		<b>Page</b>
PS-1	Oil Fields .....	6-15
PS-2	High Fire Risk Areas.....	6-19
PS-3	Flooding Hazards.....	6-25
PS-4	Geologic and Seismic Hazards .....	6-33
PS-5	Noise Contours (2001).....	6-37
PS-6	Noise/Land Use Compatibility.....	6-40
PS-7	Future Noise Contours .....	6-43

Chapter 1  
**INTRODUCTION**

THE CITY OF  
BREA  
GENERAL PLAN



# Chapter 1

## INTRODUCTION



**I**magine Brea. Imagine Brea as a town where people live, work, shop, and play within one community. Where people find homes to match diverse lifestyles, access needs, and income levels. Where businesses provide jobs for local residents and create a strong economic foundation for the community. Where parks and recreational facilities offer opportunities for a diverse population to exercise and interact. Where schools and teachers educate the youth and inspire the mind. Where the natural environment is protected for its inherent visual and wildlife resource values. This is how the community imagines the Brea of the future, and this General Plan will take Brea there.

### WHAT IS THE GENERAL PLAN?

The Brea General Plan identifies the community's vision for its collective future and establishes the fundamental framework to guide future decision-making about development, resource management, public safety, public services, and general community well-being. During creation of the General Plan, the community worked together to craft this vision and define it through text and illustrations. This General Plan reflects the planning desires and values of Brea residents, the business community, community educators, and elected and appointed officials.

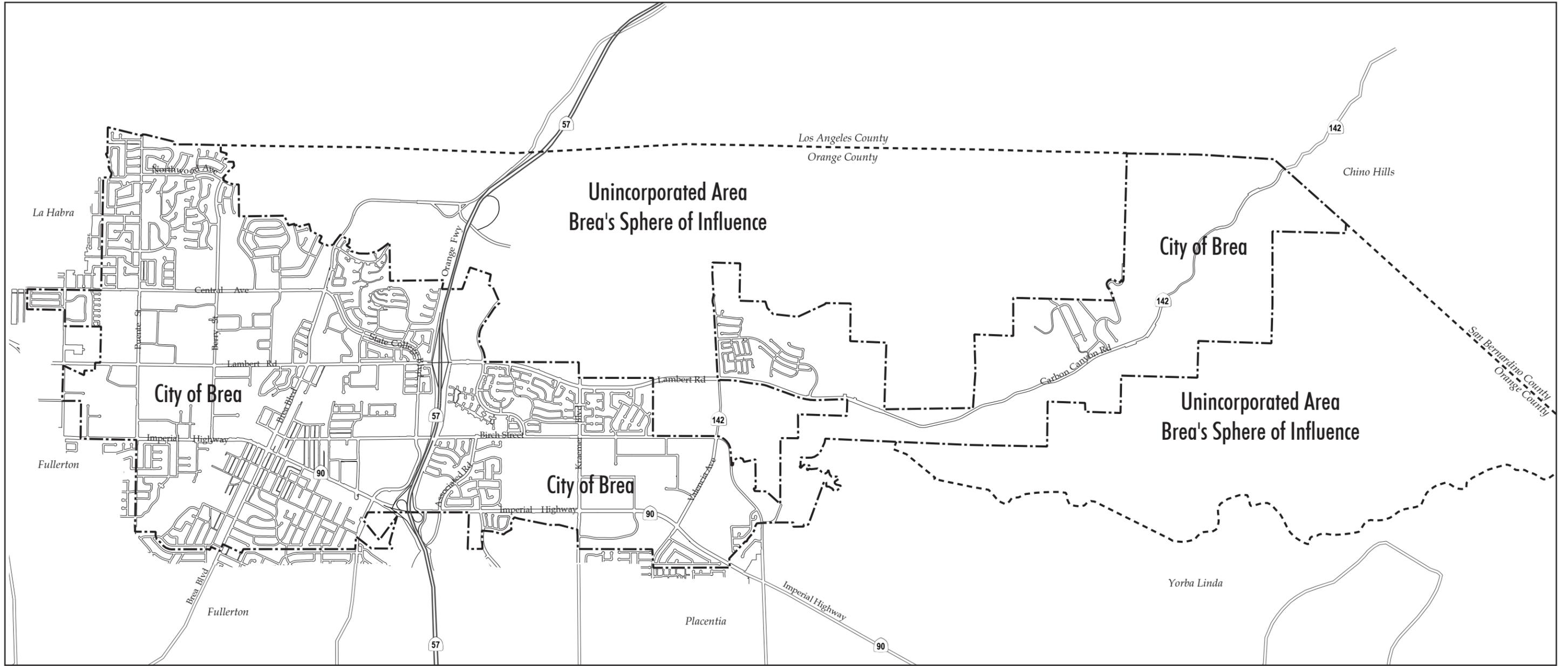
The Brea General Plan describes how Breans will work to retain the natural and aesthetic qualities that make their City unique, but at the same time, respond to the dynamics of regional growth and meeting changing community needs. The General Plan serves as a policy guide, balancing these interrelated factors to Brea's community vision.

## *Imagine Brea*

Importantly, this General Plan addresses not just lands within the City's corporate limits, but also unincorporated Orange County properties surrounding Brea, within its so-called sphere of influence. In this General Plan, the combined City area and sphere of influence are termed the "planning area," illustrated in Figure I-1. While properties within the sphere of influence are under the jurisdiction of Orange County agencies, these properties bear a critical relationship to Brea's planning activities and, from a visual standpoint, form a significant backdrop to the community. One day, these properties may become part of the City proper, and planning for service extensions, integrated infrastructure, and high design quality is timely and prudent.



*In the foreground is Carbon Canyon Regional Park, partially located within the corporate limits of Brea. In the background are the Chino Hills in unincorporated Orange County; both lie within Brea's planning area.*



**Brea's Planning Area**

- City Boundary
- - - Sphere of Influence

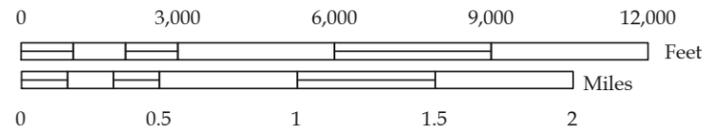
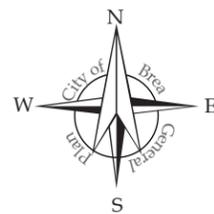


Figure I-1  
Planning Area



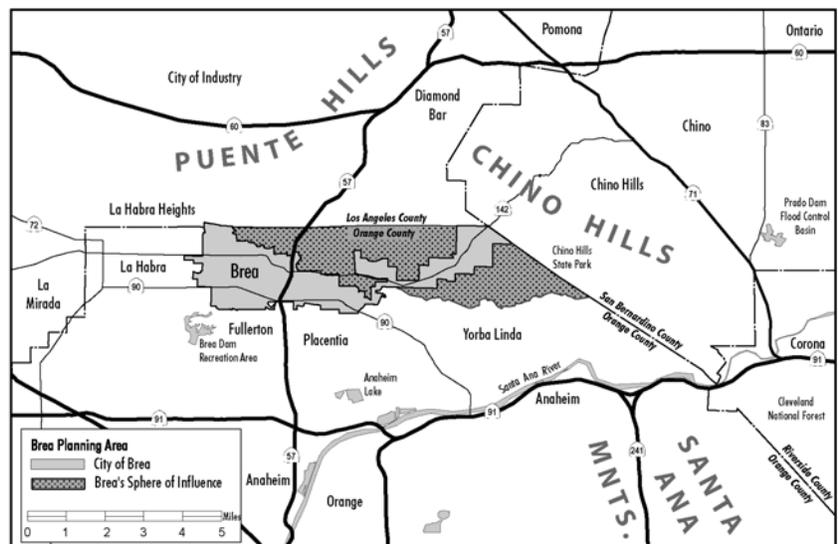
# HOW WE IMAGINE BREA

The Brea of the future builds upon the Brea of the past. Throughout its history, the City has embraced changes that have expanded economic development opportunities, created new housing, improved the services and facilities available to residents, and preserved resources integral to the health of the community. As Brea moves through the first two decades of the twenty-first century, the community will continue to experience change, guided by the goals and policies contained in this General Plan.

## Brea’s Beginnings – A Planning Context

The Brea of today lies at the base of the Puente and Chino Hills, geographic features that provide Breans with scenic views of natural beauty. The hills contrast with and complement the urban forms of the City. Whereas the SR-57 freeway and arterial roadways link Brea to adjacent communities in Orange, Los Angeles, San Bernardino, and Riverside counties, the hills tie Brea to its past. Brea occupies a unique geographic location that offers residents and businesses easy access to both the benefits of urban living and the quiet solitude of open spaces (Figure I-2).

**Figure I-2. Brea’s Planning Area and Regional Location**



## *Imagine Brea*

The properties comprising current day Brea were once part of the large land holdings of the San Gabriel Mission established in 1771 by the Franciscan Padres. Vast herds of Mexican cattle pastured on all the land in and surrounding Brea during the Mission period and the subsequent Rancho era. In 1863, thousands of acres of Rancho lands, including Brea, were acquired by Abel Stearns, a Los Angeles businessman who later leased much of his land to sheep ranchers.

Sheep ranching and oil production represented the two predominant business activities during the latter half of the 1800s. Beneath the hills in Brea lie what was once one of the world's richest deposits of oil. The Puente Hills and Brea Canyon supported substantial petroleum production. By 1895, the Puente Hills Oil Company had approximately 30 oil wells in the Puente Hills producing 700 barrels of oil a day. Union Oil Company was also lured by the riches of Brea and began purchasing land in Brea for oil production and leasing it out to other oil companies. Wooden oil derricks and oil production machinery covered the hills and canyons. Evidence of oil production and remaining steel derricks can be seen today in the canyons. In Spanish, *brea* means tar. Thus, the City's name reflects its history as an oil community.



*Oil derricks can still be seen along the hillsides in Brea Canyon.*

The first real village in Brea was called Olinda and was originally sited where Carbon Canyon Regional Park lies today. Olinda, literally surrounded by hundreds of oil wells, was the workplace for many of the oil field workers. The village contained a church, barbershop, livery stable, boarding house, dance hall, pump house, Santa Fe Freight Depot, a school, and an oil storage tank.

The official founding date for the town of Brea is January 19, 1911. The City incorporated in 1917 with a population of 732. Many of the buildings of the original town were situated along Pomona Avenue (current day Brea Boulevard). As a booming oil town, the City's population began to grow rapidly. Development of new housing, businesses, and civic buildings began to shape the Brea of today. In 1918, the Pacific Electric Railway connected Brea to La Habra, Whittier, and other Los Angeles communities.

With the oil boom, land in and around the City began converting from sheep ranches to orange groves. The Bastanchury Ranch Company in La Habra, Fullerton, and Brea was said to have the largest citrus groves in the world, which included over 5,350 acres of orange groves during the 1920s. Union Oil Company also realized the potential of turning over underutilized properties to citrus production.

In Brea and throughout Southern California, the post-World War II years were a period of substantial growth. Citrus groves were plowed under and the land subdivided for greater profits. Brea also experienced an influx of manufacturing and industrial businesses. In 1972, completion of the 57 Freeway made agricultural lands and oil lands surrounding Brea even more accessible. Five years later, the Brea Mall opened, providing economic growth for the City while supplying retail opportunities for Breans and the region.

During the latter decades of the twentieth century, as the flatter lands below the base of the hills built out, development started to occur along the lower hillslopes. Also, properties once in active oil production transitioned to urban development as the economics of the energy industry changed. The Olinda Ranch development at Lambert Road and Valencia Avenue transformed 277 acres of oil fields into a residential community with substantial open space features.

As the community moves through the first twenty years of the twenty-first century, continuing regional growth pressures will again transform Brea. The question this General Plan answers is, How do we imagine Brea in 2020?

## Imagining Brea - Year 2020

How do we imagine the Brea of tomorrow? As part of the General Plan program, the City undertook a substantial public outreach program to understand current community values and

to establish a new General Plan policy foundation based on those values. The outreach effort arose from Brea's long-standing practice of involving its citizenry in important policy discussion, as well as in reaction to public discussion in 2000 regarding the future of Brea's hillsides, a discussion that culminated in a much-debated ballot measure. The outreach program was multi-faceted and included the components described below.

- As part of the General Plan update program, a **General Plan Advisory Committee** (GPAC) was formed to serve as an advisory committee to the City Council and Planning Commission. The GPAC members represented various City commissions and committees, the school board, community groups, and community at large. The GPAC conducted eight meetings over a one-year period, during which time the group formulated a long-range vision for Brea and assisted in translating this vision into General Plan goals and policies, as well as suggestions for land use and development capacity within the hillsides and unincorporated areas. The public was invited to share their comment and concerns with the members of the GPAC during these meetings. Two special meetings were conducted to which owners of large vacant hillside properties were specifically invited to engage in dialogue about hillside development policy.



*At the Imagine Brea workshop, residents learned about different urban design and architectural concepts that may be used in Brea.*

- Full-scale public participation began at the **Imagine Brea Workshop** involving the GPAC, City staff, City policy makers, consultants, and the public. This interactive workshop – which was presented in the format of “virtual Brea”, with the meeting room laid out as a map of the City – allowed the public ask to questions about the General

Plan program, collectively create a vision, and discuss the level of detail and framework of the General Plan. Major planning issues were identified and discussed.

- The second **Community Workshop** was a mobile workshop, whereby participants, maps in hand, drove to key sites in the City, reviewed land use options, and indicated a preference among the options presented. The information from this workshop helped the GPAC craft a preferred land use policy map.
- At the City's annual **Wellness Festival**, attendees were invited to visit the General Plan booth and identify preferences for various types of development: suburban versus urban, clustered versus the more traditional lotting patterns in Brea. This mini-workshop helped refine different land use alternatives for consideration by the GPAC.
- Once foundation goals and policies had been drafted, the public participated in a third **Community Workshop** designed to gain affirmation of policy direction. The two key questions City staff asked the public to answer were, "Did we get it right?" and "What did we miss?"
- The City utilized its **website** to provide current information on the General Plan update program. Summaries of the public meetings and information on program progress were posted for review. The public was allowed to comment on various aspects of the program during its formation.
- A special General Plan **newsletter** summarizing the General Plan Chapters was prepared and distributed to residents, with a brief description of the proposed General Plan and its contents.
- **Brea Line**, the City's community newsletter, was used to keep the public continually informed about the General Plan program. One issue focused on the General Plan.
- An internal City staff advisory committee, the **General Plan Action Team**, met at key points in the work program to review issues, goals and policies, and land use/circulation alternatives.

- Prior to adoption of the General Plan, the Draft General Plan and associated Environmental Impact Report were circulated for public review and comment, and a final community workshop was held before **General Plan Public Hearings** were conducted before the Planning Commission and City Council.

Through this extensive outreach process, the City learned how residents and the business community imagine Brea.

## Imagine Brea

Brea will be a community that provides great places to live, work, learn, and play, places that respect the community's natural and cultural resources, provide open space and public spaces that appeal to all Breans, encourage economic vitality, and enhance the overall quality of life for all residents.

In support of this vision statement, the community defined four overarching goals that embody the community's values and will guide all decisions made pursuant to this General Plan.

## Overarching General Plan Goals

### **Goal 1**

Create an inclusive community that strives to meet the needs of residents of all ages, income levels, occupations, family types, and lifestyles.

### **Goal 2**

Plan for the sustainable stewardship of natural resources.

### **Goal 3**

Provide a range of mobility options that reduce dependence on the automobile.

### **Goal 4**

Maintain a sustainable economic base to provide a solid fiscal foundation and diverse employment opportunities, and to ensure the provision of quality community facilities and services.

# ORGANIZATION OF THE GENERAL PLAN

Under California law, every city must adopt a comprehensive, long-term General Plan to guide the physical development of the city's incorporated area, plus to plan for land outside of the municipal boundaries that bear a relationship to a city's planning activities. As described above, such planning is not just a State mandate; it simply makes good sense. State law further indicates that the General Plan is the primary document a jurisdiction must utilize to regulate land use. Consequently, the zoning ordinance, specific plans, and individual public and private development proposals must be consistent with General Plan goals, policies, and standards.

To ensure that every city and county prepares General Plans that are comprehensive and long-term in perspective, State statutes establish requirements for the minimum contents of a General Plan. (Interested readers are referred to Sections 65350 through 65590 of the Government Code.) By law, a General Plan must contain the following seven "elements" and must be internally consistent element to element:

- Land Use
- Circulation
- Housing
- Conservation
- Open Space
- Noise
- Safety

Jurisdictions may prepare and adopt any other General Plan element or address any other subject of particular relevance or interest to that community, with the understanding that these optional elements must be implemented with the same vigor – and are subject to the same legal scrutiny – as the mandatory elements.

The Brea General Plan has been constructed to reflect the issues and policies and issues unique to this City. Rather than "elements", the Plan is organized by themed chapters, each of which examines and addresses several interrelated issues. Separate sections within the chapters treat a specific issue. For example, the Community Development Chapter contains sections related to development: land use, circulation, economic

development, urban design, and infrastructure. Figure I-3 illustrates this organization.

**Figure I-3. Organization of the General Plan**

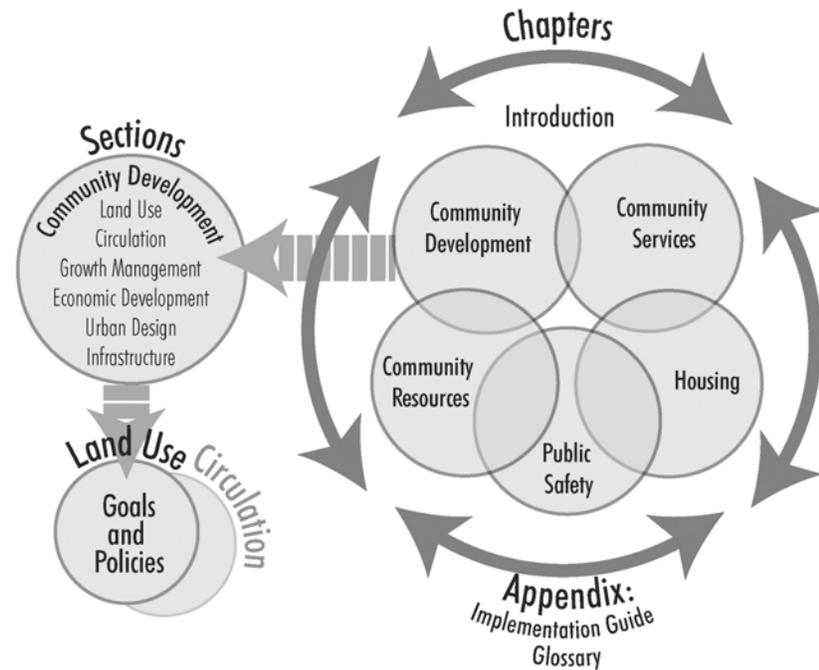


Table I-1 indicates how each of these chapters corresponds to the seven State-mandated elements.

The **Community Development** Chapter contains the following sections: Land Use, Circulation, Infrastructure, Urban Design, Economic Development, and Growth Management. This chapter focuses on the built environment aspect of Brea and pulls together issues and goals from the other chapters and lays out the framework for balancing development with broader community aims.

The **Housing** Chapter addresses issues, goals, and policies related to ensuring an adequate supply of housing opportunities for persons of all needs and income levels. Unlike the other elements, State law sets forth very specific regulations regarding the content and breadth of the Housing Element. Housing elements must be updated every five years.

The **Community Resources** Chapter examines both the natural and human-made environments, and establishes policies to protect those resources that distinguish and define Brea. Issues addressed are Parks and Open Space, Wildlife Habitat, Trails, Scenic

**Table I-1  
General Plan Chapters and State-Mandated Elements**

Brea General Plan Chapters/Sections	State Mandated Elements							Optional
	Land Use	Circulation	Housing	Open Space	Conservation	Safety	Noise	
<b>Chapter 2: Community Development</b>								
Land Use	•							
Circulation		•						
Infrastructure		•						
Urban Design								•
Economic Development								•
Growth Management <sup>(a)</sup>								•
<b>Chapter 3: Housing</b>								
Housing			•					
<b>Chapter 4: Community Resources</b>								
Parks and Open Space				•				
Wildlife Habitat					•			
Trails		•						
Scenic Resources					•			
Air Quality								•
Water Conservation and Quality					•			
Historic Resources								•
<b>Chapter 5: Community Services</b>								
Recreation Programs								•
Human Services								•
Cultural Arts								•
Education								•
Libraries								•
<b>Chapter 6: Public Safety</b>								
Emergency Services						•		
Hazards Management						•		
Wildland Fires						•		
Flood Concerns						•		
Geologic and Seismic Considerations						•		
Noise							•	

(a) While Growth Management is not a State-mandated element, it is mandated by Measure M for all jurisdictions in Orange County.

Resources, Air Quality, Water Conservation and Quality, and Historic Resources.

The **Community Services** Chapter emphasizes the human elements of the City: Recreation Programs, Human Services, Cultural Arts, Education, and Libraries.

The **Public Safety** Chapter is concerned with identifying hazards present in the community and ensuring that proper planning and emergency response services can mitigate the hazards. Sections in the chapter include: Emergency Services, Hazards Management, Wildland Fires, Flood Concerns, Geologic and Seismic Considerations, and Noise.

## USING THE GENERAL PLAN

Because the General Plan is a community document intended for use by all Breans, not just City staff and decision-makers, the Plan has been written and organized for ease of use. Tables, diagrams, and maps help readers understand planning concepts, and sidebar notes define terms and direct users to chapters addressing related topics or policies. At the end of each goals and policies section, readers can follow links to the Implementation Guide contained in Appendix A of the General Plan.

### Key Terms Used

As stated above, goals and policies represent the Plan's foundation. A **goal** is an overall statement of community desires and consists of a broad statement of purpose or direction. For each goal in this General Plan, associated and more definitive policy statements follow. A **policy** provides guidance to the City Council, Planning Commission, other City commissions and boards, and City staff in their review of development proposals and the actions taken.

The organization of the General Plan allows users to turn to the section that interests them and quickly obtain a perspective of City policies on the subject. However, General Plan users should realize that the policies throughout all chapters are interrelated and should be examined comprehensively. All of these policy components must be considered together when making planning decisions.

Some readers may find that the goals and policies do not fully articulate how the City will achieve its aims. Further articulation can be found in the **Implementation Guide** (Appendix A). The Implementation Guide identifies specific actions the City will undertake toward putting each goal and policy into action. Adopted separately from the General Plan, the Implementation Guide is intended to be reviewed and updated annually to allow decision-makers to adjust to current community priorities and funding resources.

## Related Plans and Programs

As referenced above, State law places the General Plan atop the hierarchy of land use planning regulations. Several local ordinances and other City plans must conform with General Plan policy direction and work to implement the Plan. Also, regional governmental agencies, such as the Southern California Association of Governments and the South Coast Air Quality Management District, have been established in recognition of the fact that planning issues extend beyond the boundaries of individual cities. Efforts to address regional planning issues such as air quality, transportation, affordable housing, and habitat conservation have resulted in the adoption of regional plans. The policies Brea adopts are affected by these plans. The paragraphs below describe ordinances, plans, and programs important to be consulted in association with the General Plan in development and planning decisions.

### Brea Zoning Ordinance

The Zoning Ordinance, the primary tool used to implement the General Plan, regulates development type and intensity citywide. Development regulations imposed include those setting limits on building height, requiring setbacks, and specifying the percentage of a site which must be landscaped. The Zoning Ordinance also outlines standards for residential planned unit development, affordable housing, adult entertainment businesses, hillside management, historic preservation, and off-site hazardous waste facilities, among many other land use issues.

### Brea Redevelopment Plans

Under California Law, cities can form redevelopment agencies and adopt redevelopment plans as mechanisms for facilitating community renewal. Since establishment of redevelopment authority in 1972, the Brea Economic Development Department has been instrumental in restoring a vibrant downtown commercial district, providing numerous affordable housing opportunities, and achieving economic development successes. Brea's City Council acts as the Redevelopment Agency.

## Specific Plans in Brea

A Specific Plan is a detailed plan for the development of a particular area. Falling under the broader umbrella of the General Plan, Specific Plans are intended to provide more finite specification of the types of uses to be permitted, development standards (setbacks, heights, landscape, architecture, etc), and circulation and infrastructure improvements. Specific Plans are often used to ensure that multiple property owners and developers adhere to a single common development plan, as well as to provide flexibility in development standards beyond those contained in the Zoning Ordinance. Brea has utilized Specific Plans as a tool to achieve the coordinated development of individual parcels within a broader context. Active Specific Plans include:

- Birch Hills
- Brea Industrial
- Brea Towne Plaza
- Carbon Canyon
- Olinda Heights (Ranch)
- Tomlinson Park

## California Environmental Quality Act (CEQA)

The California Environmental Quality Act (CEQA) was adopted by the State legislature in response to a public mandate for thorough environmental analysis of projects impacting the environment. The provisions of the law and environmental review procedure are described in the CEQA Law and Guidelines. CEQA is the instrument for ensuring that environmental impacts of local development projects are appropriately assessed and mitigated.

## SCAG Regional Comprehensive Plan and Guide

The Southern California Association of Governments (SCAG) undertakes regional planning for the six-county SCAG region of Los Angeles, Orange, Riverside, San Bernardino, Imperial and Ventura counties. SCAG's efforts focus on developing regional strategies to minimize traffic congestion, protect environmental quality, and provide adequate housing. The *Regional Comprehensive Plan and Guide* sets forth broad goals intended to be implemented by participating local and regional jurisdictions and the South Coast Air Quality Management District. SCAG has adopted companion documents to the *Regional Comprehensive Plan and Guide*, most notably the *Regional Transportation Plan*.

## Air Quality Management Plan

The federal Clean Air Act requires preparation of plans to improve air quality in any region designated as a nonattainment area.<sup>1</sup> The *Air Quality Management Plan*, or AQMP, prepared by the South

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<sup>1</sup> A nonattainment area is a geographic region identified by the Environmental Protection Agency and/or California Air Resources Board as not meeting State or federal standards for a given pollutant.

Coast Air Quality Management District, first adopted in 1994 and updated on a three-year cycle, contains policies and measures designed to achieve federal and State air quality standards within the South Coast Air Basin. The assumptions and programs in the AQMP draw directly from regional goals, objectives and assumptions in SCAG’s *Regional Comprehensive Plan and Guide*.

**Orange County  
General Plan**

The Orange County General Plan guides land use decision-making in unincorporated sections of the County. Brea’s sphere of influence is so governed. The County and incorporated communities within the County have agreed through a formal Memorandum of Understanding to consult with each other whenever development actions are proposed within a city’s Sphere of Influence.

**Orange County  
Growth  
Management Plan**

The Orange County Growth Management Plan, adopted by Orange County voters in 1990 via Measure M, works to ensure that Countywide transportation systems and public facilities are adequate to meet the current and projected needs of County residents and businesses. To receive associated Measure M benefits, cities are required to include a Growth Management Element in their General Plans.

**County of Orange  
Master Plan of  
Arterial Highways**

The County of Orange Master Plan of Arterial Highways (MPAH) forms part of the Orange County General Plan and designates the arterial system in the circulation element of the General Plan. Defined according to specific arterial functional classifications, the MPAH serves to define the intended future roadway system for the County. Cities within the County are expected to achieve consistency with the MPAH in individual General Plan circulation elements. To implement changes to the MPAH, approval from the Orange County Transportation Authority (OCTA) is required.<sup>2</sup>

**Local Agency  
Formation  
Commission  
Guidelines**

The provisions of the Cortese-Knox-Hertzberg Local Government Reorganization Act of 2000 set forth procedures for Local Agency Formation Commissions, or LAFCOs, throughout the State to review annexation applications. The Act was adopted to:

- Encourage orderly development
- Ensure that populations receive efficient and quality governmental services
- Guide development away from open space and prime agricultural lands, unless such action promotes planned, orderly, and efficient development

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<sup>2</sup> A city’s Sphere of Influence (SOI) is established by the Local Agency Formation Commission (LAFCO). It is anticipated that as properties in the SOI develop they would be annexed to and served by the adjacent city.

*Imagine Brea*

The Orange County LAFCO must adhere to adopted guidelines pursuant to the Act in its review of future City annexations within the Brea sphere of influence.

Chapter 2

# COMMUNITY DEVELOPMENT

THE CITY OF  
BRE A

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GENERAL PLAN



# Chapter 2

# COMMUNITY DEVELOPMENT

LAND USE • CIRCULATION • INFRASTRUCTURE • URBAN DESIGN •  
ECONOMIC DEVELOPMENT • GROWTH MANAGEMENT



## VISION

**I**magine Brea, where the urban form creates a living, active, and diverse environment that complements all lifestyles and enhances neighborhoods without comprising the valued resources that make Brea unique. Imagine a community where the synergetic planning of land use, circulation, infrastructure, urban design, and economic development works to manage growth.

Imagine residential, commercial, industrial, and public uses existing in harmony with the surrounding urban and natural environments. Imagine reducing the impact of motor vehicle traffic and creating additional opportunities for walking and biking. Imagine architecturally enriching environments that highlight Brea's identity, and that exhibit innovative and modern design features complementary to the City's historic fabric. Imagine visually attractive, high-amenity streetscapes, pedestrian paseos and paths, and urban outdoor rooms. Imagine a local economy that provides a sufficient base of revenue, employment options, and ample commercial, financial, office, and industrial opportunities. Imagine the management of development in a manner that ensures adequate public services and limits impact on the natural environment. Imagine new residents and businesses coming to Brea at a pace that allows infrastructure, public services, and school facilities to meet increased demand and maintain the high standard of education Brea enjoys.

## *Imagine Brea*

This Community Development Chapter addresses the building blocks of the community: its land use patterns and intensities, its infrastructure, the economic engine that provides high-quality jobs and supports public services, and the visual character of public places and private development. The goals and policies in this Chapter will guide all land use and development decisions in the City and within the sphere of influence, with the overarching aim to continue to improve the quality of life and reinforce the sense of place in Brea.



*The Ash Street Cottages, a 96-unit single-family development adjacent to Downtown Brea, were envisioned as a true downtown neighborhood. The project has met the goal of a truly urban living experience with resident-serving amenities such as a supermarket, restaurants, and entertainment all within walking distance. The units have retained their cottage charm with many owners adding rose and other gardens to their front yards. Ash Street Cottages has won several awards for innovative design and quality of development.*

# LAND USE

Imagine flying over Brea in the year 2020. Will Brea look different than the City did at the turn of the century? Will land use patterns have changed significantly? Will you note any new features? This Land Use section identifies how the Brea of the future will appear in terms of land use distribution, type, and intensities, and provides guidance to City staff and decision-makers for realizing the land use vision.



*The Evangelical Christian Credit Union headquarters, constructed as part of a Specific Plan, is one example of the quality of architecture found in buildings throughout Brea.*

The Land Use Policy Map on page 2-9 identifies the form of Brea in two dimensions. The map indicates that traditional land use patterns of residential neighborhoods and commercial and industrial districts will continue, reflecting stable, long-established areas. The map also shows that opportunities have been created for drawing from the successes of Downtown Brea to create vibrant mixed-use neighborhoods and districts with focal points and pedestrian connections. The land uses permitted reflect the community's goals for its form and character, and the desire to maintain a quality living environment for families, to preserve the City's existing, well-maintained neighborhoods, to strengthen Brea's distinctive image, to diversify and expand the local economy, and to preserve the hillside and natural areas that make Brea unique.

To gain a comprehensive overview of all policies affecting land use, readers are encouraged to look as well to the Community Resources Chapter and the sections addressing Parks and Open Space and Wildlife Habitat.

## Context

Brea lies at the base of the Puente and Chino Hills, which provide a scenic contrast to the relatively flat lands upon which much of the City has been developed. This contrast has influenced and continues to shape the types, intensities, and locations of land uses throughout the community. The City's historical uses have also shaped the urban form and street patterns seen in Brea today.

Brea's incorporated city limits, as of 2002, encompassed approximately 7,000 acres or 11 square miles. The community displays a balanced, complementary mix of residential neighborhoods of various housing types and densities, local and large-scale regional commercial businesses and centers, modern industrial uses in business park settings, schools, and public parks. Of the 7,000 acres (Table CD-1), 26 percent are developed with residences, 20 percent with businesses, 5 percent with schools and major public facilities like the Civic and Cultural Center, and 14 percent with parks and other open spaces, with Chino Hills State Park representing a major open space feature.

**Table CD-1  
Land Use Distribution**

<b>Land Use Distribution</b>	<b>Percent</b>
Residential (single-family, multi-family, and mobile homes)	26%
Commercial and Office	5%
Industrial	12%
Parks and Open Space	14%
Public Facilities (schools, Civic and Cultural Center, drainage channels, fire stations, and other public facilities)	4%
Carbon Canyon Hillsides Areas and Vacant Lands	21%
Agriculture	1%
Vacant	2%
Other (streets)	15%
<b>Total</b>	<b>100%</b>

*Source: City of Brea and Cotton/Bridges/Associates (2001)*

Residential neighborhoods — the pride of Brea — contain homes and representative architectural styles from almost every period dating to the early twentieth century. The historic core of the City surrounding City Hall Park still holds the traditional grid street pattern, while development from the 1960s through 1990s is generally suburban in nature, with curving streets and cul-de-sacs. During the 1990s, emerging development trends and the foresightedness of City leaders allowed more traditional urban forms – or “neo-traditional” development – to reappear as higher



*Brea's housing stock dates from the early 20<sup>th</sup> century.*

density small lot subdivisions, townhomes, and apartments near the City center, and as mixed use combined with commercial businesses. More compact development also occurred in the foothills, with the clustered forms arranged largely to avoid environmentally constrained properties. Given the built-out character of the flat lands, denser infill development and sensitively designed residential projects on the City fringe will characterize any new housing opportunities.

Commercial uses of early Brea centered near the Birch Street/Brea Boulevard intersection, and while the original buildings no longer remain, this area continues to represent a major commercial center and community gathering place. Both the revitalized Downtown Brea and Brea Mall, a major revenue generator for the City, represent shining examples of careful, daring, and creative planning by City leaders to facilitate development of varied benefits to the community. The large commercial centers along Imperial Highway, east and west of the SR-57 freeway, complement the other commercial centers, and the industrial businesses in west and central Brea provide jobs of varied skill levels.

The hills north of Brea lie in unincorporated Orange and Los Angeles counties, with the Orange County portion within the City's sphere of influence. As discussed in the Introduction chapter, the properties in Orange County bear a direct relationship to land use planning in the City. Visually, the hills frame and define Brea, and the homes, roads, and other infrastructure interface directly with these roughly 7,500 acres. Historically, oil production represented the predominant use in the hillside areas, with the County's Olinda Alpha landfill occupying many acres as well. During the 1980s and 1990s, the gradual depletion of oil resources in the hills set property owners to planning for alternative uses. Also, environmental interests and State park planners recognized that

actions were required to stop increased loss of open space and valuable habitat throughout the Puente and Chino Hills, and managed to secure many acres as permanent, protected open space use, such as Chino Hills State Park.

As Brea moves toward the year 2020, it faces the challenge of retaining those features that define the community while responding to growth pressures placed by forces beyond the City's control. California, and Southern California in particular, will continue to experience an influx of people, both from other states and foreign countries, as well as population growth from new births. What level of growth is anticipated?



*Townhomes create opportunities for home ownership in well-planned neighborhoods.*

The State Department of Finance estimated Brea's January 1, 2002 population at 36,857<sup>1</sup> people. Population projections developed by the Southern California Association of Governments (SCAG) indicate that regional growth will cause the City's population to experience a 27% increase by the year 2020, to 46,900 Breans. This represents an increase of nearly 10,000 residents over a 20-year period.

The question is not whether Brea can shut the door to growth but how Brea can open its door and welcome development that complements the existing community, enhances the City's character, provides the infrastructure needed to support new uses, and conforms to the City's vision expressed through the General Plan goals and policies.

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<sup>1</sup>State of California, Department of Finance, *City/County Population and Housing Estimates, 2002, Revised 2001, with 2000 Census Counts*. Sacramento, California, May 2002.

# THE LAND USE PLAN

The Land Use Policy Map displayed in Figure CD-2 illustrates the distribution of land use throughout the Brea Planning Area to be achieved over the long term. The map shows nineteen land use categories that reflect long-established land use patterns and intensities, and that respond to community desires for changes in specific areas. This Land Use Plan section describes the land use categories shown on the map and provides more directed policy discussion for those areas of Brea identified as needing focused attention and treatment.

## Density and Intensity

People can easily recognize *how* a property is used. The use may be residential or commercial or a park. To describe the level of use, that is, how much development exists on a property (and presumably, what the associated activity level is), land use planners have developed measures called *density* and *intensity*. The term *density* is used for residential uses and refers to the population and development capacity of residential land. Density is described in terms of dwelling units per acre of land (du/ac).

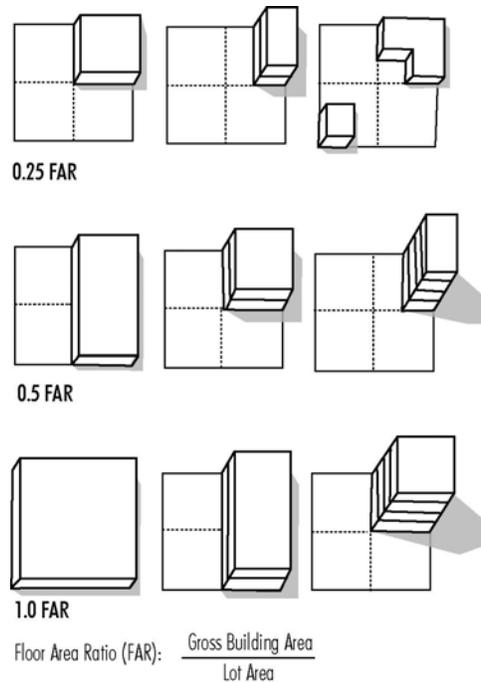


*Residential units at Olinda Heights (Ranch) have been built at a density of 12 dwelling units per acre (Medium Density Residential). Different phases of the development are built at lower and higher densities ranging from 6 to 15 dwelling units per acre.*

Development *intensity* refers to the extent of development on a parcel of land or lot (that is, the total building square footage, building height, the floor-area ratio, and/or the percent of lot coverage). While intensity generally describes nonresidential development levels, in a broader sense, intensity also can be used

to express overall levels of both residential and nonresidential development types. For land uses in Brea, floor-area ratio and building square footage are used as measures of nonresidential development intensity. The floor-area ratio, or FAR, describes the relationship between the total square footage of development on a lot and the area of that lot. Figure CD-1 illustrates different FAR calculations.

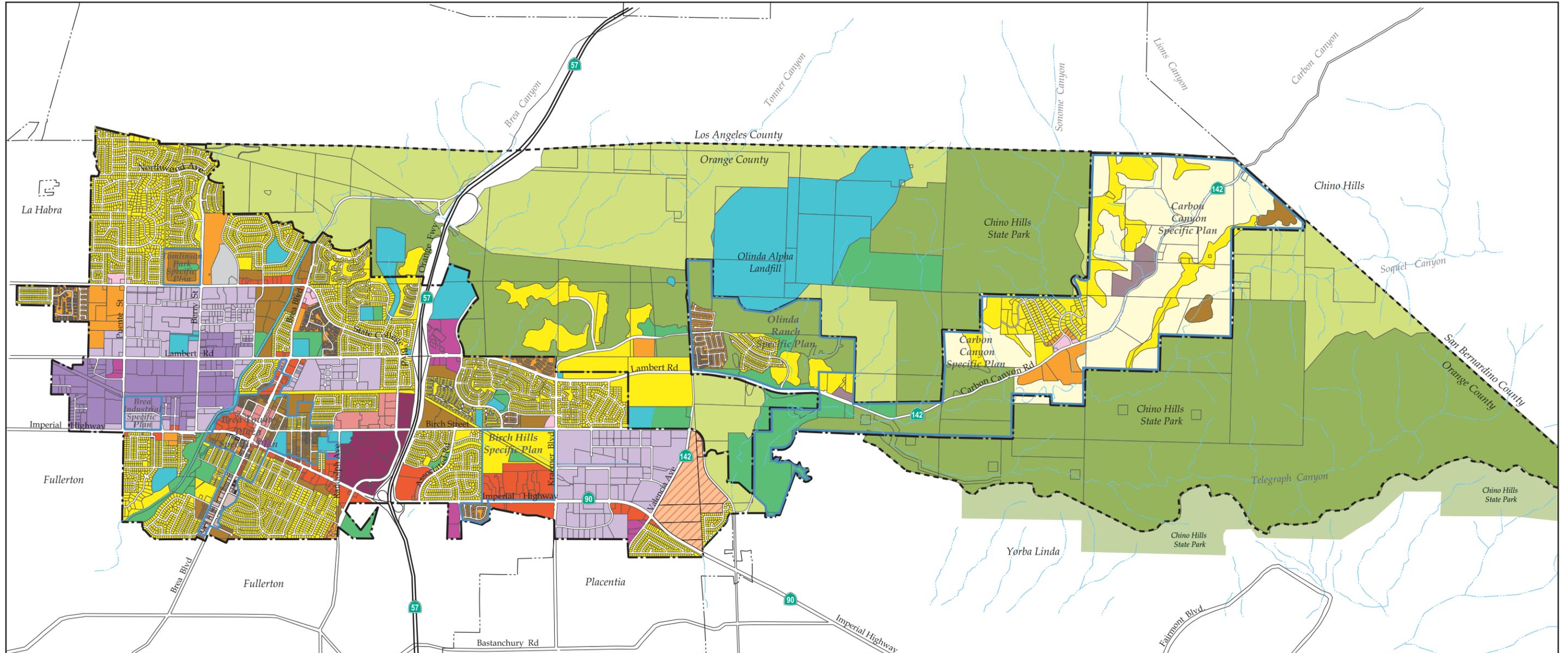
**Figure CD-1: Examples of Floor Area Ratios (FAR)**



The FAR controls the intensity of use on a lot. FAR is determined by dividing the gross floor area of all buildings on a lot by the land area of that lot. For example, a 20,000 square foot building on a 40,000 square foot lot yields a FAR of 0.50:1. A 0.50:1 FAR allows a single-story building, which covers half the lot, or a two-story building with reduced lot coverage.

## Land Use Designations

The Land Use Policy Map (Figure CD-2) graphically represents General Plan goals and policies. The following land use designations correspond to the categories shown on the Land Use Policy Map.



**Legend**

- City Boundary
- Sphere of Influence
- Specific Plan Boundary

**General Plan Land Use Designations**

*Residential Designations*

- Hillside Residential
- Very Low Density Residential
- Low Density Residential
- Medium Density Residential
- High Density Residential

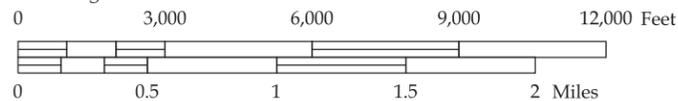
*Mixed Use Designations*

- Mixed Use I
- Mixed Use II
- Mixed Use III

*Non-Residential Designations*

- Regional Commercial
- General Commercial
- Neighborhood Commercial
- Recreational Commercial
- Office/Financial
- Light Industrial
- General Industrial

- Public Facilities
- Parks/Recreation/Open Space
- Natural Open Space
- Cemetery



*Notes:*

The Land Use Policy Map provides general guidance regarding the type and intensity/density of use permitted on a specific property. Users must consult the entire General Plan, the City's Zoning Ordinance, and the Hillside Development Ordinance to determine the extent to which a property may be developed and/or used.

To determine the residential density and development capacity allowed in the Hillside Residential designation, the Hillside Density Calculation Process (as described on page CD 2-19 to CD 2-24 of the Land Use Section in the Community Development Chapter) must be applied.



August 19, 2003

Figure CD-2  
Land Use Policy Map



## Residential Categories

Five residential land use designations are established to reflect the diverse residential character of Brea. The preservation and enhancement of existing single-family residential areas, particularly older established neighborhoods adjacent to City Hall Park, is a key goal. New development must be compatible with and complement existing residential neighborhoods. Special standards address residential development in the hillsides areas. Densities are stated as the number of housing units per net acre of developable land, excluding areas containing geological, slope, and related environmental constraints.

### **Hillside Residential**

Unit Density: Based on calculated property slope  
Population Density: approximately 3 persons/acre

Hillside Residential areas generally are located in hillside areas. Properties designated Hillside Residential are moderately to severely constrained by unique or significant features such as ridgelines, earthquake faults, steep and/or unstable slopes, creeks, sensitive habitat, sensitive wildlife species, mapped landslides, soil conditions, accessibility issues, and wildland fire hazards. Such properties are subject to the City's Hillside Management Ordinance.

Permitted uses include detached single-family residences and ancillary uses, limited commercial recreational uses with discretionary review, places for religious worship (with discretionary review), public schools, group homes, and public and private hiking trails and related facilities. Clustering of units is preferred to achieve protection of sensitive resources and avoid environmentally challenged areas.

Permitted density calculations are based on a three-step approach described beginning on page 2-19 of this chapter. The steps involve determining the maximum density allowed based on the average slope calculation for a property, identifying biological resources to be protected, and meeting performance standards crafted specifically for the hillsides.

**Very Low Density Residential**

Unit Density: 1-2 du/ac

Population Density: approximately 6 persons/acre

Areas designated Very Low Density Residential provide for very low-density, detached dwelling units on properties that are moderately constrained by topographic or geologic conditions, accessibility issues, and wildland fire hazards. Such properties may be subject to the Hillside Management Ordinance.

Permitted uses include detached single-family residences and ancillary uses, limited commercial recreational uses with discretionary review, places for religious worship (with discretionary review), public schools, group homes, and public and private hiking trails and related facilities.

**Low Density Residential**

Unit Density: 1-6 du/ac

Population Density: approximately 16 persons/acre

The Low Density Residential land use designation provides for the development of low-density single-family dwellings and accessory buildings. Types of uses include detached single-family homes and attached single-family residences (townhouses, row houses). Other uses permitted consistent with zoning ordinance requirements include:

- Group homes
- Religious and similar institutional facilities
- Public and private schools
- Hiking and other trails
- Public parks

**Medium Density Residential**

Unit Density: 6.1-12.0 du/ac

Population Density: approximately 32 persons/ acre

The Medium Density Residential designation provides for the development of medium-density single-family and multi-family dwelling units. The emphasis is on locating such development convenient to nonresidential uses that meet a variety of community needs. Permitted uses include detached and attached single-family homes, apartments, townhouses, condominiums, mobile home parks, and duplexes, all of which should provide varying amounts of open space and landscaping. Other uses permitted consistent with zoning ordinance requirements include:

- Group homes
- Religious facilities and similar institutional uses
- Public and private schools
- Hiking and other trails
- Public parks

**High Density Residential**

Unit Density: 12.1-24.89 du/ac

Population Density: approximately 67 persons/ acre

High Density Residential areas typically are located near transportation, shopping, and local employment. This designation permits the development of quality condominiums, apartments, townhouses, and senior housing, combined with varying amounts of open space and landscaping. Other uses permitted consistent with zoning ordinance requirements include:

- Group homes
- Religious facilities and similar institutional use
- Public and private schools
- Community care facilities
- Hiking and other trails
- Public parks

**Commercial and  
Industrial  
Categories**

Seven commercial and industrial land use designations are established to reflect the diverse range of general commercial, business, office, manufacturing, and industrial businesses important to Brea.

**Neighborhood Commercial**

Maximum FAR: 0.35

The Neighborhood Commercial designation provides for a variety of retail, office, and service-oriented business activities that serve a local neighborhood area and population. Site development standards encourage small projects that provide appropriate setbacks, parking, landscaping, buffering from residential land use areas, and other features that create well-designed, attractive projects. Public and private hiking trails and related facilities can be established within the Neighborhood Commercial designation.

**General Commercial**

Maximum FAR: 0.5

The General Commercial designation creates areas where a broad range of retail, office, and service-oriented business activities can locate. Business should be intended to serve a community-wide area and population. Public and private hiking trails and related facilities can be established within the General Commercial designation.

**Regional Commercial**

Maximum FAR: 0.65

The Regional Commercial designation is intended for diversified commercial uses serving a wide regional market area. Uses include large retail centers, the Brea Mall, service-oriented business activities, and other similar uses. Uses determined to be compatible may also be allowed. These uses should be located near the SR-57 freeway and major arterial highways due to vehicular travel demands. Public and private hiking trails and related facilities can be established within the Regional Commercial designation.

**Recreational Commercial**

Maximum FAR: 0.4

The Recreational Commercial designation accommodates commercial uses that specifically provide a recreational or leisure component as the focal use. Examples include destination hotels/resorts, private country clubs and similar facilities, and private stables. Public and private hiking trails and related facilities can be established.

**Office/Financial Commercial**

Maximum FAR: 1.5

The Office/Financial Commercial designation provides for single-tenant and multi-tenant offices that house professional, legal, medical, financial, administrative, research and development, corporate and general business offices, and other uses. Support uses appropriate within the areas designated Office/Financial Commercial include small convenience or service commercial activities intended to meet the needs of the on-site employee population. Public and private hiking trails and related facilities can be established within the Office/Financial Commercial designation.

**Light Industrial**

Maximum FAR: 0.75

The Light Industrial designation accommodates industrial uses that are low intensity and contained entirely within buildings. Allowable uses include research and development, light manufacturing and processing, offices, light warehousing and storage, high-technology production, and related uses. Other uses determined to be compatible may also be allowed. Public and private hiking trails and related facilities can be established within the Light Industrial designation.

**General Industrial**

Maximum FAR: 0.75

The General Industrial designation identifies lands used for larger-scale, heavier industrial activities in areas where possible hazards, noise, and other disturbances will not impact residential neighborhoods, schools, and similar uses. Allowable uses include

research and development, manufacturing and processing, offices, warehousing and storage, high-technology production, and related uses. Other uses determined to be compatible may also be allowed. Public and private hiking trails and related facilities can be established within the General Industrial designation.

## Open Space and Recreation Areas

### **Parks and Recreation** No Intensity Limit

The Parks and Recreation designation allows for a variety of active recreational uses, including neighborhood parks, community parks, specialty parks (for example, sports park, skateboard park, and/or golf course), and regional parks. This designation is intended to provide a variety of recreational opportunities that meet the diverse needs and interests of the community. Public and private hiking trails and related facilities are encouraged within the Parks and Recreation designation.

### **Natural Open Space** No Intensity Limit

The Natural Open Space designation allows for passive outdoor activities and open space conservation. Within Natural Open Space areas, improvements will be limited to those necessary to facilitate access and accommodate walking, hiking, mountain biking, horseback riding, and wildlife and nature viewing. The designation is also intended to protect hillsides and major ridgelines, wildlife corridors, native vegetation communities, creeks and streams, wetlands, habitat areas, and other critical natural resources.

## Mixed Use Designations

### **Mixed Use I** Residential Density: 12.1 - 50 du/ac Population Density: approximately 135 persons/acre Maximum FAR: 3.00

The Mixed Use I designation provides areas for intense, mixed-use urban environments that offer opportunities for people to live, work, shop, and recreate without having to use their cars. This designation encourages vertical integration of compatible residential and commercial uses, whereby such uses share the

same building or lot. The Mixed Use I designation applies to Downtown Brea, and the Birch Street Corridor, as well as other infill sites located throughout the community with ready access to major roadways and public transit. Land uses specifically prohibited within the Mixed Use I designation include the manufacturing and processing of goods and materials, and any warehousing. Public and private hiking trails and related facilities can be established within the Mixed Use I designation.

**Mixed Use II**

Residential Density: 6.1 - 40 du/ac

Population Density: approximately 108 persons/acre

Maximum FAR: 2.00

The Mixed Use II designation provides opportunities for the coordinated development of urban villages that offer a diverse range of complementary land uses in close proximity to one another. Either vertical or horizontal integration of uses is permitted, with an emphasis on tying together the uses with pedestrian linkages. Residential densities at the higher end of the scale will be permitted for developments that clearly integrate uses.

The Mixed Use II designation applies to the Unocal Research Center property and similar larger properties that would benefit from a coordinated, integrated approach to development. Development densities and intensities at the upper end of the stated range will only be permitted for those projects that include public and/or private educational facilities, as well as functional and attractive public open space amenities. Land uses specifically prohibited within the Mixed Use II designation include regional-serving retail uses, the manufacturing and processing of goods and materials, and any warehousing. Public and private hiking trails and related facilities can be established within the Mixed Use II designation.

**Mixed Use III**

Residential Density: 6.1 – 18 du/ac

Population Density: approximately 50 persons/acre

Maximum FAR: 1.00

The Mixed Use III designation provides opportunities for the revitalization of deteriorated commercial centers by allowing the development of neighborhood-serving commercial uses and very low intensity offices paired with housing. Either vertical or horizontal integration of uses is permitted, with an emphasis on tying together the uses through pedestrian linkages and public open space, such as linkage parks or small plazas. Building forms would be less intense than those found in either Mixed Use I or II.

The Mixed Use III designation applies to specific properties within the Brea Towne Plaza Specific Plan, primarily those that front South Brea Boulevard and a portion of Imperial Highway. Land Uses specifically prohibited within the Mixed Use III designation include regional-serving retail uses, general industrial uses, and any warehousing. Public walkways and related facilities can be established in Mixed Use III designation.

Other Designations

**Public Facilities**

Maximum FAR: 1.0

The Public Facilities designation applies to existing and potential public facilities. These facilities have been so designated to acknowledge their location and ensure that suitable areas will be set aside to accommodate schools, government offices, post offices, fire and police stations, community centers, sewer and water facilities, and the Olinda-Alpha Landfill. Public and private hiking trails and related facilities can be established within the Public Facilities designation.

**Cemetery**

No Intensity Limit

The Cemetery designation applies to properties developed specifically for interment of the deceased and related purposes, including crematoriums, mausoleums, mortuaries, and maintenance facilities.

## Specific Plans

The land use designations establish basic guidelines for property development: permitted uses and maximum permitted development densities. Further requirements are spelled out in the City's zoning ordinance, the primary tool used to implement land use policy. Specific Plans represent a second tool; Specific Plans provide a detailed plan for development within a particular area that may or may not reflect standard zoning ordinance requirements. Several Specific Plans have been approved, and these plans dictate the types of uses permitted, applicable development standards (setbacks, heights, landscape, architecture, etc), and circulation and infrastructure improvements required. Figure CD-2, the Land Use Policy Map, indicates the boundaries of the following Specific Plans:

- Birch Hills
- Brea Industrial
- Brea Towne Plaza
- Carbon Canyon
- Olinda Heights (Ranch)
- Tomlinson Park

The City will continue to use these adopted plans to guide development of properties within the various Specific Plan boundaries. As a general practice, however, the City will discourage further use of Specific Plans unless circumstances indicate that a Specific Plan represents the best method of achieving a cohesive land use plan for a property or related properties.

## Hillside Development Policy

The hillsides represent a resource long valued for the scenic, open space, and biological resources they offer. The community has long identified its desire to preserve the hillsides as undeveloped open space. However, most properties are privately held, and the City must allow property owners to obtain some economic value from their lands, either by continuing to exploit the oil resources buried deep within the earth below or by transitioning the properties, where safe to do so, to some other economically viable use.

To balance its preservation goals with the realities of land use law, the City has crafted an approach to regulating the type, location, density, and intensity of residential development permitted, that works to protect sensitive lands. On a given property designated

Hillside Residential, the level of development that can take place is based on hillside development policies and the City's goals to preserve riparian areas, creeks, streams, wetlands, and wildlife habitat; slope; avoid oil production hazards (ongoing and residual); avoid earthquake fault zones; minimize fire hazards; require adequate levels of public services; and meet project-related and cumulative infrastructure demands. At the same time, incorporating growth management policies is also important given the isolated nature of the hillside areas relative to infrastructure and public services.

### Hillside Density Calculation Process

Calculating density yield involves a three-step approach. The first step involves establishing a maximum allowable residential density/yield on a given property based on average property slope. The second step requires the developer to identify opportunities and constraints to be considered in the drafting of a development plan. Third, the developer must demonstrate compliance with specified performance criteria.



*Brea residents and visitors to the community enjoy the hills that frame the City.*

**Step 1: Establish Allowed Density Based on Average Slope**

The slope of a hillside is a major determining factor on how much development on a hillside can take place. It determines the extent of grading, cut-and-fill activity, hillside erosion, and the placement and configuration of lots. An important concept to understand is that the steeper the slope, the more difficult – and costly – it is to build on that slope. The maximum allowable density based on the following average property slope ranges:

**Table CD-2  
Allowed Density Based on Average Slope**

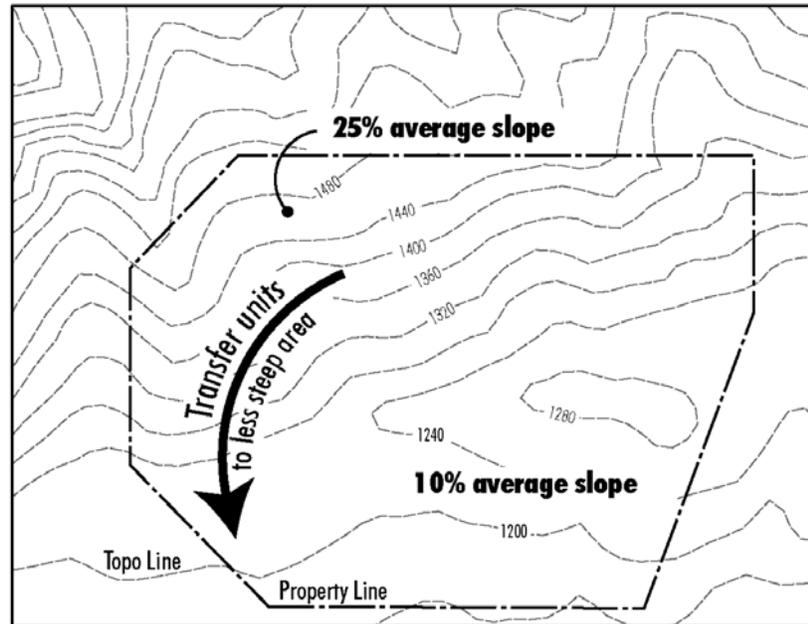
<b>Average Slope</b>	<b>Maximum Allowable Density</b>
Less than 10%, inclusive	2.2 units/acre
10.1 – 20%	1.6 units/acre
20.1 – 25%	1 unit/acre
25.1-30%	1 unit/5 acres
Greater than 30%	1 unit/20 acres

The maximum allowed yield will be considered a maximum limit and not an entitlement. Property owners may not be able to achieve the maximum number of units allowed on a given piece of land. Other factors, described below, must be considered in determining the unit yield (the so-called carrying capacity of a property).

Density transfers from one portion of a property to a more developable area will be permitted. As an example (see Figure CD-3), consider a 50-acre property with 30 acres having an average slope of 10% (maximum 2.2 units/acre) and the remaining 20 acres with a 25% slope (maximum 1 unit/acre). To avoid building on the steeper slopes, the developer may opt to transfer the 20 units allowed on the 25% slope land to the other portion of the property, essentially clustering units on less steep terrain.

Developers may opt to transfer units from very steep terrain to flatter portions of the same property, thus clustering residential units in a more developable area.

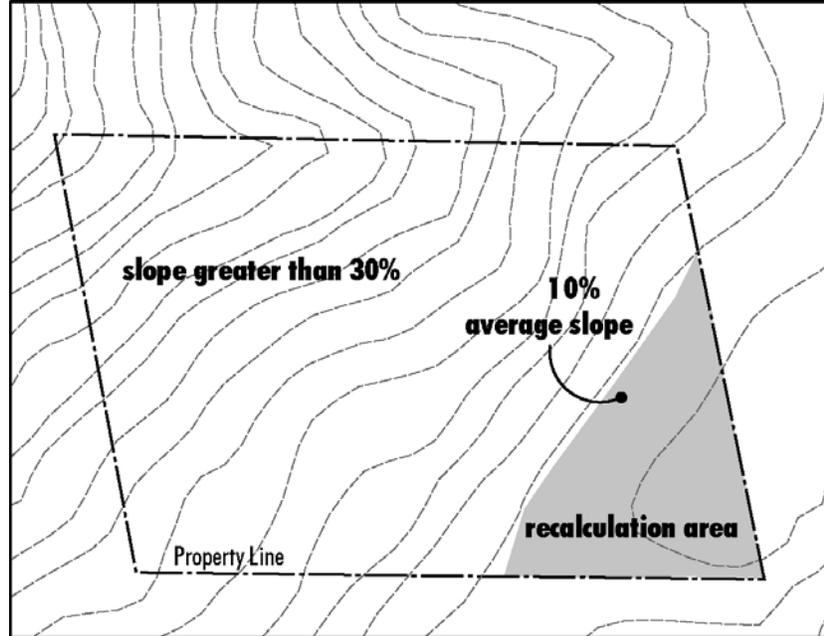
**Figure CD-3: Example of Unit Transfer**



As an alternative to the density transfer approach, a developer may exercise the option of identifying large contiguous areas of where the slope is 25% or less on a property. This area may be used as the basis for a recalculation of the average slope for that portion of the property (see Figure CD-4). Based on this recalculation, the appropriate maximum allowable density range may then be applied to that portion of the property (still subject to Steps 2 and 3), with the remainder of the property thereby preserved from future development as a condition of this option. For example, a 100-acre property with an average slope of 30% would be allowed a maximum of 1 unit per 20 acres under the slope density formula. If there were no other constraints on the property (Step 2) and all performance criteria could be met (Step 3), this would yield a maximum of 5 units for the 100-acre property. However, within this 100-acre property there could exist a contiguous 10-acre portion that has an average slope of 10%. The developer could opt to have the average slope recalculated on this piece of property, thereby yielding 2.2 units per acre, or 22 units for the 10-acre piece, subject to further evaluation through Steps 2 and 3. The developer would then be required to preserve the remainder of the property from further development as a consideration for the average density recalculation. The preservation of the remaining property could be achieved through a conservation easement, approved in form and content by the City Attorney, or other mechanism satisfactory to the City. The preserved property cannot be counted toward any open space requirements of the actual development itself.

*Developers may opt to recalculate the slope for areas that are not as steep.*

**Figure CD-4: Example of a Recalculation of Average Slope**



### **Step 2: Identify Opportunities and Constraints**

Goals for hillside areas clearly identify the community's desire to protect important visual, biotic, and open space resources. In addition, developing in areas that contain significant environmental resources is often more costly and more time consuming due to the need for higher level regulatory agency permits. Thus, the second step in the land use determination process involves identifying significant on-site resources that must be protected. Resources to be considered include, but are not limited to:

- Ridgelines (as identified in the City's Hillside Development Ordinance)
- Blueline streams
- Riparian habitat areas
- Other unique/sensitive habitats (woodlands and chaparral)

In determining the area available for development on a given property, the owner is encouraged to eliminate from consideration those areas containing these significant resources.

Another area requiring special consideration is the Alquist-Priolo Earthquake Fault Zone. State law limits the type of structures that can be built within the vicinity of active earthquake fault zones. Owners of properties affected by the designated fault zone will be required to undertake special studies and avoid development within constrained areas. No density transfers will be allowed from such affected areas.

**Step 3: Conformance with Performance Criteria**

Developers will be required to meet performance criteria when developing in the hillside areas. The following provide examples of the criteria to be used.

1. Development will be subject to the Hillside Management Ordinance.
2. All development must be located within an “X”-minute response zone for emergency services.
3. No road shall have a grade in excess of “Y”%.

Precise mechanisms for implementing this three-step approach are detailed in the Hillside Management Ordinance.

The allowed density applicable to hillside property, which is the subject of a development entitlement application, may be increased to the extent necessary to avoid the unconstitutional taking of the subject property. Any such increase shall be the subject of a finding by the Planning Commission or City Council, whichever is the final administrative body considering the application. The finding shall be supported by substantial evidence introduced during the hearing on the issue conducted by the Planning Commission and/or City Council.

## Goals and Policies

The following goals and policies address issues first of a citywide relevance and then those pertaining to specific focus areas within the planning area.

### Citywide

Brea is a diverse community. To continue this diversity, new development must be compatible with the established urban fabric. Residential, commercial, industrial, parks, and open space are to provide a balance and mixture of uses, not only complementing the existing community, but accommodating future needs and desires of the community.

<b>Goal CD-1</b>	<b>Provide a balance of land uses to meet the present and future needs of all residents.</b>
------------------	--

Policy CD-1.1 Create neighborhoods that effectively integrate single-family and multi-family housing with convenience and neighborhood shopping centers, park and recreation areas, and other uses appropriate for the neighborhoods.

Policy CD-1.2 Maintain a land use structure that balances the provision of jobs and housing with available infrastructure and public and human services.

Policy CD-1.3 Endeavor to create a mixture of employment opportunities for all economic levels of citizens.

Policy CD-1.4 Ensure that the City maintains a balance among residential, commercial, and industrial land uses.

Policy CD-1.5 Provide opportunities for development of housing that responds to diverse community needs in terms of density, size, location, design, and cost.

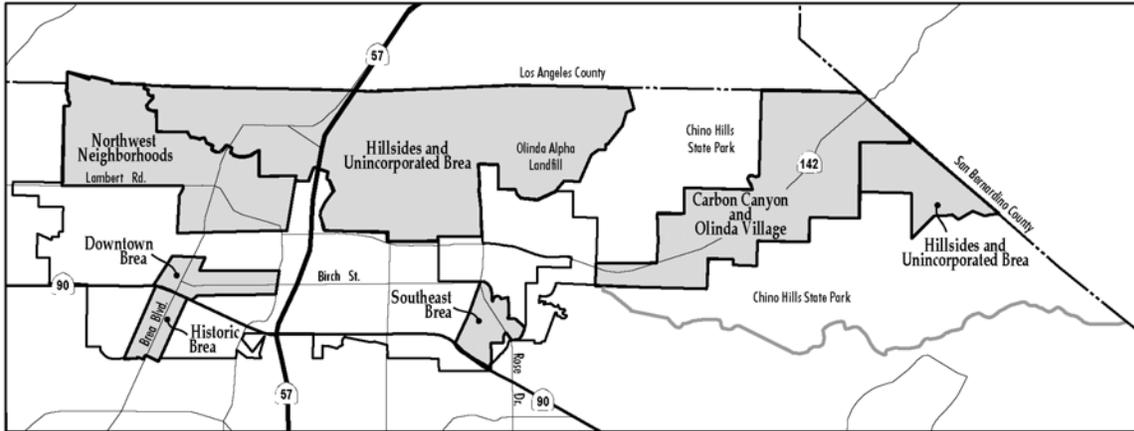
Policy CD-1.6 Accommodate a broad range of business uses that provide employment at all income levels and that make a positive contribution to the City's tax base.

- Policy CD-1.7 Create and maintain linked open spaces and pedestrian access that serve the entire community.
- Policy CD-1.8 Connect, where possible, all neighborhoods of the community and surrounding areas located within the City and Sphere of Influence with greenways and well-planned, well-located park areas. Maximize connections to hillside and open space areas.
- Policy CD-1.9 Encourage new development that is organized around compact, walkable, mixed-use neighborhoods and districts to conserve open space resources, minimize infrastructure costs, and reduce reliance on the automobile.
- Policy CD-1.10 Preserve open space wherever possible, especially in the hillside areas.
- Policy CD-1.11 Maintain a mixture of business and retail uses within the community.
- Policy CD-1.12 Preserve existing older but well-maintained neighborhoods.
- Policy CD-1.13 Address “mansionization” in the community.
- Policy CD-1.14 Update and enforce the Neighborhood Preservation Ordinance.
- Policy CD-1.15 Strongly encourage the master planning of any large contiguous land holdings.
- Policy CD-1.15 Strongly encourage the rezoning and annexation of unincorporated properties in the City of Brea in order to avoid the creation of new County islands.

**Focus Areas** Specific goals and policies have been developed for six distinct areas (Figure CD-5) that need special planning attention:

- Northwest Neighborhoods
- Downtown Brea
- Historic Brea
- Carbon Canyon and Olinda Village
- Hillsides and Unincorporated Brea
- Southeast Brea

**Figure CD-5 Focus Areas**



**Northwest Neighborhoods**

The northwest corner of Brea contains a mix of residential neighborhoods, including several senior housing developments. The edges of several northern neighborhoods abut undeveloped open space in unincorporated Orange County. Most of the homes were built between the 1950s and 1970s, with some newer tracts built in the 1980s and 1990s. Given the age of the homes, focused attention will be required to maintain the quality of the neighborhoods.

A key opportunity exists to revitalize aging commercial strips with either new neighborhood-serving commercial centers or mixed-use developments that provide both needed housing and local commercial shops and services.

**Goal CD-2 Preserve and enhance the character of neighborhoods in northwest Brea.**

**Policy CD-2.1** Ensure that the design of new residential developments is sensitive to the character of existing neighborhoods.

- Policy CD-2.2 Ensure that new developments are integrated with established neighborhoods through a network of street and pedestrian connections.
- Policy CD-2.3 Pursue circulation improvements that promote safe vehicle speeds. Utilize creative methods to reduce speeds, and improve circulation such as timed traffic lights and traffic calming devices.
- Policy CD-2.4 Preserve existing neighborhood characteristics, including tree-lined streets, sidewalks, and building orientation.
- Policy CD-2.5 Improve existing small, commercial centers to improve access, aesthetics, and business success.

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<b>Goal CD-3</b>	<b>Improve access to transportation, shopping, and community services throughout existing neighborhoods.</b>
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- Policy CD-3.1 Promote greater mobility through pedestrian improvements and improved transit access.
- Policy CD-3.2 Increase the number and variety of services, transportation access, and activity centers for seniors.
- Policy CD-3.4 Encourage local retail businesses to serve the Northwest area.
- Policy CD-3.5 Provide visual links between the Northwest area Downtown Brea.
- Policy CD-3.6 Provide appropriate and accessible public transportation service to the Northwest neighborhoods.

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**Downtown Brea**

The success of Downtown can be attributed to its integration of residential and commercial land uses. Brea residents and visitors from surrounding communities enjoy the shopping, entertainment, and dining available within a compact, pedestrian-friendly area. Linking surrounding neighborhoods to Downtown Brea is vital to the continued success of Downtown.

<b>Goal CD-4</b>	<b>Maintain and improve the vitality, economic strength, accessibility, and livability of Downtown.</b>
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- Policy CD-4.1 Explore economic and employment opportunities to diversify the business mix in Downtown.
- Policy CD-4.2 Improve transportation, pedestrian, and visual connections between Downtown and the rest of the community.
- Policy CD-4.3 Utilize traffic calming measures as appropriate to improve safety and access.
- Policy CD-4.4 Extend mixed-use development from Downtown along the Brea Boulevard and Birch Street corridors.
- Policy CD-4.5 Create large interactive and inviting public spaces.
- Policy CD-4.6 Build linkages between Downtown and the Civic Center/Brea Mall area.
- Policy CD-4.7 Encourage the use of historic features in the Downtown area.
- Policy CD-4.8 Explore the use of a trolley system to serve Downtown and surrounding areas.

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**Historic Brea**

Neighborhoods in southwest Brea contain a concentration of homes dating to the early twentieth century. City Hall Park is a unique and treasured community resource, containing Brea’s Old City Hall, the Brea Plunge, and the Old American Legion Hall. The South Walnut residential neighborhood surrounding City Hall Park contains many structures 80 to 90 years old that have not been properly repaired and are in danger of ruin. Many of these structures exhibit craftsmanship characteristic of their era and incorporate unique materials.

South Brea Boulevard serves as the primary arterial for the neighborhoods to the east and west of it. While the street is primarily oriented toward automobile uses, its current mix of

services and shops is relatively accessible to the pedestrian, and the street functions as a community gathering place as neighbors meet while running errands or at local restaurants. Many of the commercial properties, however, are deteriorated and in need of revitalization. In addition, improvements to the pedestrian environment would enhance this area that serves as the “public living room” for the neighborhoods to the east and west of the Boulevard. Preserving the unique community environment that exists on this thoroughfare today, while providing for opportunities to revitalize the commercial uses, create appropriate housing opportunities, improve pedestrian accessibility, and avoid impacts on surrounding neighborhoods, is a priority for this area.

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<b>Goal CD-5</b>	<b>Preserve Brea’s unique historic and cultural resources and neighborhoods.</b>
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- Policy CD-5.1 Ensure new development is compatible with the style, theme, and design of established structures and neighborhoods.
- Policy CD-5.2 Promote preservation of historic single-family homes by ensuring that General Plan and zoning designations reflect the single-family nature of specific neighborhoods, and by providing City resources or incentives that foster rehabilitation.
- Policy CD-5.3 Provide landscaping and amenities that complement historic resources and neighborhoods.
- Policy CD-5.4 Ensure that development within and surrounding City Hall Park respects and responds to this important resource.
- Policy CD-5.5 Create an easily identifiable historic district in Brea that is closely linked with Downtown.
- Policy CD-5.6 Establish design guidelines of standards for commercial development on South Brea Boulevard that respect and complement the historic character of surrounding neighborhoods.
- Policy CD-5.7 Establish a program that would enable historic neighborhoods to be designated as either a landmark district or historic overlay zone.

<b>Goal CD-6</b>	<b>Provide for the revitalization of the South Brea Boulevard Core.</b>
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- Policy CD-6.1 Encourage redevelopment of South Brea Boulevard properties through economic development.
- Policy CD-6.2 Where housing is a component of a project within properties in the Mixed Use III area, the development of ownership housing opportunities shall be encouraged.
- Policy CD-6.3 Ensure that traffic generated by new development along South Brea Boulevard, particularly where that traffic generated by new residential development, takes ingress/egress from South Brea Boulevard and is not directed into neighborhood streets.
- Policy CD-6.4 Develop programs, such as traffic calming, that discourage use of neighborhood streets for regional cut-through traffic.
- Policy CD-6.5 Encourage the inclusion of open spaces, linkage parks, street trees, and enhanced pedestrian amenities on South Brea Boulevard.

**Carbon Canyon and Olinda Village**

Carbon Canyon contains important biological habitat. Prior community visioning programs have emphasized the community’s ideas regarding resource preservation. Olinda Village is a distinct residential neighborhood within Carbon Canyon. The rural character of the area, together with the development constraints imposed by topography, geologic conditions, and the configuration of Carbon Canyon Road, a State highway, may limit land use opportunities within Carbon Canyon.

<b>Goal CD-7</b>	<b>Create an environment in Carbon Canyon that balances the community’s long-term housing needs with community open space, habitat conservation, and public safety goals.</b>
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- Policy CD-7.1 Base allowable development on the ability of infrastructure, landforms, physical constraints, and emergency response capabilities to support new development.

- Policy CD-7.2 Preserve key wildlife migration corridors and habitat areas within Carbon Canyon.
- Policy CD-7.3 Allow and encourage clustering of housing as a means of protecting resources.
- Policy CD-7.4 Require that development preserve prominent landforms consistent with the City's hillside management ordinance.
- Policy CD-7.5 Limit commercial uses in Olinda Village to those that serve local neighborhood needs.
- Policy CD-7.6 Preserve a rural atmosphere in Olinda Village.
- Policy CD-7.7 Consider establishing a transfer of development rights (TDR) ordinance that would apply to Carbon Canyon as a means of preserving sensitive hillside areas.
- Policy CD-7.8 Consider alternatives to a specific plan for regulating land use within the Carbon Canyon area.

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### **Hillsides and Unincorporated Brea**

Brea values the hillsides. Landowners will continue to seek entitlements for development, and the City will continue to balance development interests with broader community preservation goals.

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<b>Goal CD-8</b>	<b>Minimize the extent of urban development in the hillsides, and mitigate any adverse consequences associated with urbanization.</b>
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- Policy CD-8.1 Aggressively pursue methods to preserve open space and natural habitat, including but not limited to:
- 1) Negotiating with property owners during the development process
  - 2) Regulating preservation of ridgelines and hillsides
  - 3) Acquiring private lands using City funds, State and Federal funds, grants, bonds, or assessment districts

- 4) Mitigation banking
- 5) Transfer of development rights

- Policy CD-8.2 Base allowable development on the ability of infrastructure, landform, physical constraints, and emergency response capabilities to support new development.
- Policy CD-8.3 Require developers to employ sustainable approaches to development and environmentally sensitive design.
- Policy CD-8.4 Ensure that Olinda Landfill operations allow for eventual reclamation and reuse of the property for open space purposes.
- Policy CD-8.5 Support efforts of State and federal agencies and private conservation organizations to acquire properties for open space and conservation uses.
- Policy CD-8.6 Ensure that any new development that does occur is integrated effectively into the existing City via convenient street and/or pedestrian connections, as well as visual connections.
- Policy CD-8.7 Work closely with the County of Orange and emphasize the City's need to participate in the development review process of projects proposed in surrounding unincorporated areas. Work to ensure that such developments proceed consistent with City standards.
- Policy CD-8.8 Ensure that height and density levels are limited for development in hillside areas or in areas of steep topography in order to preserve life safety.

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**Southeast Brea**

Southeast Brea includes the Unocal Research Center property and other surrounding former oil-use-related and agricultural properties that are expected to transition over time to other uses.

<b>Goal CD-9</b>	<b>Create a dynamic, mixed-use urban village that integrates a range of housing types (including senior housing), moderate-intensity commercial uses, educational and public uses, and parks.</b>
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Policy CD-9.1 Ensure that new commercial uses complement rather than compete with businesses along Imperial Highway and in Downtown.

Policy CD-9.2 Accommodate emerging housing trends, and encourage pedestrian linkage to surrounding neighborhoods and activity centers.

Policy CD-9.3 Encourage the establishment of community recreation and park facilities in the area.

Policy CD-9.4 Support efforts to establish quality, community institutions in the area.

Policy CD-9.5 Provide quality, affordable housing that would accommodate young families, college students, and educators.

Policy CD-9.6 Preserve open space within this area, and provide outdoor recreation facilities.

Policy CD-9.7 Strongly encourage the master planning of any large contiguous land holdings in this area.

Policy CD-9.8 Strongly encourage the rezoning and annexation of the unincorporated properties in this area into the City the Brea in order to avoid the creation of new County islands.



See Section I of the Implementation Guide for action programs.

## Implications of Land Use Policy

Over time, as properties recycle to new uses and vacant lands are developed, the distribution of uses within the community will change. Table CD-3 on the following page summarizes the distribution of uses and shows that buildout of the planning area pursuant to Land Use policy will allow many new residents to call Brea “home.”

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**Table CD-3  
Implications of Land Use Policy**

	Assumptions				Net Acres			Projected Dwelling Units / Households			Projected Non-Residential Square Feet (Thousands)			Projected Population		
	Maximum Du/Acre	FAR	Average Du/Acre <sup>1</sup>	FAR <sup>2</sup>	Existing City	Sphere of Influence	Total	Existing City	Sphere of Influence	Total	Existing City	Sphere of Influence	Total	Existing City	Sphere of Influence	Total
<b>Residential Land Use Designations</b>																
Hillside Residential <sup>3</sup>					46	1,609	1,655	262	986	1,248				693	2,609	3,302
Very Low Density Residential	2		0.5		821	8	829	410	4	414				1,086	10	1,096
Low Density Residential Developable	6		5		1,161	293	1,454	5,805	1,464	7,269				15,360	3,872	19,233
Non-Developable (easements/open space)					134		134									
Medium Density Residential	12		10		311	12	323	3,105	120	3,225				8,217	318	8,534
High Density Residential	24.89		20		259		259	5,119		5,119				13,545		13,545
<b>Commercial and Business Land Uses</b>																
Regional Commercial		0.65		0.39	102		102				1,727		1,727			
General Commercial		0.5		0.25	216		216				2,353		2,353			
Neighborhood Commercial		0.35		0.3	14		14				179		179			
Recreational Commercial		0.4		0.1	49		49				215		215			
Office/Financial		1.5		0.4	72		72				1,249		1,249			
Light Industrial		0.75		0.4	655		655				11,421		11,421			
General Industrial		0.75		0.4	310		310				5,402		5,402			
<b>Open Spaces</b>																
Parks/Recreation/Open Space		0.5		0.1	322	166	488				1,401	724	2,125			
Natural Open Space		0		0	451	3,356	3,807									
Agriculture		0.25		0.1	0		0									
<b>Other Land Uses</b>																
Mixed Use I					58		58									
Residential (20% of total)	50		20		12		12	232		232				614		614
Commercial (80% of total)		3.0		1.0	46		46				2,022		2,022			
Mixed Use II					119		119									
Residential (72% of total)	40		15		86		86	1,326		1,326				3,508		3,508
Commercial (20% of total)		2.0		0.5	24		24				518		518			
Parks/Open Space (8% of total)					10		10									
Mixed Use III					25		25									
Residential (72% of total)	18		15		18		18	279		279				738		738
Commercial (20% of total)		2.0		0.5	5		5				109		109			
Parks/Open Space (8% of total)					2		2									
Public Facilities		1.0		0.4	177	481	659				3,092	8,385	11,477			
Cemetery					25		25									
<b>TOTAL</b>					5,351	5,925	11,250	16,538	2,573	19,112	29,687	9,109	38,797	43,760	6,809	50,570

*Assumptions and Estimates*

1. Average assumptions derived from TAZ existing land use analysis
2. Estimates and assumptions derived from TAZ existing land use analysis
3. Density slope calculation used to determine number of units

*Additional Assumptions*

- 2.7 Persons per Household
- Vacancy Rate of 1.6% per 2000 Census



# CIRCULATION

Moving people and goods between destinations within the urban environment efficiently and effectively allows a community to function well economically and socially. People should be able to circulate within their community from home to school, work, or shopping with ease and safety. Alternatives to the private car – transit, biking, walking, and carpooling – can offer choice and convenience. Goods must also be easily transported locally and regionally to retain a working economy.

This Circulation section guides continued development of the circulation system to support planned growth. New development will increase use of local and regional roadways, and the plan and policies here identify strategies the City will pursue to maintain good service levels. Because local circulation is linked with the regional system, policies highlight Brea’s continued need to participate in regional programs to alleviate traffic congestion through capacity enhancements and trip reduction. Reduced dependency on the automobile works toward these goals and also improves environmental quality.

## Context

When considering circulation, we think of both the physical infrastructure systems – the roadways, rails, and trails – as well as the method of getting around, by car or bike or bus or on foot. In Brea, the physical system includes the local street network and several routes of regional significance: State Route 57, Carbon Canyon and Lambert Roads, Brea Boulevard, and Imperial Highway. While these regional roads provide Breans with ready access to surrounding destinations, the roads also bring regional traffic into and through the City. Much of the traffic traveling on the freeway, Carbon Canyon Road, and Imperial Highway during peak periods simply passes through Brea to other employment centers.

### Regional Transportation Planning

As part of a large metropolitan area and of necessity, Brea has integrated its local street system with existing and planned regional and local systems. Transportation planning and management require cooperation and coordination among local cities and the Orange County Transportation Authority (OCTA), as well as jurisdictions and agencies in adjacent Los Angeles County. Working together, agencies can address the physical infrastructure needed to support regional demands and to ensure that

convenient alternative transportation modes allow for an integrated approach to addressing traffic problems.

In Orange County, OCTA has adopted the County of Orange Master Plan of Arterial Highways (MPAH) to define the intended future roadway system for the County. All cities, including Brea, must reflect the MPAH in their General Plans to receive Measure M transportation funds. OCTA also is responsible for administering the State-mandated Congestion Management Program, or CMP. The goals of the CMP are to reduce traffic congestion and to provide a mechanism for coordinating land use development and transportation improvement decisions.

The California Department of Transportation (Caltrans) has authority over the State highway system, including mainline facilities and interchanges. Caltrans must be involved in and approve the planning and design of improvements for state highway facilities. State highway facilities in Brea include State Route 57 and associated interchanges, Imperial Highway, Carbon Canyon Road, Valencia Avenue, and Imperial Highway.

### Transportation Services

OCTA provides transit and paratransit service within the Brea planning area and throughout Orange County. The agency also coordinates commuter rail service in the County, including leading efforts to construct light rail routes consistent with adopted plans. To supplement this service for more localized needs, the City operates the Brea Shuttle Express, a curb-to-curb transportation shuttle.

### Master Plan of Countywide Bikeways

The County's *Master Plan of Countywide Bikeways* designates locations and classes of bike routes in unincorporated areas and through cities. The primary aim is to provide consistency between the countywide and local jurisdiction bikeway plans. The Brea bikeway plan in this chapter utilizes the countywide classification system and links to County routes.

## The Street System and Master Plan of Roadways

Brea's street system is defined by a hierarchical classification system, with roadway functional classifications differentiated by size, function, and capacity. The categories are:

**Freeway:** Freeways are limited-access, high-speed travelways included in the State and federal highway systems. Their purpose is to carry regional through traffic (traffic passing through Brea

without stopping). Access is provided at interchanges. State Route 57 is the only local freeway connecting Brea to regional destinations.

**Major Arterial:** Typically constructed within a right-of-way of 120 feet, with a curb-to-curb pavement width of 102 feet, this roadway has a maximum capacity of 56,300 average daily trips and a peak-hour capacity of 5,630 trips. Major Arterials carry a large volume of regional traffic not handled by the freeway. They are typically six-lane, divided roadways. The roadway geometrics and right-of-way width may vary depending on localized conditions.

**Primary Arterial:** Typically constructed within a right-of-way of 100 feet, with a curb-to-curb pavement width of 84 feet, this roadway has a maximum capacity of 37,500 average daily trips and a peak-hour capacity of 3,750 trips. The roadway geometrics and right-of-way width may vary depending on localized conditions. A Primary Arterial's function is similar to that of a Major Arterial, with the principal difference being capacity. Primary Arterials are typically four-lane divided roadways.

**Modified Secondary Arterial:** A Modified Secondary Arterial is a two- to four-lane roadway constructed within a right-of-way of 100 feet, with a varying curb-to-curb pavement width, depending upon the number of lanes provided. The street functions similar to a Secondary Arterial. The right-of-way not improved with paved lanes can be used for a bike path or lane, multi-use trail, and extensive landscaping.

**Secondary Arterial:** Typically constructed within a right-of-way of 80 feet with a curb-to-curb pavement width of 64 feet, Secondary Arterials serve as collectors that distribute traffic between local streets and four- to six-lane divided arterials. The maximum roadway capacity is 25,000 average daily trips and 2,500 peak-hour trips.

**Collector Roadway:** A Collector roadway is a two- to four-lane unrestricted access roadway with capacity ranging from 12,500 average daily trips for two-lane undivided to 25,000 average daily trips for four-lane roads (with peak-hour capacities of 1,250 and 2,500, respectively.) By strict definition, a Collector facility is not an arterial highway. It differs from a local street in its ability to handle through traffic movements between arterials.

**Local Roadway:** A local roadway provides direct access to abutting properties as their primary function. Local streets rarely have more than two travel lanes, and speed limits are generally kept low (25 miles per hour).

**Smart Street:** In addition to the arterial classification, the MPAH recognizes Smart Streets as arterials with enhanced traffic-carrying capacity. Such enhancements include:

- Addition of through or turn lanes
- Preferential traffic signal timing and synchronization
- Removal of on street parking
- Intersection grade separations
- Grade separated turning movements
- Access limitations
- Access consolidations
- Frontage roads
- Pedestrian grade separations

Figures CD-6 and CD-7 show schematic cross sections of each category of arterial and collector roadway. These sections represent desirable standards, but variation in right-of-way width and specific road improvements will occur in certain cases due to physical constraints and/or right-of-way limitations. In particular, the median width of Major and Primary Arterials will vary according to the area being served, right-of-way constraints, and turn lane requirements. Any of the Arterial classifications may deviate from the standards where physical constraints exist or where preservation of community character dictates special treatment.

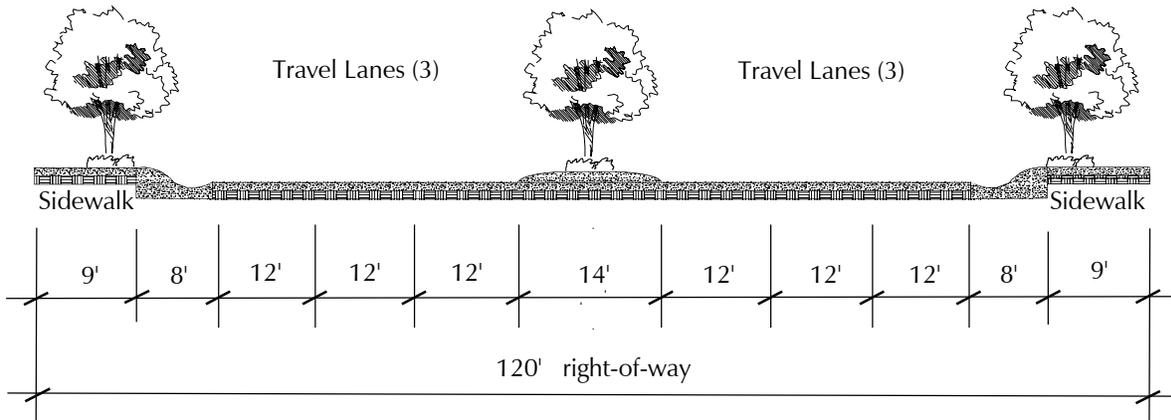
Bikeways and sidewalks also affect the specific standards applied to various facilities. Another design consideration is the need to comply with Brea capacity requirements. The overriding circulation goal is that all roadways carry the designed volumes of traffic at the desired level of service.

## Level of Service Standards

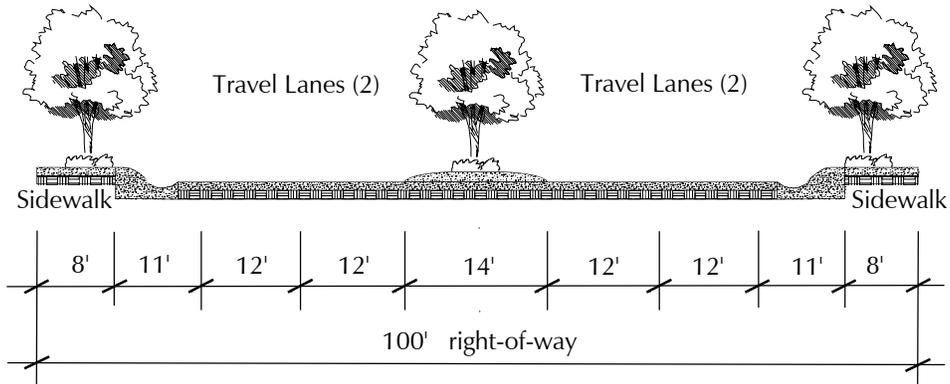
Evaluating the ability of the circulation system to serve Brea's residents and businesses requires establishing suitable performance criteria. Performance criteria have a policy component that establishes a desired level of service and a technical component that specifies how traffic forecast data can be used to measure the achievement of the criteria.

For Brea, the volume-to-capacity (V/C) ratio represents the criteria used to measure level of service. A V/C ratio is calculated based on average daily traffic volumes on a roadway and the daily capacity value for that roadway. A level of service (LOS) scale is used to evaluate roadway performance based on the V/C ratio. The levels range from "A" to "F", with LOS A representing free-flow traffic and LOS F representing extreme congestion, with traffic

Major Arterial



Primary Arterial



Modified Secondary Arterial

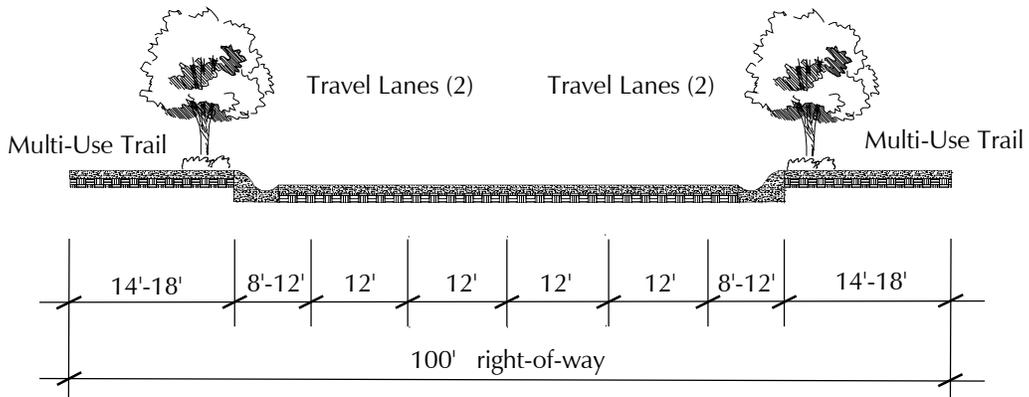
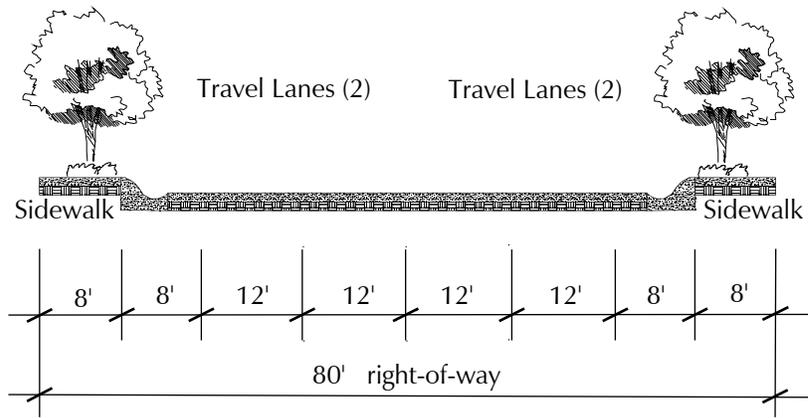
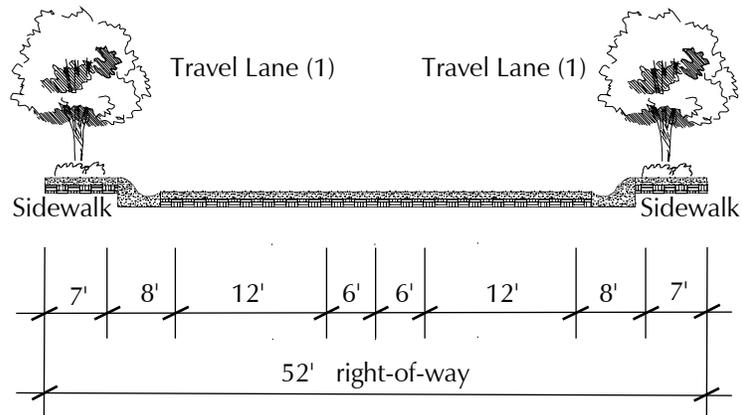


Figure CD-6  
Typical Roadway Cross Sections

Secondary Arterial



Collector Roadway (with Median Turn Lane)



Collector Roadway

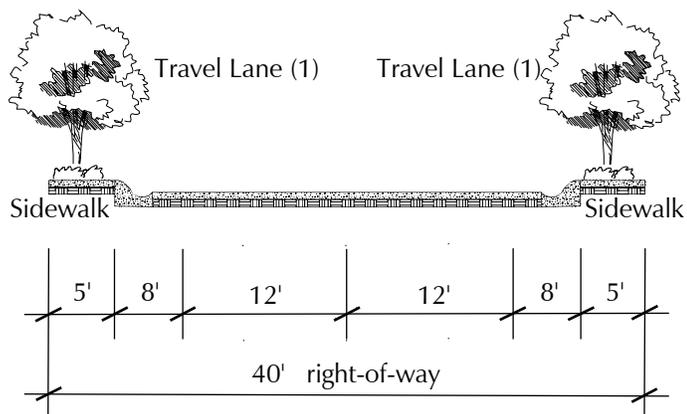


Figure CD-7  
Typical Roadway Cross Sections

levels above the capacity of the facility. Descriptions of traffic flow for the different levels of service are provided in Table CD-4.

**Table CD-4  
Level of Service Definition**

**Signalized Intersections**

Level of Service (LOS)	Vehicle Delay (seconds/vehicle)	Volume to Capacity Ratio (V/C)	Description
A	≤ 5.00	0.00-0.60	Free Flow/Insignificant Delays: No approach phase is fully utilized by traffic and no vehicle waits longer than one red indication.
B	5.1-15.0	0.61-0.70	Stable Operation/Minimal Delays: An occasional approach phase is fully utilized. Many drivers feel somewhat restricted within platoons of vehicles.
C	15.1-25.0	0.71-0.80	Stable Operation/Acceptable Delays: Major approach phases fully utilized. Most drivers feel somewhat restricted.
D	25.1-40.0	0.81-0.90	Approaching Unstable/Tolerable Delays: Drivers may have to wait through more than one red signal indication. Queues may develop but dissipate rapidly, without excessive delays.
E	40.1-60.0	0.91-1.00	Unstable Operation/Significant Delays: Volumes at or near capacity. Vehicles may wait through several signal cycles. Long queues form upstream from intersection.
F	≥ 60.0	>1.00	Forced Flow/Excessive Delays: Represents jammed conditions. Intersection operates below capacity with low volumes. Queues may block upstream intersections.

**Unsignalized Intersections**

Level of Service	Vehicle Delay (seconds/vehicle)	Description
A	≤ 5	Little or no delay.
B	> 5 and ≤ 10	Short traffic delay.
C	> 10 and ≤ 20	Average traffic delays.
D	> 20 and ≤ 30	Long traffic delays.
E	> 30 and ≤ 45	Very long traffic delays.
F	> 45	Extreme delays potentially affecting other traffic movements in the intersection.

Source: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington DC 1994.

**Roadway  
Congestion  
Concerns**

Within Brea, Imperial Highway has been identified as part of the highway system identified in the Orange County CMP. Per the CMP and Orange County Growth Management Plan, the LOS at identified intersections must be maintained at LOS D or better for Brea to be eligible to receive earmarked State transportation funds. The CMP intersections in Brea are State College Boulevard at Imperial Highway, Valencia Avenue at Imperial Highway, and the SR-57 ramps at Imperial Highway.

The most critical time of the day for traffic operations in Brea is during the morning and evening peak hours, typically lasting from 6 to 8 A.M. in the morning and 4 to 6 P.M. in the evening. The freeway interchanges at Brea Boulevard, Lambert Road, and Imperial Highway are heavily congested during peak hours of the

day. Roads directly serving the local public schools also experience their highest levels of congestion during the morning and mid-afternoon school peak periods.

During evening commute hours, Valencia Avenue and Lambert Road/Carbon Canyon Road (SR-142) experience LOS F conditions. Carbon Canyon Road is one of the few roads that connect Brea and Orange County to San Bernardino County to the east. Thus, commuters use the road as a regional link between housing in San Bernardino and Riverside counties and employment in Orange County. East of the Olinda Ranch neighborhood, Carbon Canyon Road remains a two-lane highway with severe traffic during both A.M. and P.M. peak hours. Topography and sensitive habitat within Carbon Canyon severely limit opportunities to widen the roadway to relieve congestion.

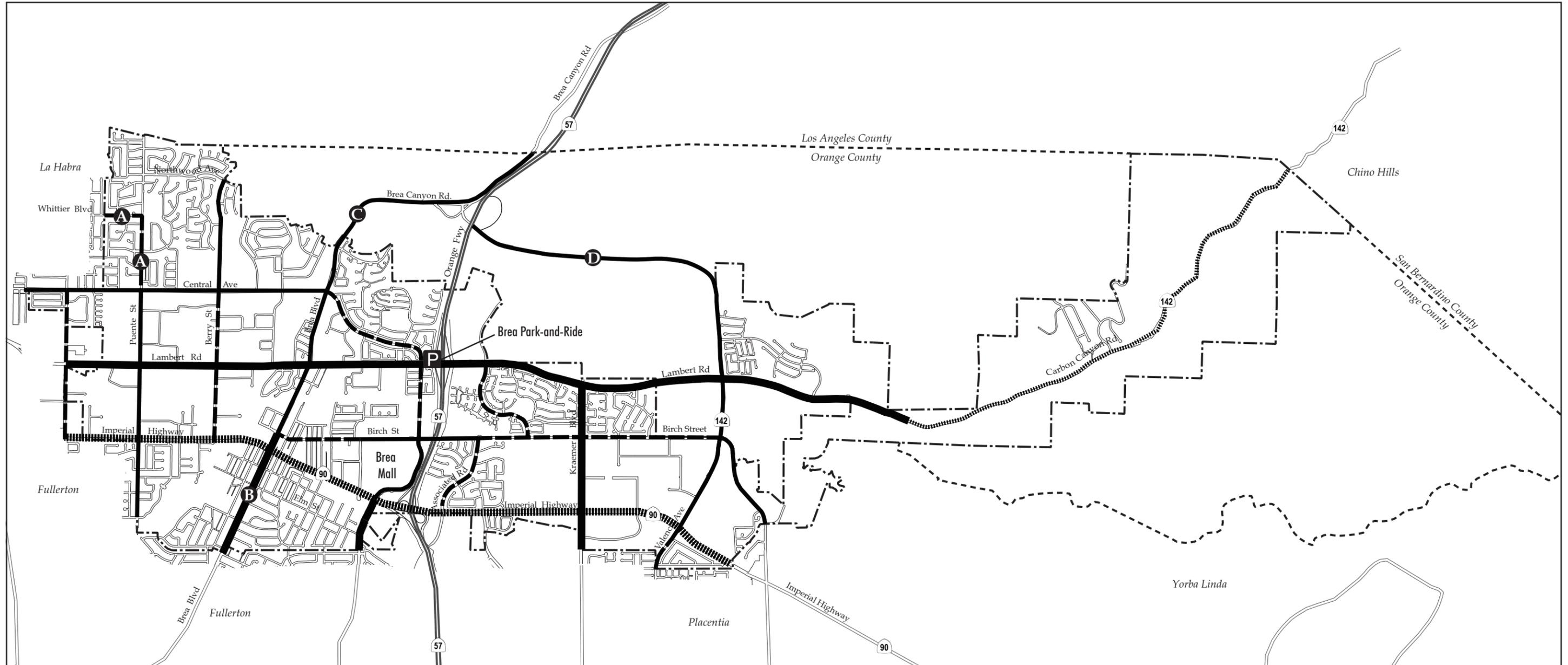
Imperial Highway, also a major east-west roadway, carries extremely high traffic volumes during peak hours and also can experience congestion during off-peak periods.

## Master Plan of Roadways

To address areas where congestion historically has occurred and to accommodate long-term traffic needs, the City has developed a Master Plan of Roadways, illustrated in Figure CD-7.

The Plan meets Brea's long-term circulation needs, reflects City policies regarding environmental resource protection, and largely implements County Master Plan of Arterial Highways. The City Master Roadway Plan provides for changes to the County MPAH as described below. Figure CD-8 shows current MPAH classification for these segments, with the indication that the classifications will be changed following changes to the MPAH. The Master Plan of Roadways plans for the following changes:

- **Reclassify Puente Avenue and Whittier Boulevard as a Collector Arterial.** This modification accurately represents the function of this roadway.
- **Reclassify Brea Canyon Road (north of Lambert Road) as a Modified Secondary Arterial.** The Modified Secondary Arterial classification best reflects the function Brea Canyon Boulevard serves during most travel hours. Only during the evening commute does the roadway experience heavy use, typically as an alternative to the crowded SR-57 freeway. The City will preserve right-of-way adequate for a Primary Arterial, but the Secondary designation indicates the anticipated roadway function.



**Legend**

- City Boundary
- - - Sphere of Influence

**Arterial Highways**

- Major Arterial
- Smart Street
- Primary Arterial
- Secondary Arterial
- Modified Secondary Arterial
- Collector/Commuter Roadway
- Park and Ride

**Notes:**

- A** Whittier Boulevard and Puente Street north of Central Avenue will be redesignated as Collector/Commuter Roadway following parallel changes to the Orange County Transportation Authority (OCTA) Master Plan of Arterial Highways (MPAH).
- B** South Brea Boulevard south of Imperial Highway will be redesignated Primary Arterial following parallel changes to the OCTA MPAH.
- C** Brea Canyon Road will be redesignated Modified Secondary Arterial following changes to the OCTA MPAH.
- D** The Valencia Avenue extension between the entrance to the Olinda Alpha landfill and SR-57 will be deleted following parallel changes to the OCTA MPAH.

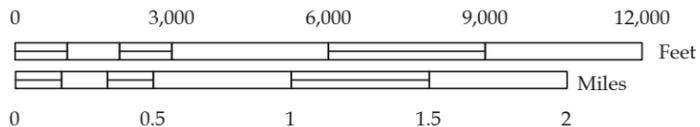
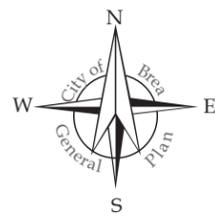


Figure CD-8  
Master Plan of Roadways



- **Reclassify South Brea Boulevard (south of Imperial Highway) as Primary Arterial.** This modification preserves right-of-way consistent with a Major Arterial standard but indicates the City's intention to maintain no more than two travel lanes in each direction. The land use plan provides for South Brea Boulevard to be a mixed-use, pedestrian-friendly district. The wide right-of-way will provide space for on-street parking, pedestrian enhancements, ample landscaping, and street-oriented development.
- **Eliminate Tonner/Valencia Avenue (north of Lambert Road) as Proposed Primary Arterial.** Land use policy north of Lambert Road does not support the MPAH alignment, nor do planned densities require a roadway of this size.

## Planned Circulation Improvements

Over time, the City will pursue transportation system improvements consistent with the Master Plan of Roadways. Focused improvements will provide for enhanced function of intersections, the control points in the circulation network. Examples of planned improvements include:

- State Route 57 northbound on-ramp at Lambert road widening project
- Rose Drive, Birch Street, and Valencia Avenue intersection improvements
- Kramer Avenue and Birch Street intersection improvements
- Imperial Highway Smart Street project
- Birch Street streetscape improvements
- Lambert Road streetscape improvements
- Various traffic safety projects

The City's Capital Improvement Program (CIP) will continue to be the tool used to identify needed circulation improvements and to commit required funding.

## Regional Circulation System

Coordination with regional and State transportation planning efforts and those efforts adjacent jurisdictions is essential to addressing the regional traffic concerns that impact Brea directly, particularly within the Four Corners area.

## Four Corners

The term “Four Corners” represents the intra-urban metropolitan area where the four county boundaries of Los Angeles, Orange, San Bernardino, and Riverside counties converge. Serving as the gateway between the Inland Empire and the more urbanized region of Los Angeles and Orange counties, during the latter decades of the twentieth century this area experienced a tremendous influx in commuter traffic and congestion. Expanding employment opportunities in Orange and Los Angeles counties and relatively affordable residential development in the Inland Empire have resulted in many, many commuters traveling from east to west in the morning and in the reverse direction after work. Demographic projections point toward continuation of this trend so that by 2020, many workers living in the Inland Empire are still expected to depend on jobs in other areas.

Four travel routes accommodate the east-west commute pattern: the SR-60 and SR-91 freeways, and two arterial streets, Grand Avenue through Diamond Bar and Carbon Canyon Road (SR-241) through Chino Hills and Brea. North-south traffic is restricted to SR-57 and Brea Canyon Road.<sup>2</sup> The Chino Hills and Santa Ana mountains limit opportunities for providing additional capacity.

According to the Four Corners June, 2000 study, the following issues and problems will intensify through the year 2020:

- Heavy directional commute problems resulting in significant delays during peak hours throughout the Four Corners area
- Major traffic streams through the very constrained convergence of the SR-60 and SR-57 freeways
- Slow eastbound traffic on SR-60 east of SR-57 due to a steep grade through the hills
- The presence of several colleges and universities along the SR-57 corridor, which increases congestion during the school year
- Use of Carbon Canyon Road to avoid freeway congestion

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<sup>2</sup> Four Corners Study. Final Report. OCTA, SCAG, Four Corners Transportation Group. June 2000.

- Future travel patterns and congestion problems are expected to be less dominated by commuter trips as regional growth leads to other types of trip-making during peak periods

## Improvement Strategies

The Four Corners participants, including Brea, recognize that both physical infrastructure improvements and “soft” traffic management strategies will be required to address regional traffic concerns. Recommendations contained in the June, 2000 Four Corners study that would affect Brea directly include:

- Adding two lanes on SR-57 between SR-60 and SR-22
- Developing a new road from Chino Hills to SR-57 pursuant to results of detailed feasibility and alignment study
- Developing express bus service from the City of Industry station to business and activity centers in Brea
- Studying the potential for ramps to and from R-57 carpool lanes in Brea

Brea will continue to investigate these and other strategies in cooperation with other Four Corners agencies.

## Truck Routes

Brea experiences moderate amounts of truck traffic generated by commercial and light industrial uses and the Olinda-Alpha Landfill. Truck traffic may increase in future years in support of new businesses. The amount of truck traffic associated with the Olinda-Alpha Landfill is related to daily tonnage limits that are regulated by an agreement between Orange County and the City of Brea. Should the life of the landfill be extended, the City does not anticipate an increase in daily tonnage limits, given agreement restrictions. However, truck traffic will continue through the extended landfill operational period.

To minimize truck traffic impacts on noise-sensitive uses, the City has developed a truck route plan that directs truck traffic to Arterials.



*A major source of truck traffic in Brea is the Olinda-Alpha Landfill located at the terminus of Valencia Avenue. The landfill can be seen in the photo on the right side, just above the houses.*

## Pedestrian Circulation System

Walkability, access, and connections are necessary components of a circulation system that easily and specifically accommodates pedestrians. Walkability includes wide sidewalks, safe street crossings, features that encourage cautious driving, and a pleasant and safe walking environment. Walkways, mid-block crossings, pathways, and pedestrian short-cuts allow people to get from one destination point to another with ease and quickly. Dedicated pedestrian paths can provide access between residential and retail areas, especially if streets are not feasible. Pedestrian connections should be provided primarily to and from intense commercial activity centers such as Downtown Brea and transit stops. Handicapped access strategies should be incorporated into all street and pathway plans.

Residential development tracts designed inward or isolated from adjoining neighborhoods create obstacles to pedestrian movement due to walls, limited access ways, and long walking distances. Residential tracts should have pathways to commercial centers, parks, schools, and transit stops. Activities of daily life must be within walking distance so that people will opt to walk rather than use the automobile for short trips.

The Community Resources Chapter contains an entire section on trails, including urban trails and sidewalks. Chapter Six, Public Safety, contains several goals and policies related to pedestrian safety.



*This pedestrian access point, located at the southeast corner of Birch Street and Poplar Avenue, allows residents of the adjoining residential development to easily walk onto Birch Street without having to travel all the way around to the automobile street entrances. Birch Street leads pedestrians easily to Downtown Brea, the Civic and Cultural Center, and the Brea Mall.*

## Public Transportation System

Promoting the use of alternative transportation modes such as transit, bicycling, and walking produces a number of community benefits, including reduced traffic, reduced need for costly roadway improvement projects, improved air quality, and a healthier population. Transit in particular reduces vehicles on freeways and arterial roadways and offers mobility for those who cannot or do not wish to drive.

Public bus service is provided by OCTA. An established network of bus routes provides access to employment centers, shopping, and recreational areas within the City. Transit routes link Brea to other communities such as La Habra, Fullerton, Orange, Santa Ana, Costa Mesa, Tustin, Anaheim, Garden Grove, Irvine, and Newport Beach. OCTA periodically updates its countywide Bus Service Implementation Program to respond to necessary changes to service levels and route configurations.

Paratransit services are provided by ACCESS, OCTA's shared-ride service for people who are unable to use the regular, fixed-route bus service because of functional limitations caused by a disability. The City operates the Brea Shuttle Express, a curb-to-curb transportation shuttle. Local mobility could be enhanced by a Brea-based fixed-route service that in particular serves the schools and major retail areas.

OCTA also oversees commuter rail planning and operations. Brea supports efforts of OCTA to extend light rail or other commuter rail services to North Orange County, provided any such program maintains the quality of life in the City.

## Accommodating Bicyclists

In Brea and throughout Orange County, where rain falls less than 30 days a year and temperatures are generally moderate, the climate is perfect for bicycling. People can easily cycle to work or school, provided safe routes are available. Allowing bicycles on buses or providing secure bicycle parking facilities can further encourage bicycling for longer trips. Cycling is also a major recreational activity, both on mountain tracks and along the roads. The numerous backbone and single-track trails for the advance and beginning mountain bikers in Chino Hills Park are purely for fun and exercise. For the road cyclist, Rose Drive leads cyclists to a paved pathway in Yorba Linda that guides them to the Santa Ana River Bike Trail, a great paved bike path that follows the Santa Ana River to the Pacific Ocean.

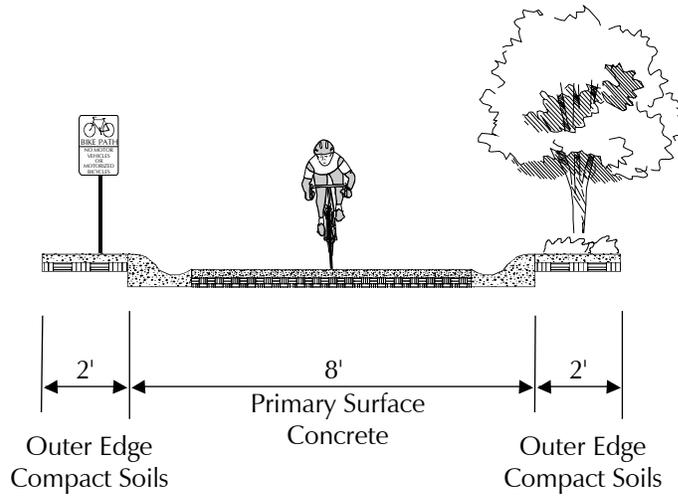
Bikeways, like roadways, come in several forms. Brea has adopted three bikeway standards that parallel those presented in OCTA's *Bikeways Strategic Plan*. Descriptions of these classifications are presented in Table CD-5 and illustrated in Figure CD-9.



*Bikeways can provide access to recreational areas, such as this bike path entrance at Rose Drive. This particular bike path winds past Carbon Canyon Dam and straight into Carbon Canyon Regional Park. The Park has immediate access into Chino Hills State Park where various off-road bikeways for mountain biking can be found.*

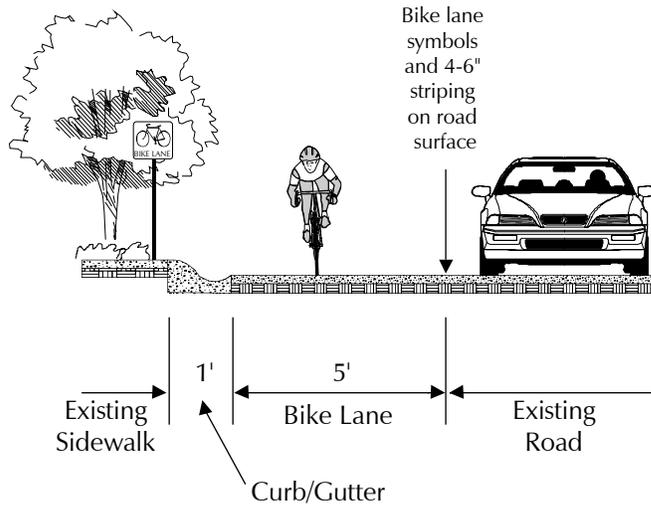
### Class I (Bike Path)

Wider lanes recommended for high bike volumes or high levels of mixed use.

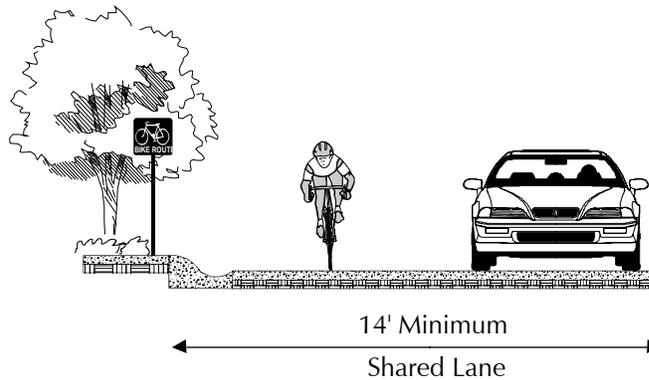


### Class II (Bike Lane)

5' total width where curb occurs. Wider bike lane recommended for high bike volumes or if adjacent to on-street parking.



### Class III (Bike Route)



**Table CD-5  
Bicycle Network Classifications**

Type	Class	Description
Bike Paths	I	Bike paths are paved facilities designated for bicycle use that are physically separated from roadways by space or a physical barrier, and are referred to as Class I bike paths.
Bike Lanes	II	Bike lanes are lanes on the outside edge of roadways reserved for the exclusive use of bicycles, and designated with special signing and pavement markings. Bike lanes are referred to as Class II bike facilities.
Bike Routes	III	Bike routes are roadways recommended for bicycle use and often connect to bike lands and bike paths. Routes are designated with signs only and may not include additional pavement width. Bike routes are referred to as Class III facilities.
Off Road Paths	N/A	Off road bike paths are typically fire access roads or dirt trails that provide access into wilderness areas.

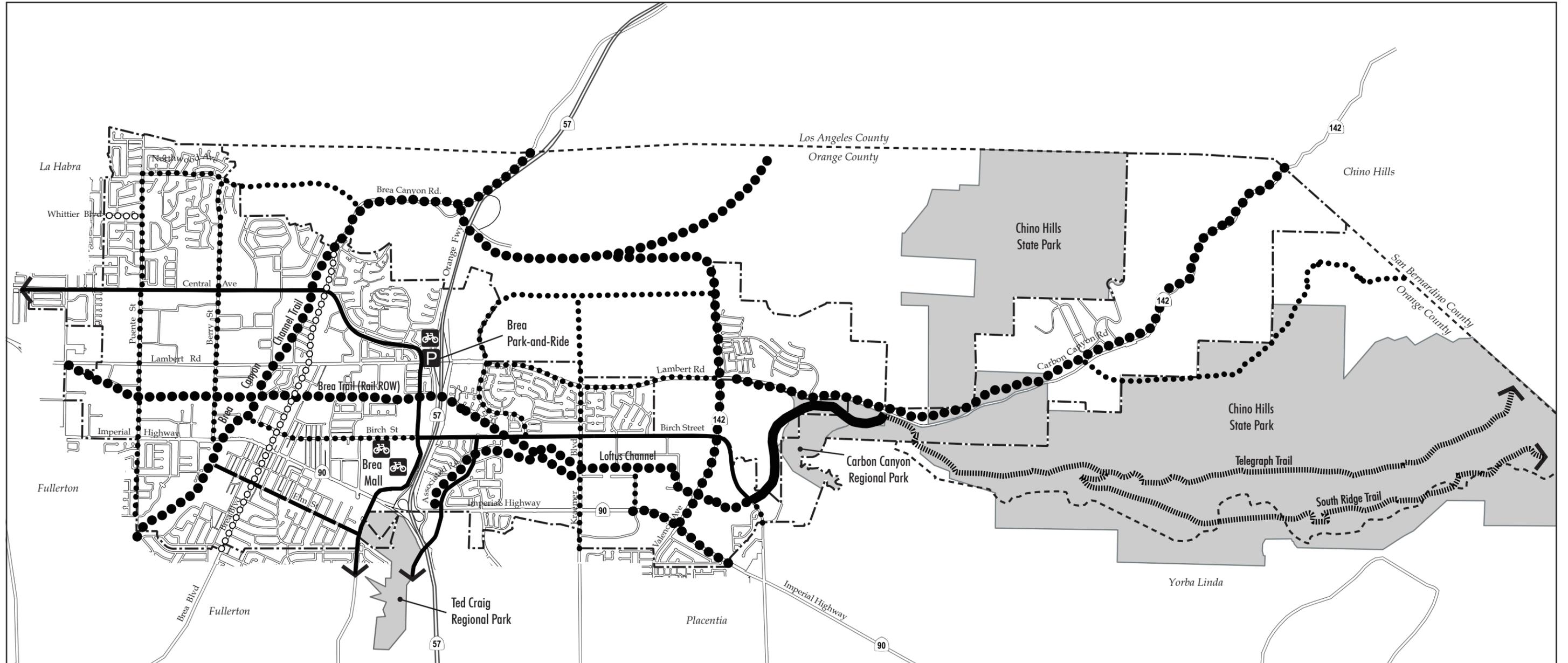
Source: Orange County Transit Authority Bikeways Strategic Plan, 2001



*Bike lanes or Class II bike facilities provide visual identifications of the lane on the street and with signage, as shown here on State College Boulevard just south of Avocado Road.*

**Bikeway Plan**

Figure CD-10 illustrates the Brea bikeway plan, designed to allow access to primary community centers, to connect Breans via bicycles to surrounding communities, and to provide people with an excellent source of recreation and fun. Key routes include Class II facilities along Central Avenue, State College Boulevard, Birch Street, Associated Road, Kraemer Boulevard, and Rose Drive. These routes link downtown Brea and several employment centers and residential neighborhoods.



Source: City of Brea, 1993 and OCTA, 2002.

**Legend**

- City Boundary
- - - Sphere of Influence

**Bike Ways**

- Bike Path (Class I)
- Bike Lane (Class II)
- Bike Route (Class III)
- Off Road (Unpaved)

**Bike Ways (Proposed)**

- Bike Path (Class I)
- Bike Lane (Class II)
- Bike Route (Class III)

- Park and Ride
- Bike Parking Facility

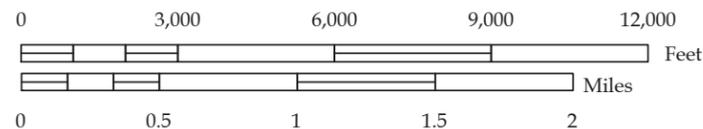
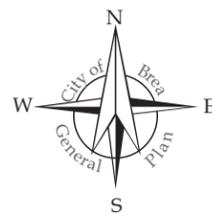


Figure CD-10  
Bike Plan



Priority projects include developing bike lanes and paths along Lambert Road, Brea Canyon Channel, the Union Pacific rail trail, Berry and Puente Streets, and Carbon Canyon Road.

### Linkages to Other Travel Modes

Providing a bicycle-to-transit link is a critical component to encouraging daily bicycle use. By linking bicycles with mass transit such as the train and bus, barriers between lengthy trips, personal security, and poor weather conditions are eliminated. In addition, bicycling to transit reduces taxpayer and parking costs, air pollution, energy consumption and traffic congestion.



*Bike racks as shown here at the Brea Park-and-Ride (near the intersection of Lambert Road and State Route 57) provide great opportunities to link bicycling to other modes of travel, such as carpooling/van pooling and transit.*

There are four main components of bicycle-transit integration:

- Allowing bicycles on transit
- Offering bicycle parking at transit locations
- Improving bikeways to transit
- Encouraging usage of bicycle and transit programs

## Parking

Parking typically is considered a separate issue from vehicle circulation. However, the presence of on-street parking has a direct effect on roadway capacity. In addition, off-street parking deficiencies can cause vehicles to re-circulate on public streets, which also increases traffic volumes and congestion by reducing capacity for through traffic.

The City's zoning ordinance includes parking requirements to ensure that adequate number of spaces are provided on-site for most uses, as well as minimum stall dimensions that are consistent with current standards for other jurisdictions. These regulations apply to all new developments and may be applied to existing uses that are modified or expanded.

## Goals and Policies

The following goals and policies ensure that Brea maintains a safe and efficient transportation network, and that a variety of mobility choices are available throughout the community.

### Regional Transportation Facilities

Transportation in Brea is directly related to an overall transportation network for the four-county area. Roadway facilities in Brea accommodate regional traffic resulting from congestion on State Route 57 and limited access between San Bernardino and Riverside counties to Orange and Los Angeles counties. Planning for the needs of the community necessarily includes recognition of the related transportation needs and planning efforts of the surrounding communities, County, and region. With that recognition is the need for the City to actively monitor transportation planning in the surrounding area and strongly encourage regional transportation improvements.

<b>Goal CD-10:</b>	<b>Maintain an effective regional transportation network.</b>
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Policy CD-10.1	Work continually with Caltrans to improve access to and from State Route 57.
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Policy CD-10.2	Support efforts to establish rail travel connections with a regional network.
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- Policy CD-10.3 Cooperate with surrounding jurisdictions to ensure the efficient operation of the arterial network system.
- Policy CD-10.4 Work with Caltrans, the Orange County Transportation Authority, and surrounding jurisdictions to provide adequate capacity on regional routes for through traffic and to minimize cut-through traffic on the local street system.
- Policy CD-10.5 Work with Orange County Transportation Authority to ensure that the County Master Plan of Arterial Highways is consistent with the City’s Master Plan of Roadways.
- Policy CD-10.6 Recognize that Carbon Canyon Road will continue to serve high volumes of regional traffic despite its designation as a Modified Commuter. Thus, examine design solution alternatives that can improve the safety and efficiency of Carbon Canyon Road.
- Policy CD-10.7 Continue to work with the Four Corners Group to explore regional solutions to the four-county area.

**Local Circulation System**

A well-designed local roadway system is needed to provide safe and convenient access to activities in Brea. The local roadway serves the community’s primary need for mobility and includes a hierarchy of city streets to meet that need.

**Goal CD-11 Provide a safe and efficient circulation system that meets the needs of the community.**

- Policy CD-11.1 Maintain a circulation system that is based upon and is in balance with the Land Use Element of the General Plan.
- Policy CD-11.2 Establish Level of Service goals for designated City streets, and ensure that new development maintains these service levels.

- Policy CD-11.3 Plan neighborhood streets, pedestrian walks, and bicycle paths as a system of fully connected routes throughout the City.
- Policy CD-11.4 Protect residential streets from arterial street traffic.
- Policy CD-11.5 Use traffic calming measures in residential neighborhoods where warranted and appropriate to enhance safety for pedestrians.
- Policy CD-11.6 Utilize creative methods to reduce congestion and improve circulation.
- Policy CD-11.7 Maintain the existing width of streets and roads that serve Olinda Village.
- Policy CD-11.9 Consider establishing landscaped center medians on arterial streets such as Imperial Highway, Birch Street, and South Brea Boulevard.
- Policy CD-11.10 Work with the Brea Olinda Unified School District to establish safe routes to all schools and to facilitate better circulation surrounding schools in the A.M. and P.M. peak traffic periods.
- Policy CD-11.11 Examine alternative methods such as traffic calming, landscaping, provision of bike/transit lanes to slow traffic, improve street capacity, and increase safety.

**Public  
Transportation  
System**

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To maximize use of the existing public transportation facilities and services, there is a need to increase the availability and use of public transit and non-vehicular methods of travel.

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**Goal CD-12 Promote and support an efficient public transportation system.**

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- Policy CD-12.1 Support transit providers such as the Orange County Transportation Authority in granting additional service routes within the City as needed.

- Policy CD-12.2 Promote and market the regional transportation network to encourage transit use.
- Policy CD-12.3 Study the transportation need of seniors. Plan for transit that suits the daily needs of Brea residents.
- Policy CD-12.4 Implement local transit or paratransit service to provide efficient connections from residential neighborhoods to and among urban centers and Downtown.
- Policy CD-12.5 Require new developments to incorporate transit-oriented design features, as appropriate.
- Policy CD-12.6 Balance accommodations for automobiles, transit, bicycles, and pedestrians in the design of new streets and streetscape improvements.

## Pedestrian and Bicycle Facilities

Non-vehicular methods of travel, such as walking or bicycling, can also reduce demands on the roadway system where necessary improvements exist to promote those methods. It is important that facilities are in place to make walking and biking easy, comfortable, convenient, and safe. Together, public transit and non-vehicular modes can provide healthy and environmentally conscience modes of travel than to the automobile.

**Goal CD-13 Provide for an extensive, integrated, and safe bicycle, hiking, and pedestrian network throughout the community, and make Brea a pedestrian-friendly community.**

- Policy CD-13.1 Develop and maintain a comprehensive and integrated system of bikeways that promotes bicycling riding for commuting and recreation.
- Policy CD-13.2 Provide for safe and convenient pedestrian connections to and from Downtown, other commercial districts, neighborhoods, and major activity centers within the City.

Policy CD-13.3 Establish the Birch Street corridor between Downtown Brea and the Civic and Cultural Center/Brea Mall as a pedestrian and bicycle-friendly travel way.

Policy CD-13.4 Require new developments to provide for the use of alternative modes of transit via internal trails or travel ways – public or private – for pedestrians and vehicles other than cars. New developments shall include such features as well-designed sidewalks and parkways, bike lanes and paths, and dedicated bus turn-outs.

**Implementation  
Guide**



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See Section II of the Implementation Guide for action programs.

# INFRASTRUCTURE

The City's water, sewer, and storm drain systems represent the hidden support network for development in Brea. These systems must be maintained to ensure that existing residents and business people can reliably turn on a tap, run the washing machine, or use water for industrial processes knowing that the infrastructure systems will not fail. As infrastructure ages, isolated failures represent a real potential. To preserve high service levels in Brea, water and sewer lines will need to be expanded and replaced to maximize efficiency and increase capacity. For new development, the infrastructure must be able to support new needs and demands without burdening current users.

## Context

The City maintains the water distribution, sewage collection, and local storm drain systems in Brea. Water supplies are purchased from wholesale providers (refer to the discussion in the Community Resources Chapter), and the City is responsible for storage and distribution. Sewage collected in laterals and City trunk lines flows into regional lines maintained by the Orange County Sanitation District. With regard to flood control, City storm drains direct runoff into major channels and other facilities under the control of the Orange County Flood Control District.

### Water Distribution System

Because the groundwater quality of the La Habra basin underlying Brea is poor, the City purchases water supplies from two agencies: the Metropolitan Water District of Southern California (MWD) and the California Domestic Water Company (CDWC). The one groundwater well owned by the City is used strictly for irrigation purposes.

Brea's distribution facilities include supply connections to MWD and CDWC, as well as emergency connections with neighboring agencies. Water enters the distribution systems from transmission mains through various pressure reducing stations and the Berry Street Pumping Station. The water is stored in reservoirs in the hills and other functionally strategic locations, and the City also has storage capacity in the Orange County Reservoir, which is jointly owned by Brea, La Habra, and Fullerton.



*Inside one of Brea's water reservoirs during its construction.*

In addition to water demands placed on the system for domestic and commercial use and irrigation, the water system must be capable of providing adequate pressures for fire-fighting purposes. Since fires can occur at any time, the water system must be ready at all times to provide the required flow.

In 2002, the City prepared a Water System Master Plan that analyzed the domestic water system to determine what improvements would be required over the long term to best serve established development, as well as requirements for new facilities based on the General Plan land use plan. Table CD-6 indicates the combined summary of total demand by land use within the planning area.

Over the long term, projected water demands are expected to increase from a 2002 baseline use of about 9.8 million gallons per day to 18.1 million gallons per day at build-out, an increase of 85 percent. To meet this anticipated demand, the City will be required to increase the capacity of the MWD OC-29 connection by five cubic feet per second. Also, low water pressure at higher elevations in Olinda Village and lack of sufficient pumping capacity at the Berry Street Booster Pumping Station will require upgrades to the pressure system.

Figure CD-11 identifies the required water system improvements within the planning area that will be pursued over the long term, with priority given to meeting the needs of established uses.

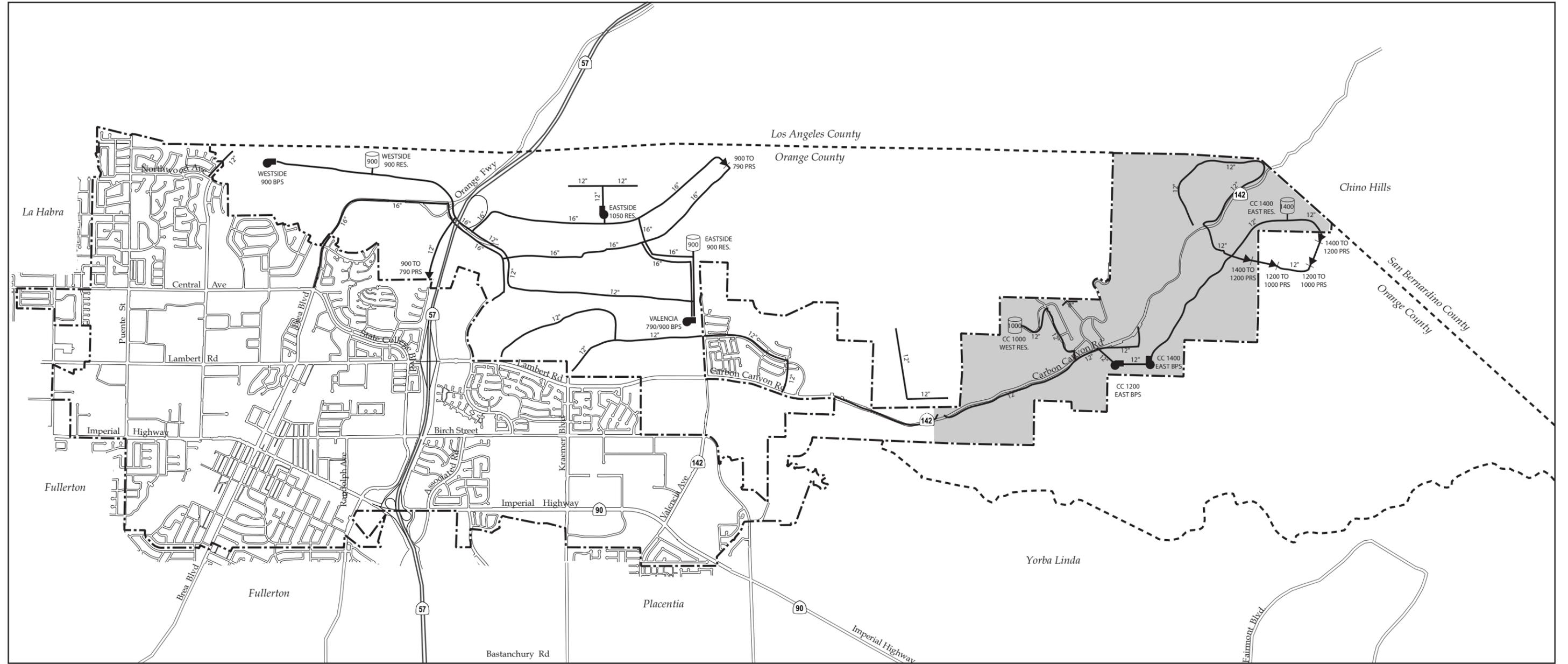
**Table CD-6  
Total Water Demand by Land Use**

<b>Zoning Categories</b>	<b>Area (acres)</b>	<b>Water Demand Factor (gpd/ac)</b>	<b>Average Day Water Demand (gpd)</b>
Hillside Residential	246.5	2,000	493,000
Single Family - Hillside	66.9	2,000	133,870
Single Family (7,200 sq. ft./du)	1,676.6	1,500	2,514,900
Single Family (5,000 sq. ft./du)	10.9	1,500	2,514,900
Multiple Family (4,500 sq. ft./du)	301.2	2,000	602,384
Multiple Family (1,750 sq. ft./du)	169.4	2,800	474,313
Multiple Family (Incentive)	4.1	2,800	11,351
Administrative and Professional	61.8	2,000	123,612
Neighborhood Commercial	11.4	2,000	22,751
Major Shopping Center	249.5	3,000	748,384
General Commercial	318.2	1,400	445,480
Industrial Commercial	67.8	3,000	203,359
Light Industrial	616.8	3,000	1,850,357
General Industrial	496.2	2,000	992,400
Planned Community	311.2	2,300	715,661
<b>Large Water Users Adjustment</b>			504,347
AERA	332.8	n/a	520,980
Tonner	610.6	n/a	198,360
Tonner Hills	808.0	n/a	1,114,920
Tomlinson	39.5	n/a	146,977
Olinda Ranch	375.6	n/a	782,738
BHSP	153.7	n/a	521,792
PBR	190.6	n/a	625,368
Canyon Crest	377.0	n/a	486,780
SOI	5,924	n/a	4,702,122
<b>Total</b>			<b>18,127,167</b>

Abbreviations: gpd=gallons per day

Source: Brea Water Master Plan Update, 2002

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Source: City of Brea Water Master Plan Update (Draft), 2002.

**Legend**

- City Boundary
- Sphere of Influence
- 12" Improvements
- Pressure Reducing Station
- Booster Pumping Station
- Reservoir (HWL)
- Carbon Canyon Service Area

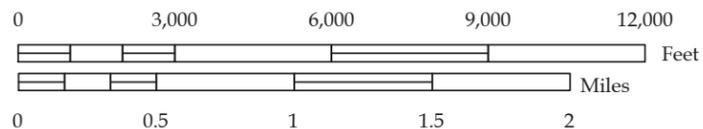
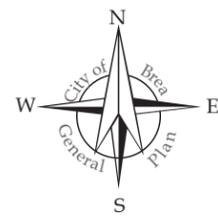


Figure CD-11

Water System Improvements



## Sewage Collection System

The local sewage collection system provides wastewater collection services to all areas within the City's corporate boundaries, portions of unincorporated Orange County, and a small portion of Placentia. The gravity system consists of over 100 miles of pipe, the majority of which consist of vitrified clay pipe ranging in size from 8 to 27 inches in diameter. The service area consists of ten major sewer drainage areas that flow into City trunk sewers, which in turn outlet to Orange County Sanitation District (OCSD) facilities. OCSD collects sewage from cities throughout Orange County and treats it at regional facilities.

In 2001, the City conducted an extensive analysis of the local sewer system to identify deficiencies in the existing system and to anticipate long-term needs as the City accommodates new residential and other uses. A sewer system model projected wastewater flows associated with build-out consistent with General Plan land use policy. Table CD-7 indicates long-term conditions within each of the ten drainage areas.

Ensuring adequate sewer service throughout the entire planning area will require replacement facilities and the extension of new infrastructure to serve new development. Figure CD-12 identifies the required sewer system improvements.

The Sewer Master Plan establishes the following priorities for recommended master plan capital improvements:

1. Facilities identified as deficient under existing peak-flow conditions shall be given high priority. For facilities known to require frequent maintenance, those facilities shall be considered of highest priority.
2. Facilities identified as deficient under ultimate conditions and dependent on future development shall be given a moderate priority.
3. Facilities affecting the greatest number of customers or which would cause the most damage in the event of failure are given higher priority rankings than those that do not. This means that downstream facilities with larger tributary flows receive higher priority than upstream facilities.

**Table CD-7  
Ultimate Wastewater Flows by Drainage Region**

<b>Number</b>	<b>Region Name</b>	<b>Average Dry Weather Flow (mgd)</b>	<b>Peak Dry Weather Flow (mgd)</b>	<b>Inflow/ Infiltration (mgd)</b>	<b>Peak Wet Weather Flow (mgd)</b>
1	Imperial	1.154	2.027	0.507	2.534
2	Fullerton	1.851	3.130	0.783	3.913
3	Brea	0.233	0.465	0.116	0.581
4	Laurel	0.219	0.439	0.110	0.549
5	Rolling Hills	1.268	2.211	0.553	2.764
6	Associated	0.776	1.407	0.352	1.759
7	Cypress	0.207	0.418	0.105	0.523
8	Kraemer	0.995	1.768	0.442	2.210
9	Valencia	0.445	0.843	0.211	1.054
10	Carbon Canyon	0.934	1.668	0.417	2.085
<b>City Total</b>		<b>8.081</b>			

Abbreviations: mgd=million gallons per day

Source: Sewer System Master Plan, 2001

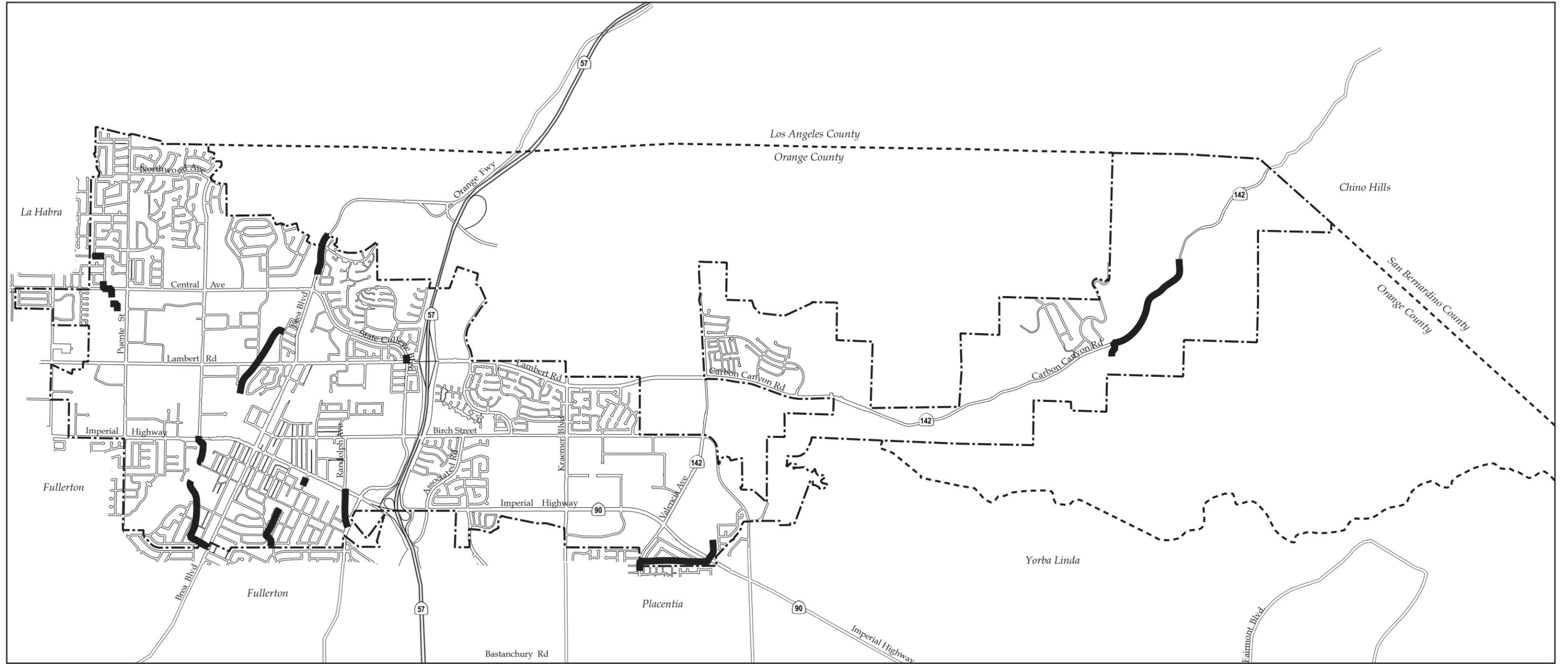
## Storm Drains

Storm water is rainwater plus anything the rain carries along with it. In urban areas, rain that falls on roofs or collects on paved areas is carried away through a system of gutters, pipes, and culverts. Storm water runoff flows directly into the City's storm drain system via street gutters and other inlets, and this flow in turn discharges into County flood control channels which ultimately drain to the Pacific Ocean. The Orange County Flood Control District (OCFCD) maintains the regional storm drain system.

From a planning standpoint, the two important considerations to focus on regarding storm drains are: 1) ensuring adequate capacity to collect and carry storm water and thereby avoid flooding, and 2) working to reduce pollutant loads in storm water as part of regional efforts to improve water quality in surface waters.

### Storm Drainage

In the hills above Brea, Brea Creek, Fullerton Creek (via the Loftus Diversion Channel), and Carbon Canyon Creek are the three primary drainage courses. All three are part of the greater watershed of the San Gabriel River, which outlets into the Pacific Ocean at Seal Beach. In Carbon Canyon, drainages and intermittent streams flow into the Carbon Canyon Dam drainage area. Stream flows below the dam are directed to the San Gabriel River via Fullerton Creek or diverted to the Santa Ana River.



Source: City of Brea Sewer Master Plan Update, May 2001.

- Legend**
- City Boundary
  - ..... Sphere of Influence
  - Sewer System Improvements

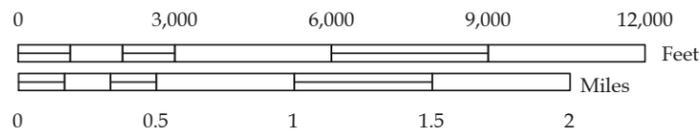
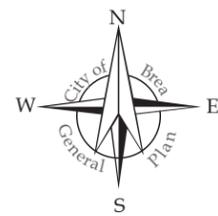


Figure CD-12  
 Sewer System Improvements





*Brea Canyon Channel adjacent to Arovista Park is a reinforced concrete trapezoidal channel having a soft-bottom covered with vegetation. Brea Canyon Channel flows into Brea Dam located in the City of Fullerton.*

Within the urbanized areas of Brea, surface flows are contained within concrete channels, including the Brea Canyon channel, Memory Gardens channel, Imperial channel, and Loftus Diversion channel. These channels, maintained by the OCFCD, provide a high degree of protection from excessive storm water flows. (Storm water flooding is addressed as a public safety issue in the Public Safety Chapter.)

### **Storm Pollutants**

When it rains, trash, litter, silt, automotive chemicals (oils and grease, antifreeze, and fine dust from tire wear), animal wastes, and many other contaminants are washed into the storm drain system. Since storm drains are designed to carry only storm water, these drains typically are not equipped with filters or cleaning systems and consequently, deliver polluted urban runoff directly into local flood control channels and the ocean. Many of the pollutants found in this runoff are toxic to fish, marine mammals, and other aquatic life.

The Federal Water Pollution Control Act prohibits the discharge of any pollutant to navigable waters from a point source unless the discharge is authorized by a National Pollutant Discharge Elimination System (NPDES) permit. In 1987, the passage of the Water Quality Act established NPDES permit requirements for discharges of storm water. The NPDES permit program controls water pollution by regulating point sources that discharge pollutants into waters of the United States. Since 1990, operators of storm water systems such as Orange County's have been required to develop a storm water management program designed to prevent harmful pollutants from being washed away by storm water runoff and discharged into local water bodies. Brea

participates in the NPDES permit program via a partnership consisting of the County, all cities within Orange County, and the County Flood Control District.

## Goals and Policies

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<b>Goal CD-14</b>	<b>Provide sufficient levels of water, sewer, and storm drain service throughout the community.</b>
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Policy CD-14.1      Coordinate the demands of new development with the capacity of water and sewer systems.

Policy CD-14.2      Implement the City’s water and sewer master plans to correct known deficiencies.

Policy CD-14.3      Require that new developments fund fair-share costs associated with City provision of water, sewer, and storm drain service.

Policy CD-14.4      Work with developers to ensure that adequate funding and support for required infrastructure is provided or ensured via bonds.

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<b>Goal CD-15</b>	<b>Minimize damage to the wastewater collection and treatment systems by preventing discharge of materials that are toxic or which would obstruct flows.</b>
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Policy CD-15.1:      Pursue treatment and disposal methods which, to the maximum extent feasible, provide for further beneficial use of wastewater and allow beneficial uses of land or water receiving the effluent.

Policy CD-15.2:      Continue to investigate and carry out cost-effective methods for reducing storm water flows into the wastewater system.

Policy CD-15.3:      Update and enforce Brea’s standards for the quality of wastewater discharged to the system.

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## High Technology and Infrastructure

Just like traditional infrastructure such as roads, railroads, and telephone networks, digital and communications infrastructure provides a vital framework for the community. Communications infrastructure links the community with the nation and the world. Expanding access and capabilities of technology assists Brea in meeting its goals of livability, economic growth, and provision of high-quality services to residents. Brea can work to harness new technology and telecommunications systems by including all residents and business in the use of high-speed, high-capacity, digital communication systems.

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**Goal CD-16: Assist public and private organizations to deliver digital and communication services to neighborhoods.**

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Policy CD-16.1: Help institutions provide distance learning opportunities, services, and commercial products directly to residents.

Policy CD-16.2: Explore the feasibility of allowing residents and businesses to comment on City issues, pay utility bills, apply for building permits, or register for recreational activities and classes on line.

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Implementation  
Guide 

See Section III of the Implementation Guide for action programs.

## URBAN DESIGN

### Vision

Imagine Brea with walkable, tree-lined streets and linked trails and bicycle paths. Imagine high-quality residential neighborhoods with shops, services, schools, and parks within close walking distance. Image a lively downtown and Brea Boulevard with outdoor dining, shops, and homes mixed together and places to linger and meet one another. Imagine traveling along Lambert Road, Imperial Highway, or other main arterials and instantly recognizing that you are in Brea.



*Awnings, sidewalk, street trees, and public art invite people outside to walk, socialize, and visit the interesting places Brea offers.*

The image of Brea is composed of the perception and experience of those living, working, playing, or traveling throughout the community. Urban design addresses the conservation and enhancement of the visual quality of Brea. Urban design applies to Brea's visual and physical setting and transcends into other planning areas: transportation, land use, housing, economic development, and the natural environment. Addressing urban design in Brea is especially significant given the City's unique mixed-use districts, historical and cultural resources, and rugged hillside and open space areas. This Urban Design section of the Community Development Chapter provides guidance regarding the visual appearance, character, and quality of life within the community. Principles and policies are designed to improve the community-wide appearance and character of Brea, as well as to address urban design in specific areas such as mixed-use districts and hillside areas.

## Context

The history of Brea lies in its hillsides: in the oil industry, production camps, and early settlements that predate the City's founding. Today, the rugged hills provide a scenic backdrop to the community and insulate the community geographically and psychologically from Los Angeles and San Bernardino counties. Breans remain concerned with preserving the natural environment and open space areas. Throughout the General Plan preparation process, residents identified these areas as a valuable visual resource that represent a defining component of community identity.



*Incorporating landscaping and decorative features assists in making outdoor plazas inviting.*

Brea is also in a unique position of benefiting from a vibrant, pedestrian-oriented downtown that incorporates and blends residential, commercial, retail, and entertainment uses. Downtown Brea represents a source of community pride and an important destination. Mixed-use land use designations will provide pedestrian-oriented focused areas that offer opportunities for people to live, work, shop, and play. The success of mixed use and pedestrian-oriented areas within the Downtown will be expanded into other suitable areas – along Birch Street across from the Civic and Cultural Center and along South Brea Boulevard – that have ready access to transit and community focal points.

Maintaining and improving the City's residential areas is also a key component of urban design. While the majority of the community's housing stock is in good to excellent condition, many residential neighborhoods would benefit from enhanced pedestrian and transit connections and integration of local shopping and services to meet the needs of residents.

Community members have identified several issues relating to the qualities that define Brea:

- Breans recognize the value of the community's "old town" feel and community history. This can be enhanced through the use of markers, landmarks, buildings, and signs to designate historic districts.
- Many areas of Brea are not very walkable and are visually non-descriptive, with excessive use of driveways and bland, non-cohesive storefronts. The use of design standards, interesting pavement designs, trees, benches, mixed-use, and restaurant seating on sidewalks would improve the look and walkability of these areas.
- Brea would benefit from more pocket parks, attractive public spaces, attractive development, on-street parking, historic landmarks, and signs to invoke a sense of identity and connection to the past.
- All sides of development (side, back, and front) should look attractive and be accessible.
- Brea needs a stronger sense of community identity. Capitalize on Brea's unique history and provide more visual definition at City entrances.
- The look and success of Downtown is a great asset to the community. Active pedestrian and social activities are largely achieved through design and mixed-use development. We should extend the look and success of Downtown Brea to other areas such as South Brea Boulevard.
- Green space adds tremendous value and enjoyment to residents. Since a limited amount of open space remains in Brea, this can be achieved through more landscaping and trees.

- Pedestrian-friendly streets, outdoor dining, street furniture, and sidewalks make a community more attractive and also make people want to get out of their cars and walk to different places.
- A limited number of historic structures remain in Brea; therefore, we should identify structures that are important to be saved. Many valuable historic homes and buildings can be refurbished or improved.
- Breans enjoy the community's varied and extensive outdoor art collection. We should employ more creative methods to incorporate art work to blend with new and existing buildings.
- Brea's hillsides are an important community asset. They should be developed with the least amount of impact possible, while still maintaining the ridgelines and providing pathways and viewing trails for the community.

The urban design plan presents broad citywide goals, as well as specific design policies tailored for distinct neighborhoods in the community:

- Downtown Brea
- Historic Brea
- Northwest Brea Neighborhoods
- East Brea Neighborhoods
- West Brea Industry and Commerce Center
- Unocal Research Center and Environs
- Hillside Districts
- Carbon Canyon/Olinda Village
- Olinda Alpha Landfill and Environs

The citywide urban design objectives and design guidelines for districts and neighborhoods have been crafted around the following five overarching goals:

**Goal CD-17 Promote and maintain a distinct community identity and sense of place that include the presence of identifiable districts and neighborhoods.**

**Goal CD-18 Emphasize the use of public spaces and pedestrian and transit use throughout the community.**

**Goal CD-19 Encourage active and inviting street environments that include a variety of uses within Commercial and Mixed-Use areas.**

**Goal CD-20 Encourage site planning within Commercial and Mixed-Use districts that functionally and visually integrates on-site facilities and uses, including buildings, services, access, and parking.**

**Goal CD-21 Integrate residential development with its built and natural surroundings, and in particular, encourage a strong relationship between dwellings and the street.**

**Goal CD-22 Encourage the use of native plant palettes in the creation of landscaping plans used to establish a sense of place in neighborhood identification efforts.**

**Implementation  
Guide** 

See Section III of the Implementation Guide for action programs.

## Urban Design Plan

At a citywide level, programs and improvements will be pursued that enhance the visual character of Brea and create a distinct identity.



*Homes that create a direct relationship to the street, without the looming presence of garages, reinforce the feelings of neighborhood and community.*

### Citywide Concept: Places

#### **Overall Concept**

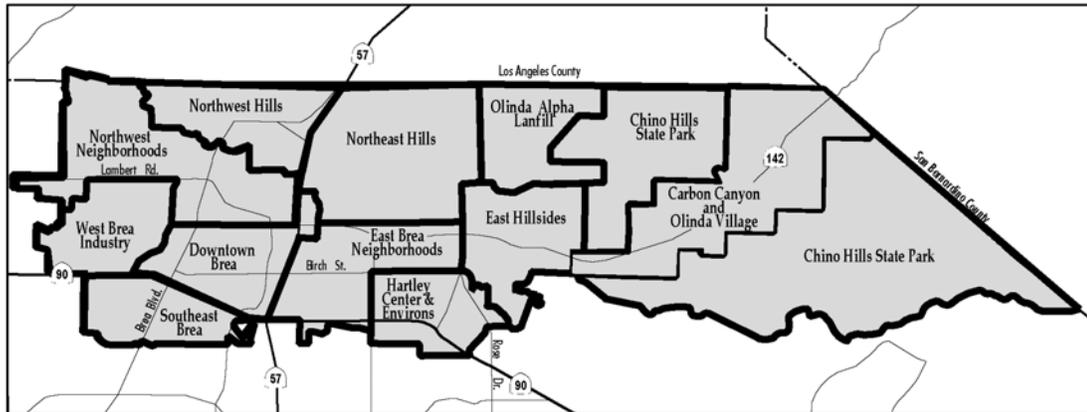
The urban design plan recognizes the multitude of places that distinguish Brea. These places vary in scale from districts to landmarks, but each component further differentiates the community and contributes to the understanding, use, and enjoyment of the City.

#### **Concept Components: Brea Places**

**Districts** describe rather broad areas of the community, each featuring a relatively consistent character and sometimes a highly unique identity (Figure CD-13). Districts should complement one another, and while some are distinguished by a diversity of uses (Downtown Brea, Historic Brea), others are identified by a single dominant land use. For instance, Northwest Brea Neighborhoods consist of mostly residential areas, with limited commercial use. Private development and public improvements should enhance the identity, character, and function of each district.

**Neighborhoods** are relatively defined areas with a primarily residential function, often focused around a park or school. Neighborhoods should be well connected with their surroundings, and integration of a limited amount of local shopping and services conveniently serving day-to-day needs is often justified. Of particular concern to Brea's residents, the quality of existing neighborhoods should be preserved, and new development should be sensitive to residential surroundings.

Figure CD-13. Urban Design District Map



**Activity Nodes** serve as important destinations, the major centers of public life within the community. These nodes require a “critical mass” and should promote compact development patterns, with particular emphasis given to main street commercial, housing over retail, and/or urban village project types. Streets should be treated as highly public places where pedestrians are given priority and there is an abundance of streetscape amenity.

**Focal Intersections** become visually distinct places through special paving, landscape treatment, and/or public art; they serve to orient residents and visitors alike. Some intersections may also function as the focus of activity for surrounding districts and nodes. Increased development intensities surrounding the intersection and buildings with strong corner massing will highlight and support this role.

**Schools and Parks** are traditionally associated with local neighborhoods, and this pattern is appropriate to Brea. A desirable balance between residential land use and open space resources is achieved, and many social and recreational needs are conveniently met. Each neighborhood should have convenient access to a local park, preferably within or directly adjacent its boundaries; however, some outlying neighborhoods are not well served.

**Urban Spaces** include publicly accessible plazas, courtyards, and similar outdoor spaces that form a vital component of a community’s public life. Therefore, these spaces are especially appropriate within identified activity nodes. As well-defined outdoor rooms, they may accommodate public gatherings and celebrations, or serve as retreats from urban activity.

**Landmarks** are highly recognizable buildings, structures, or landscape features. As such, they become powerful orienting devices that help define the City, a district, and/or a neighborhood. For instance, residents readily identify “The Plunge” with Historic Brea. Historically significant buildings and structures frequently achieve status as community landmarks and should be preserved and protected from incompatible development.

**Natural Features** generally refer to the hillside areas that provide a special backdrop to the community. The hillside areas are an indispensable component of the City’s visual character, and include environmentally significant features such as ridgelines and drainage ways that should be protected from intrusion.

Figure CD-14 indicates locations of urban design improvements planned throughout Brea.



*Gateways featuring special signage are an opportunity to reinforce Brea’s unique character and identity.*

## Citywide Concept: Connections

### Overall Concept

The urban design plan identifies opportunities to strengthen the principal connections that unite Brea’s place. These linkages build a system of open spaces comprised of landscape corridors, walkable streets, and trails connecting undeveloped hillside, public parks and schools, and urban open spaces. Brea has identified a particular need to upgrade the visual quality of many of the community’s streets. The City also recognizes the opportunity to build strong connections to new development by extending the existing road network.

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Source: □ RTKL Associated Inc., April 2002 and Cotton/Bridges/Associates, 2002. □

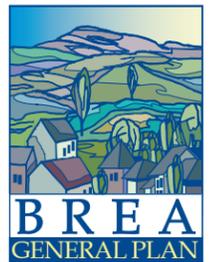
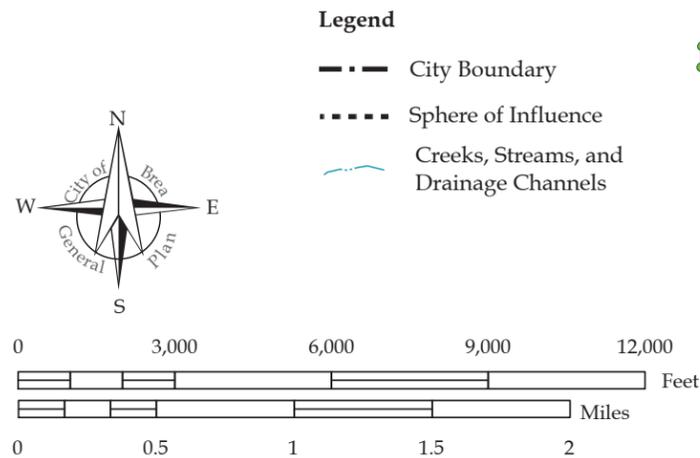


Figure CD-14  
Urban Design Plan Map





*Links between sidewalks and trails will increase access and enjoyment of the hillside and other valued community open spaces.*

### **Concept Components: Brea Connections**

**Gateways** announce arrival, as expressed through special design. City gateways are recommended where major streets cross municipal boundaries and at freeway ramps. District gateways should be considered for especially recognizable districts, most notably Downtown Brea and Historic Brea. Environmental graphics (i.e. signage) that express local history and identity are an especially important element of gateway design.

**Landscape Corridors** are a highly visible component of the cityscape. These significant regional thoroughfares benefit from extensive landscape improvements. In particular, generous medians and parkways enhance the visual quality of these otherwise auto-dominated corridors and function as an extension of the community open space network (Figure CD-13).

**Mixed-Use Boulevards** directly link and/or support Brea's principal activity nodes, requiring the transformation of auto-dominated arterials into vibrant mixed-use places. While these streets will continue to support auto and transit movement, they represent boulevards where pedestrian activity is most desired. Brea Boulevard, Birch Street, and key roadways within newly developing areas will receive this treatment. Public initiatives and private sector development will need to respond accordingly. Buildings should exhibit an exceptionally strong street orientation, and priority should be given to providing considerable streetscape amenities.

**Major Linkages** establish a community-wide network of connections between parks and schools to create an open space system. Typically, these streets are arterials and collectors that should be more accommodating of the pedestrian. Recommended streetscape improvements emphasize street trees

to unify the street's appearance, and amenities such as enhanced transit stops, benches, and pedestrian-scaled light fixtures. As far as feasible, adjacent development should orient to the street and incorporate pedestrian-friendly design features.



*Multi-use trails provide places for pedestrians, joggers, and cyclists.*

***Neighborhood Linkages*** promote local walking connections to shops and services, including neighborhood parks and schools. These streets may also offer vital connections between adjacent neighborhoods; for instance, the existing street network should be extended to discourage isolated developments. Continuous sidewalks are a necessity in both nonresidential and residential areas. In addition, opportunities for traffic calming should be explored within residential neighborhoods.

***Pedestrian Passages*** provide on-site connections between sidewalks, outdoor spaces, building entrances, and parking facilities. They are an especially important component of urban villages and activity nodes, where a well-connected and engaging pedestrian environment is most critical.

***Trails*** complement regional and community open space systems. Ridgelines, drainage ways, abandoned rail lines, urban drainage channels, and freeway rights-of-way provide the opportunity for a continuous network linking hillsides with flatlands. In the Community Resources Chapter, the Trails Plan identifies a network of urban and natural area trails linking all community places.

## District Character

### Downtown Brea Policies for Creating a Sense of Place

- Provide diverse housing, employment and cultural opportunities throughout the Downtown, with an emphasis on compact, mixed-use and pedestrian-oriented development patterns that are appropriate to the core of the City.



*Landscaping enhances private businesses where they interface with the public realm.*

- Accommodate a range of housing densities and residential building types, including non-traditional forms that enliven the Downtown setting, such as small lot single-family residential, housing over retail, lofts, and live-work accommodations.
- Promote the Brea/Birch intersection as the vibrant heart of Downtown and the principal center of public life for the community; this node should function as a highly identifiable shopping and entertainment destination for Brea and neighboring communities.
- Enhance the public character of the Civic and Cultural Center and its surroundings; this area of Downtown should be highly accessible and much more accommodating of people, with an emphasis on transit- and pedestrian-oriented development.

### Policies for Creating Connections

- Build especially prominent visual and physical connections between Downtown and the remainder of the community; key strategies for establishing strong links are district gateways,

landscape corridors, convenient transit, comfortable and attractive transit stops, and walkable streets.

- Create an extensive network of safe and comfortable pedestrian linkages throughout the Downtown, including visually attractive, high-amenity streetscapes, pedestrian paseos and paths, and urban outdoor rooms.
- Strengthen the connection along Birch Street between the heart of Downtown and the Civic and Cultural Center; pedestrian-oriented, mixed-use development, and a high level of streetscape amenity are encouraged.

## Historic Brea



*Bungalow dwellings and other historic structures contribute to Brea's historic identity.*

### **Policies for Creating a Sense of Place**

- Protect the character and enhance the quality of Historic Brea's single-family residential areas; in particular, infill development should be compatible with the scale and appearance of neighboring dwellings, with higher intensity development along South Brea Boulevard serving as a buffer to the established single-family neighborhood behind.
- Support preservation and rehabilitation of the area's single-family residential dwellings, especially those homes that are recognized as historic and/or positively contribute to the character of the neighborhood.
- Facilitate the transition of South Brea Boulevard toward a higher intensity mixture of compatible uses centered on the blocks surrounding City Hall Park; these blocks should provide a focus for the district, as well as local shopping and services.
- Ensure that new development within the district respects and responds to the historic architecture.
- Provide logical transitions between higher intensity development along South Brea Boulevard and nearby single-family neighborhoods; scale, massing, and the location of services should respond sensitively to adjacent residential uses.

### **Policies for Creating Connections**

- Strengthen connections to local parks and services, and enhance the appearance and safety of residential streets. Canopy street trees located in parkways separating sidewalks

from roadways should become an especially important feature of the District's neighborhoods.

- Use traffic calming measures to reduce travel speeds and divert through traffic from local residential streets; these measures are intended to improve neighborhood livability.
- Transform South Brea Boulevard into an active, pedestrian-friendly street that balances auto, transit, and pedestrian mobility. The street should accommodate compact and mixed-use development oriented to the sidewalk.

## Northwest Brea Neighborhoods

### **Policies for Creating a Sense of Place**

- Preserve the character and enhance the quality of the district's residential neighborhoods; new development should be compatible with the surrounding character.
- Promote high standards of maintenance and otherwise improve the appearance of multi-family residential projects. New multi-family housing should transition in scale to adjacent single-family dwellings and minimize the visibility of on-site parking.
- Integrate local shopping and services to meet the basic needs of residents. The design and layout of local shopping centers should respond sensitively to adjacent residential uses.

### **Policies for Creating Connections**

- Upgrade the appearance of visually inferior arterial and collector streets. Street trees and other landscape improvements should establish strong visual and physical linkages with adjoining districts.
- Improve access to local parks, schools, shopping and services, focusing on enhanced pedestrian and transit connections. Streetscape improvements should encourage walking and cycling.
- Use traffic calming measures to reduce travel speeds and divert through traffic from local residential streets. These measures are intended to improve neighborhood livability.

East Brea  
Neighborhoods

**Policies for Creating a Sense of Place**

- Preserve the character and enhance the quality of the district’s residential neighborhoods. New development should be compatible with the surrounding character.
- Explore opportunities for new neighborhood parks and recreational facilities, especially within or near those neighborhoods that are underserved and/or in conjunction with new development projects.



*Residential developments within the East Brea neighborhoods have enhanced the community by incorporating open space amenities.*

**Policies for Creating Connections**

- Establish linkages between adjacent developments, emphasizing an interconnected network of attractive streets, sidewalks and paths. New developments should extend the existing street grid.
- Improve access to local parks, schools, shopping and services, focusing on enhanced pedestrian and transit mobility. Streetscape improvements should encourage walking and cycling.
- Use traffic calming measures to reduce travel speeds and divert through traffic from local residential streets.

West Brea Industry  
and Commerce  
Center

**Policies for Creating a Sense of Place**

- Pursue redevelopment of functionally and visually inferior industrial properties. Contemporary office and industrial campus environments are encouraged.
- Organize buildings around on-site open space areas such as courtyards, quadrangles, and greens. Abundant landscaping should enhance sites and contribute to a campus setting.
- Accommodate commercial shopping and services that meet the day-to-day needs of area employees.

**Policies for Creating Connections**

- Build connections to adjacent districts and uses. Street trees and other landscape improvements should enhance the appearance of arterials and corridors.



*Construction of contemporary office and industrial campus environments is transforming and updating the West Brea Industry and Commerce Center.*

- Improve pedestrian access throughout the district. Continuous sidewalks and on-site paths that connect adjacent uses, buildings, outdoor spaces, and parking are strongly encouraged.
- Pursue trail opportunities within the district. Consistent with the Trails Plan (Community Resources Chapter), rail lines and urban drainage channels can be developed as open space and recreational resources for local workers and community residents alike.

Unocal Research  
Center and  
Environ

**Policies for Creating a Sense of Place**

- Consistent with land use policy, transition toward a mixed-use urban village that encompasses a range of housing types. A highly integrated mix of complementary land uses should provide jobs, housing, and services.
- Require pedestrian-oriented development that emphasizes horizontal and vertical mixing of land uses to contribute to the desired village atmosphere.
- Incorporate well-defined urban spaces that support social interaction and are comfortable to occupy. Pocket parks, plazas, and courtyards are compatible with an urban village lifestyle.
- Make provisions for educational, institutional, and active recreational uses that serve the entire community.

**Policies for Creating Connections**

- Provide convenient pedestrian and transit access throughout the district, including an interconnected network of high-amenity streetscapes, attractive and comfortable transit stops, and multiple paths that connect activities and uses.
- Improve the appearance and pedestrian orientation of arterials and corridors that pass through the district. Street trees and other landscape improvements will be used to significantly upgrade district streets.
- Promote an active street life, in keeping with the desired urban village concept. Building design and orientation should establish a strong relationship with the street.

## The Hillsides **Policies for Creating a Sense of Place**

- Preserve the scenic beauty of Brea’s hillsides, and minimize the visual and environmental impact of development upon sensitive hillside areas.
- Prohibit the construction of dwellings or other structures on the most sensitive hillside areas. In particular, prominent ridgelines, drainage ways, and significant stands of mature vegetation should be left undisturbed.
- Require sensitive grading techniques and other design measures that minimize the visual impact of development and make dwellings unobtrusive.



*The hillside areas remain mostly undeveloped, providing an attractive backdrop to the community.*

### **Policies for Creating Connections**

- Encourage preservation of the most sensitive hillside areas through conservation easements, land acquisitions, and/or other initiatives that contribute to a regional and community-wide open space network.
- Establish hillside trails along undeveloped corridors to link with regional and community systems.
- Provide sensitive transitions between large-lot hillside development and adjacent neighborhoods developed at considerably higher densities. Hillside development should, to the maximum extent feasible and practical, include roadway, open space, and trail links to adjoining neighborhoods.

### **Carbon Canyon/Olinda Village**

#### **Policies for Creating a Sense of Place**

- Preserve the scenic beauty of Brea’s hillsides, and minimize the visual and environmental impact of development upon sensitive hillside areas.
- Maintain Olinda Village’s character as a special residential enclave, with provision for a limited number of shops and services to meet the day-to-day needs of area residents.



- Prohibit the construction of dwellings or other structures on the most sensitive hillside areas; in particular, prominent ridgelines, drainage ways and significant stands of mature vegetation should be left undisturbed.
- Require sensitive grading techniques and other design measures that minimize the visual impact of development and make dwellings unobtrusive.

*Carbon Canyon retains its rural identity, embracing the small, tight-knit settlement of Olinda Village.*

### **Policies for Creating Connections**

- Encourage preservation of the most sensitive hillside areas through conservation easements, land acquisitions, and/or other initiatives that contribute to a regional and community-wide open space network.
- Establish hillside trails along undeveloped corridors that link with regional and community systems.
- Provide a sensitive transition between Olinda Village and new hillside residential development. For example, an undeveloped greenbelt corridor bordering Olinda Village will protect its distinct identity.

### **Olinda Alpha Landfill and Environs**

### **Policies for Creating a Sense of Place**

- Reclaim this area as open space subsequent to closure of the landfill. All development should be restricted, except parks, trails, and related facilities such as rest areas that have minimal impact on the landscape.
- Regenerate the landscape and reestablish wildlife habitat. Native species adapted to local climatic and environmental conditions should be planted.

### **Policies for Creating Connections**

- Provide trails that connect to community and regional systems; trails that are coordinated with recreational facilities in the adjacent Chino Hills State Park are especially encouraged.
- Design trails in this area to accommodate a variety of users. Because of prior disturbance to the site, these trails may be less restrictive and allow hiking, bicycling and equestrian activities.

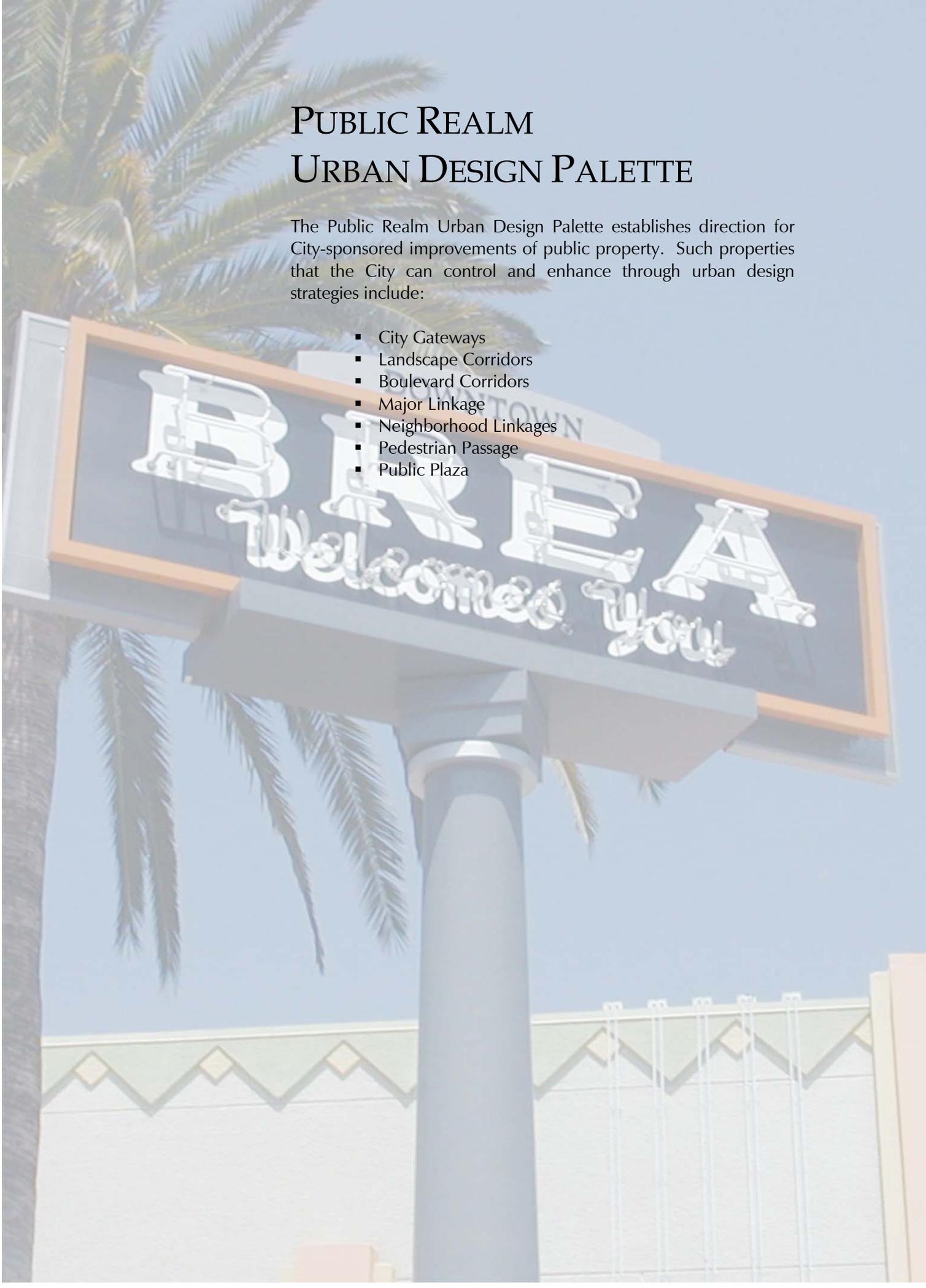


*Reclamation of the Olinda Landfill and environs presents a tremendous opportunity for the City to enhance the visual and recreational appeal of its hillsides.*

# PUBLIC REALM URBAN DESIGN PALETTE

The Public Realm Urban Design Palette establishes direction for City-sponsored improvements of public property. Such properties that the City can control and enhance through urban design strategies include:

- City Gateways
- Landscape Corridors
- Boulevard Corridors
- Major Linkage
- Neighborhood Linkages
- Pedestrian Passage
- Public Plaza



## City Gateway

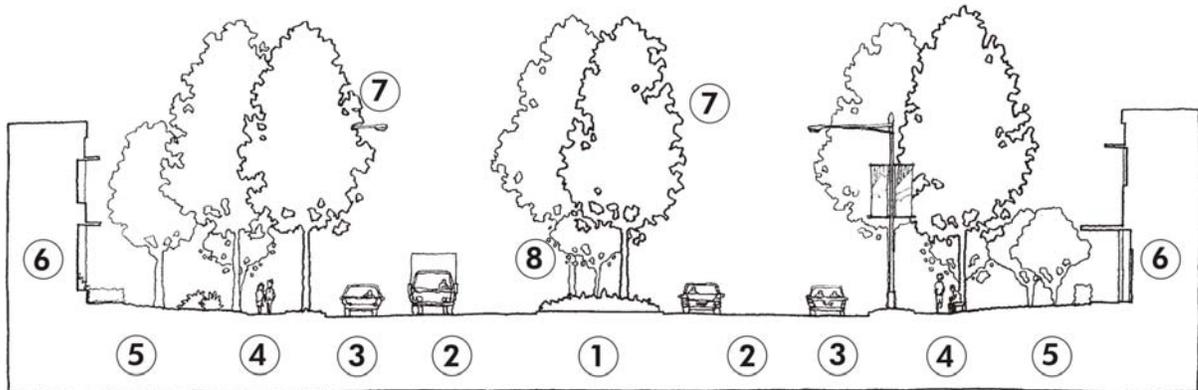
- Identify the City at prominent entrances to the community. Distinctive public art pieces, signage and/or graphics that express the history and character of the community are recommended at City gateways.



- 1. Distinctive signage  
(vertical design element)**
- 2. Special landscape treatment  
(street median)**

- Use a combination of special paving, lighting, and/or landscape treatment to augment the appearance and visual impact of gateways. For example, a change in streetscape design and appearance will communicate passage into Brea.
- Mark community thresholds with vertical design elements. Where appropriate, private development should support the notion of community gateway with strong corner massing, tower elements, or other architectural features that create a distinct profile.

## Landscape Corridor

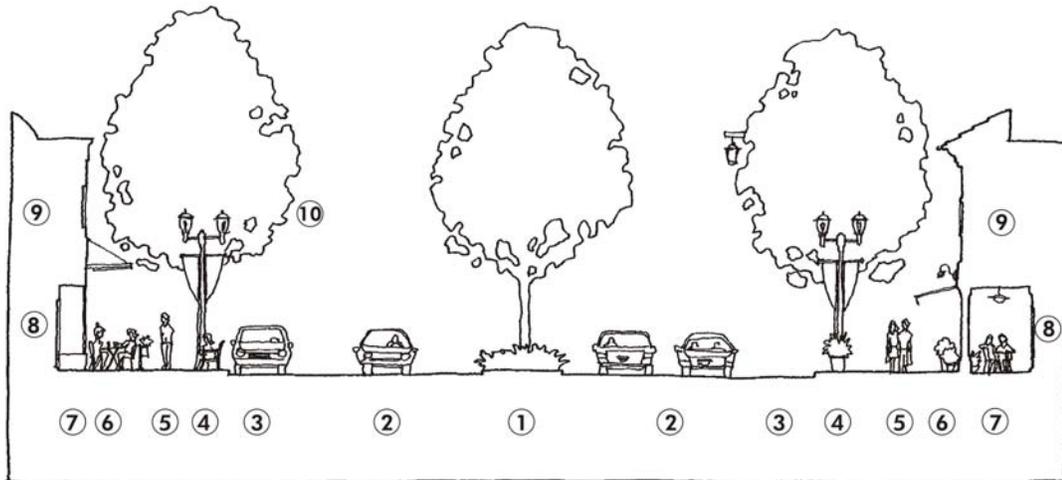


1. **Wide planted median**
2. **Multiple travel lanes each direction typ.**
3. **Travel lane or curb-side parking**
4. **Wide parkway w/ continuous sidewalk**
5. **Setback w/ abundant landscape**
6. **Adjacent use (ex: office / light industrial campus)**
7. **Large scale, tall street trees**
8. **Accent planting (ex: flowering trees)**

- Improve the visual quality of the community's major thoroughfares with abundant landscape. The design of these streets should serve as a physical and visual accompaniment to the community's open space network.
- Provide generous medians and parkways that support extensive plantings. In addition, medians and parkways should accommodate distinctive gateways at the City's boundaries.
- Include continuous sidewalks with wide parkways that establish a physical and psychological separation between pedestrians and autos. Benches and pedestrian-scaled light fixtures should also be included, where appropriate.

- Plant street trees of considerable height and stature appropriate to the scale and function of the street. Flowering trees and shrubs may be included for color interest and to augment the appearance of the street.
- Along Lambert Road east of SR-57, investigate strategies and programs to coordinate with private property owners to create a unified street presence of slopes and fences facing onto Lambert Road.

## Boulevard Corridor

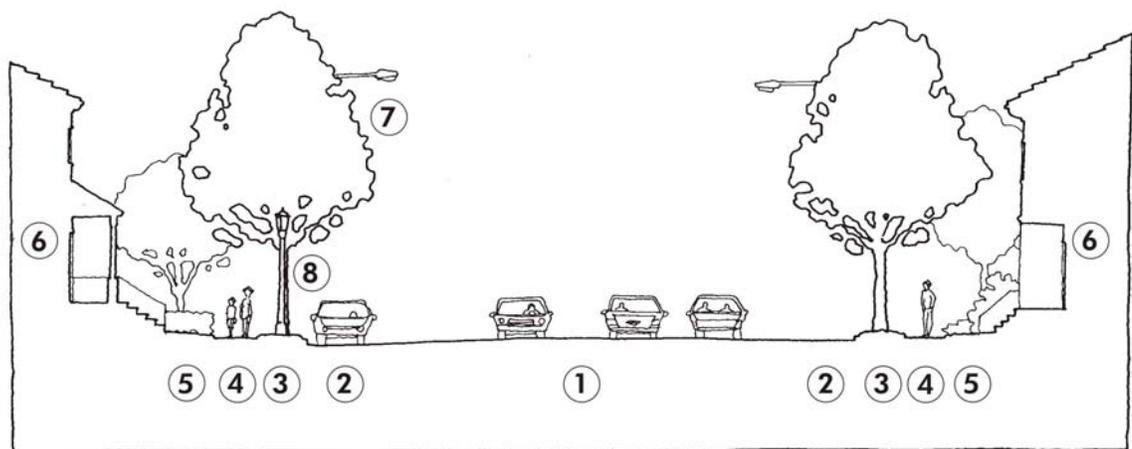


- |   |  |
|---|--|
| 1. <b>Planted median</b>  | 6. <b>Zone for commercial activity</b><br>(ex: outdoor dining) |
| 2. <b>Travel lanes</b>  | 7. <b>Recessed shop entry / Pedestrian arcade</b>              |
| 3. <b>Curb-side parking</b>   | 8. <b>Ground floor commercial</b>                              |
| 4. <b>Zone for pedestrian amenities</b><br>(trees, lighting, furniture) | 9. <b>Upper floor office or residential</b>                    |
| 5. <b>Clear pedestrian passage</b><br>(5' min. width)                   | 10. <b>Large canopy trees</b>                                  |

- Make Boulevards Corridors active streets that accommodate a balance between vehicular, transit, and pedestrian modes of travel. In particular, pedestrians should find these streets amenable to walking, strolling, lingering, meeting, and conversing.
- Provide wide sidewalks that accommodate a clear pedestrian path at least five feet wide, as well as space for streetscape amenities and commercial activities such as sidewalk dining. In general, streetscape amenities should be located adjacent the curb, and on-street parking is encouraged.

- Choose street trees that hold up to urban conditions, planted in tree wells. Tree species and spacing should establish a formal rhythm and should be of sufficient stature to reinforce the space of the street.
- Provide streetscape amenities at frequent intervals, and consider opportunities for public art, environmental graphics, special sidewalk paving, and crosswalk enhancements. At a minimum, furniture should include pedestrian-scaled light fixtures, benches, trash receptacles, and planters of compatible design.
- Emphasize development focused toward the street, with parking located behind or next to buildings rather than in front.

## Major Linkage

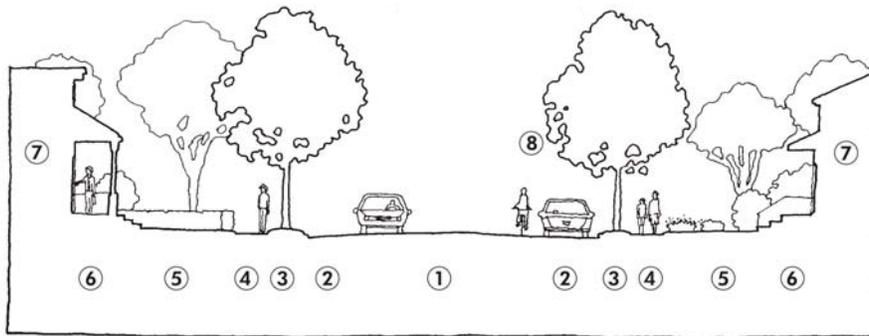


- 1. Travel lanes (no. variable & may include planted median)**
- 2. Curb-side parking**
- 3. Tree lawn**
- 4. Sidewalk (min. 5' wide)**
- 5. Setback w/ abundant landscape**
- 6. Adjacent use oriented to street (ex: townhouses)**
- 7. Canopy street trees adjacent curb**
- 8. Streetscape amenities (ex: pedestrian-scaled light fixture)**

- Improve pedestrian access throughout the community by incorporating pedestrian-oriented design features along streets identified as major linkages. Street trees and other streetscape amenities should communicate that these streets are part of the community's open space network.

- Provide tree lawns or wide sidewalks that accommodate street trees and other streetscape amenities. In most instances, street trees will be located adjacent the curb to heighten the sense of separation between the pedestrian and the automobile.
- Plant street trees that offer overhead cover for pedestrians and create a pleasant rhythm that unifies and leads the eye down the street. Choose species that will distinguish these streets as principal connectors.
- Include street furniture and other streetscape amenities that promote pedestrian safety and comfort, and consider opportunities for bus stop and crosswalk enhancements. Pedestrian-scaled light fixtures and benches that allow periodic rest are especially important.

## Neighborhood Linkages

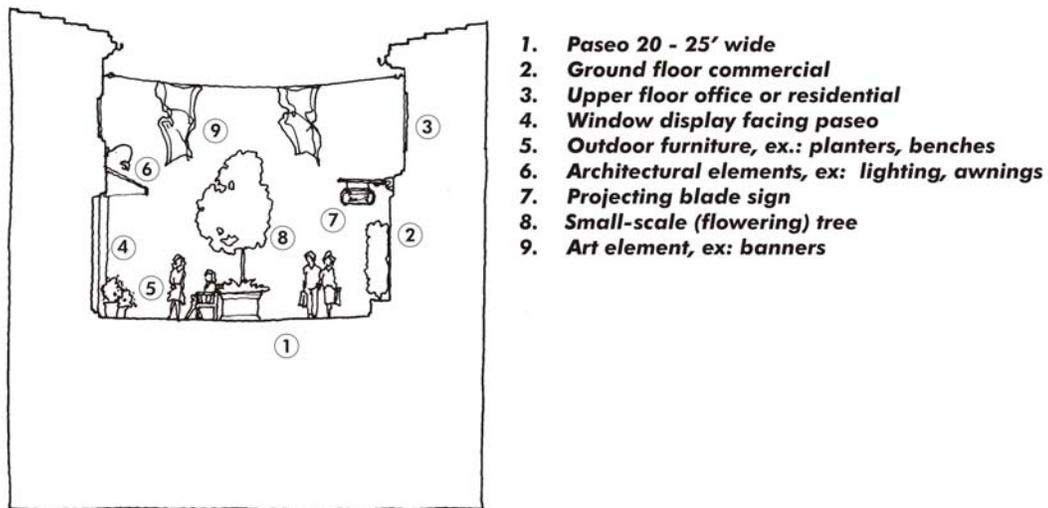


1. **Travel lane each direction (approx. 10' wide each lane)**
2. **Curb-side parking (approx. 8' wide)**
3. **Tree lawn (min. 3' wide)**
4. **Sidewalk (min. 4' wide)**
5. **Front yard area (15' typ.)**
6. **Entry porch**
7. **Dwelling oriented to street**
8. **Spreading canopy trees**

- Design neighborhood linkages streets to support neighborliness and comfortable movement on foot. Residents should feel at ease walking along these streets and should experience a sense of neighborhood pride.
- Encourage narrow travel lanes with curb-side parking to slow traffic. Consistent street tree planting will also narrow the perceptual width of the street.

- Plant spreading, deciduous canopy trees in generous tree lawns measuring at least three feet wide. Street trees may be chosen to lend a street or neighborhood a unique identity, and a formal arrangement is not necessary.
- Orient residences to the street to create attractive street scenes and increase security. Shallow setbacks and front porches are encouraged, providing a pleasant transition between the sidewalk and the dwelling.

## Pedestrian Passage



- Provide pedestrian passages and paseos in conjunction with commercial and mixed-use developments. Pedestrian passages should conveniently and comfortably connect building entrances, outdoor spaces, sidewalks, and parking.
- Incorporate outdoor furniture and other pedestrian amenities along outdoor passages. Benches and planters are especially encouraged, but should be placed at locations that will not interrupt pedestrian circulation.
- Consider opportunities for public art that will draw people through the space. Other features such as kiosks and directories are also recommended.

## Public Plaza



1. **Defined space**
2. **Seating**
3. **Civic art**  
(ex: *interactive fountain*)

- Encourage developers to incorporate public plazas within the community's most intensely active places. Plazas may be provided by public and/or private sector initiative, but should accommodate public use.
- Establish immediate physical and visual connections between public plazas and sidewalks. Public plazas should be located at or near the same grade as adjacent sidewalks.
- Dimension public plazas in accordance with their surroundings. Plazas should be defined spaces that enhance the street environment without harmful interruptions in retail and pedestrian continuity.
- Enliven public plazas with civic art and fountains, as well as seating and shade. Public plazas should accommodate comfortable human occupation and use.

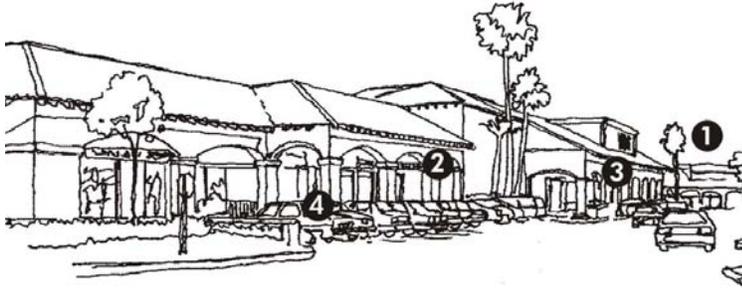
# PRIVATE REALM URBAN DESIGN PALETTE

The Private Realm Urban Design Palette features design recommendations for varied development types:

- Regional/Community Shopping Center
- Local Shopping Center
- Main Street Commercial
- Mixed Use: Housing over Retail
- Mixed Use: Urban Village
- Multi-family Residential
- Single-family Residential
- Office and Industrial Campus



## Regional/Community Shopping Center



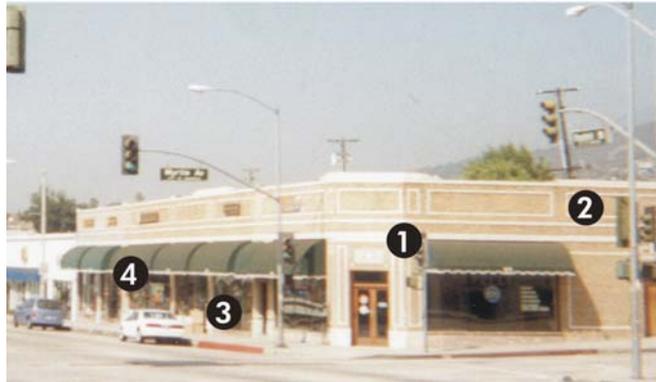
1. **Common design theme**
2. **Articulated building facades**
3. **Highly visible public entrances**
4. **Pedestrian arcade**



- Use shopping center buildings and/or landscape treatment to define street corridors. To the maximum extent feasible, a portion of the development's buildings should contribute to definition of the street edge, especially at street corners.
- Reduce the visual and environmental impact of expansive parking lots. Numerous trees and plantings should shade parking areas, define circulation routes, and screen undesirable views of parking from adjacent properties.
- Give large-scale shopping centers a unified architectural character. A common design theme for all buildings, signage, and landscape treatment is encouraged, including freestanding pad buildings.
- Minimize large, unbroken wall surfaces, and provide highly visible public entrances to all buildings and uses. In particular, public entrances and heavily trafficked outdoor areas should incorporate human-scaled details such as canopies and window displays.
- Provide well-marked and comfortable pedestrian walkways including pedestrian circulation within parking lots with connections to adjacent sidewalks and safe access from parking. Pedestrian circulation routes within parking lots should be clearly delineated, and pedestrian amenities in the form of courtyards, plazas, and covered walks are encouraged.

- Generate visual interest along outdoor passages and paseos. Building facades with articulated entrances, ground floor transparency, and architectural details such as accent lighting are recommended.

## Local Shopping Center



1. **Building defines street corner (parking in rear)**
2. **Neighborhood scale**
3. **Large, transparent shop windows**
4. **Awnings**

- Site local shopping centers in whole or in part close to the street. In particular, buildings should define the edges of street corners.
- Reduce the visual impact of parking on local neighborhoods. To the maximum extent feasible, on-site parking should be located to the rear or side of buildings, with shade trees and extensive plantings that screen undesirable views from sidewalks and adjacent properties.
- Create a street-level environment that is pedestrian-friendly. Design features such as arcades, canopies and awnings, transparent windows and windows displays, etc. should be incorporated.
- Respond to adjacent residential uses with a sensitive transition in scale and massing. For example, building mass should be broken down with step backs in height, articulated sub-volumes, and horizontal and vertical façade articulation.
- Incorporate functional pedestrian connections to adjacent uses. Nearby residential uses should benefit from convenient access to local shopping and services.

## Main Street Commercial



1. ***Buildings define sidewalk edge***
2. ***Multiple shop entrances along sidewalk***
3. ***Recessed shop entrance***
4. ***Large, transparent shop windows***
5. ***Pedestrian-scaled signage***

- Build an especially strong relationship between the building and the street. Buildings should be configured to maintain a continuous edge along the sidewalk and define public space.
- Design buildings to encourage multi-tenant occupancy and walk-in traffic at the street level. Uses that generate negligible walk-in traffic are discouraged from street-front locations.
- Provide a frequent cadence of shop entrances along the street. Recessed entries are encouraged; however, grade separations between sidewalks and ground floor commercial uses are discouraged.
- Encourage the creative expression of individual storefronts, and use shop windows to help animate the street and sustain attention. Generous street level windows that are predominantly transparent glass are strongly encouraged.
- Emphasize design elements such as separate storefronts, display windows, shop entrances, exterior light fixtures, awnings and overhangs, and pedestrian-oriented signage. These features will add interest and give a human dimension to street level facades.
- Generate visual interest along outdoor passages and paseos. Building facades with articulated entrances, ground floor transparency, and architectural details such as accent lighting are recommended.

## Mixed Use: Urban Village



- 1. Buildings define street**
- 2. Active uses along sidewalk  
(ex: ground floor shopping)**
- 3. Upper floor housing w/ balconies**
- 4. Streetscape amenities  
(ex: special paving, street trees)**

- Encourage horizontal and vertical mixing of commercial and residential uses. The inclusion of apartment, townhome, housing over commercial, and main street commercial project types will contribute to a village atmosphere that accommodates live, work, and play.
- Emphasize a street-oriented development pattern. In general, buildings should visually define streets and important public spaces, and provide active uses along sidewalks.
- Incorporate outdoor spaces that are activity oriented and support public gathering. Well-defined parks, plazas, courtyards, and connecting sidewalks and paseos should create a network of pedestrian spaces.
- Use subterranean or interior parking courts to minimize the visual impact of parking. The location and design of parking should not prohibit the desired street-oriented development pattern or inhibit the provision of comfortable and functional outdoor spaces.

## Mixed Use: Housing Over Retail



1. ***Building sited near sidewalk edge***
2. ***Ground floor shop w/ awnings***
3. ***Large, transparent shop windows***
4. ***Visually distinct upper floor housing***
5. ***Balconies & bays***

- Support a street-oriented development pattern, with buildings sited at or near the sidewalk edge. Mixed-use structures should be a component of main street commercial development and other areas where pedestrian activity is strongly encouraged.
- Rely on ground floor commercial space with frequent sidewalk entrances to promote pedestrian activity along the street. In particular, ground floor commercial uses should offer shops and services to local residents.
- Include communal and private open space areas for project residents. These spaces should be designed to limit intrusion by non-residents.
- Make ground floor commercial uses visually distinct from the residential space above. Dwelling units should exhibit a residential character, and residential entrances should read differently from entrances to ground floor commercial uses.
- Incorporate upper floor balconies, bays, and windows that overlook the street and enliven the street elevation. These will also communicate the residential function of upper levels.

## Multi-Family Dwellings



- 1. Domestic scale and appearance**
- 2. Dwellings oriented to street w/ entry stoops**
- 3. Articulated facades**
- 4. Parking interior to block**

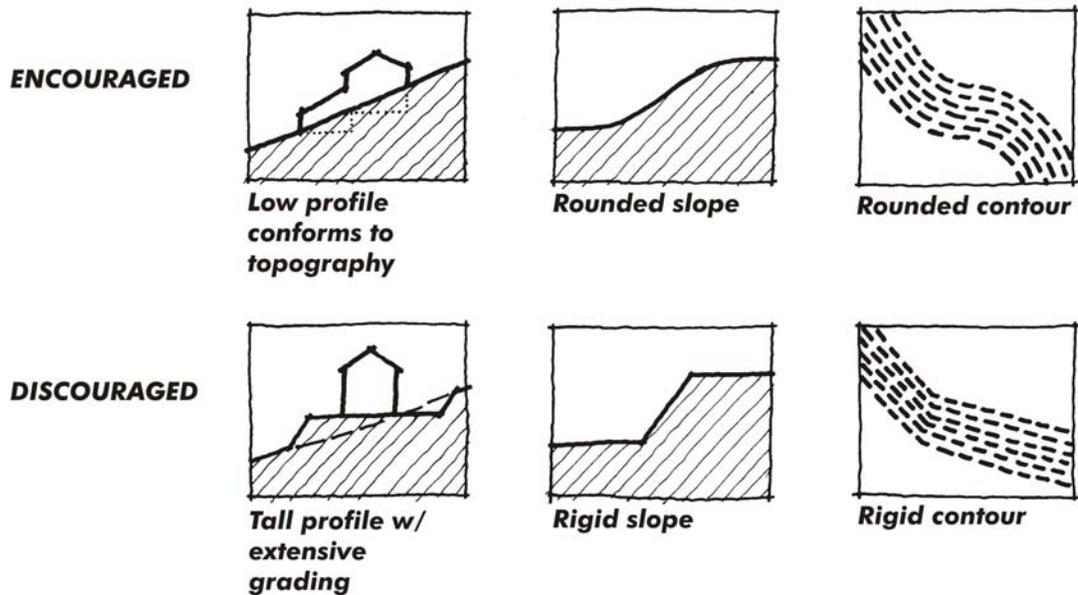
- Reduce the bulk of multi-family buildings, including a transition in scale to adjacent single-family dwellings. Building facades should be articulated to portray a domestic scale and give identity to individual dwelling units.
- Locate parking to reduce its visibility from streets and open space areas, and restrict parking from within required setbacks. Subterranean parking and parking courts interior to a block are appropriate solutions.
- Provide attractive, centrally located communal open space with functional amenities. The amount of communal open space should increase with the size of a multi-family development to meet the social and recreational needs of its inhabitants.
- Include private outdoor space in conjunction with each dwelling unit. Private open space may include patios, terraces, and well-sized balconies.

## Single-Family Dwellings



- Infill single-family residential neighborhoods with dwellings that approximate the scale of established residences. Excessively large dwellings that dominate the surrounding neighborhood are discouraged.
- Create a pleasing variety of homes within low-density residential developments. In particular, dwellings with identical elevations should not be placed on adjacent lots, and developments must include single-story units and/or building forms.
- Avoid repetitive placement of garage doors and reduce their visual prominence. Dwellings should orient to the street, with the garage setback from the front elevation or placed to the rear of the property.
- Promote site configurations that create useable yard areas. For example, consider zero-lot line layouts where they do not conflict with the dominant pattern of development.
- Articulate facades to add interest, reduce monotony, and create identity from dwelling to dwelling. Dwellings should present a simple and pleasing composition that uses a common vocabulary of forms, architectural elements, and materials.

## Hillside Residential



- Restrict construction on environmentally sensitive and visually prominent locations. Dwellings should be sited well below the ridgeline, and avoid prominent landforms, site features, drainageways, and vegetation.
- Use naturalistic grading techniques that blend grading with the existing topography. Grading should use rounded contours and slopes to minimize the apparent disturbance to the site.
- Revegetate with native and/or naturalized species where grading or other activities have disturbed the site. In general, planting should be drought resistant and control erosion.
- Design unobtrusive dwellings, requiring stepped, single story, and low profile building and roof forms that conform to the hillside topography. Tall building forms that block views and detract from the largely undeveloped character of the hillsides are strongly discouraged.
- Incorporate architectural features that reduce bulk, such as eaves, bay windows, varied elevations. Naturalistic materials and colors are also encouraged.

## Office and Industrial Campus



1. ***Facade modulation***
2. ***Highly visible entrance lobby***
3. ***Abundant landscape***

- Organize industrial and large office developments around shared courts, quadrangles, and greens. Multiple paths should connect on-site buildings and uses, transit stops, and public sidewalks.
- Support street-level activity. Buildings should provide ground-floor interest, transparency, and openness, especially views into lobbies.
- Design buildings with individual character and identity, including a highly visible main entrance. Monolithic buildings are discouraged, and massing should be broken down with vertical and horizontal façade modulation.
- Provide abundant landscape in keeping with a campus setting. For example, “orchard” tree planting should be used within parking lots for shade and visual relief.

## ECONOMIC DEVELOPMENT

A healthy local economy consists of a highly trained and educated workforce, diversified businesses, accessibility to major highways and freeways, available housing, financial assistance to businesses, and infrastructure ready to serve technologically advanced businesses. With this Brea can easily to attract, retain, and expand the City's business community. This will in turn ensure Brea's fiscal and financial health, allowing for a high level of public services and programs. Economic development goals can set the tone and direction for the business community in Brea.

### Context

Brea has a true balance of quality office, industrial, retail, residential, recreational, and entertainment uses that offer many employment opportunities and provide a healthy tax revenue base. Brea is also a regional retail center, providing a variety of goods and services for many north Orange County residents. This diversified business sector includes manufacturing, service industries, retail and distribution outlets, and professional corporations.



*Three large office parks within Brea can be found along State College Boulevard adjacent to the Brea Mall, Imperial Highway and Valencia Avenue, and at the intersection of Lambert Road and SR-57.*

In addition, Brea is actively involved in many projects and programs to enhance the business community. The City aggressively offers business retention and attraction programs, housing programs for low- to moderate-income families, and property redevelopment and revitalization programs. Brea constantly monitors available manufacturing and industrial space and provides information to businesses interested in relocating or expanding in the City of Brea. The City continually looks for new initiatives to examine the availability of appropriate infrastructure, such as fiber optics, to accommodate the growing needs of high-tech businesses.

Economic development is a regionally important issue. Many residents work outside of Brea, and many employees who live in other communities come to Brea to work. Census data (year 2000) shows at least 18,000 working residents living in Brea but 40,000 employment positions in the City, creating a net flow of approximately 22,000 jobs. Regional cooperation is needed because transportation and housing cross city lines, and Brea's decisions can affect neighboring cities. Regional cooperation comes from the North Orange County Partnership (NOCP). This organization was formed to ensure that the North Orange County region fully participates in the economic growth of the County and the greater Southern California metropolitan area. The NOCP consists of economic development professionals from the cities of Brea, Buena Park, Fullerton, La Habra, Placentia, and Yorba Linda.

## Businesses and Employment

The approximate 2,500 businesses in Brea employ at least 40,000 people. Four primary employment sectors make up Brea's economic base: trade; finance, insurance, and real estate (FIRE); services; and manufacturing. The advantage of such a diverse business composition means that the City can withstand an economic downturn without experiencing an entire collapse within the local economy.

Trade includes both retail and wholesale trade. All of the retail trade sectors account for the greatest share of employment in the City. Table CD-8 shows the total share of businesses in Brea compared to the region and the County. The percentage of retail employment results from the City serving as a retail center for the region. The Brea Mall is the largest regional retail mall in North Orange County. In addition to the Mall, Brea Union Plaza, Gateway Center, Downtown Brea, and the Brea Marketplace are significant power centers under retail trade.

The FIRE industries account for 12.2 percent of total employment, which is again higher than the region and Orange County at 7.6 percent and 8.4 percent, respectively. Most of this employment is concentrated in the insurance carrier and agents sector.

**Table CD-8  
Share of Total Businesses and Employment (2001)**

Business Sector	Brea		Region (A)		Orange County	
	Businesses	Employees	Businesses	Employees	Businesses	Employees
Services	37.7%	19.8%	42.2%	31.4%	43.8%	35.4%
Retail Trade	24.4%	26.5%	22.3%	21.8%	21.8%	20.2%
Manufacturing	11.6%	20.9%	7.2%	17.1%	7.9%	18.5%
FIRE (B)	9.4%	12.2%	8.8%	7.6%	10.0%	8.4%
Wholesale Trade	7.6%	10.2%	6.9%	8.0%	6.1%	6.7%
Construction	4.9%	4.5%	7.8%	7.9%	5.6%	3.8%
Other (C)	4.5%	6.0%	4.7%	6.3%	4.7%	7.0%
<i>Total</i>	<i>100.0%</i>	<i>100.0%</i>	<i>100.0%</i>	<i>100.0%</i>	<i>100.0%</i>	<i>100.0%</i>

(A) Region (North Orange County) includes Brea, Fullerton, La Habra, Placentia, and Yorba Linda.

(B) FIRE is an acronym for Financial, Insurance, and Real Estate businesses.

(C) Other includes agriculture, mining, government, transportation, and public utilities.

Sources: Claritas and Keyser Marston Associates, Inc.

Manufacturing employment accounts for nearly 21 percent of total employment in the City, with a concentration of manufacturing and light industrial uses in the western area of City along Imperial Highway and in the middle portion of the City east of Kraemer Boulevard along Imperial Highway.

### **Business Growth**

Supporting economic development means making space for both existing businesses to expand and for new businesses to locate in Brea. To ensure adequate supply of land, the City will need to plan in advance to anticipate business expansion potential and to attract new or added uses to the local economy. Expanding or relocating businesses seek not only a sufficient land supply, but also adequate access and infrastructure in an attractive setting with sufficient amenities and resources.

Land use policy accommodates new growth by allowing for the continued economic mix and diversity of businesses. Policy plans for increases in office, industrial, and commercial uses, and accommodates for new mixed-use development.



The Brea Mall provides retail opportunities for Breans and the region.

**Employment Growth**

Between 1999 and 2020, Brea can expect an increase in jobs available in the community to 49,700 (23 percent increase), as shown in Table CD-9. This projected growth is much higher than that projected for the region (9 percent), but lower than Orange County (32 percent) as a whole. Also, the City’s population will grow, fueling employment growth by creating demand for additional retail, service, and public-sector uses and jobs. With a diverse employment base and expansion of employment opportunities for local residents, Breans will continue to benefit from the local economy.

**Table CD-9  
Employment Projection 1990 to 2020**

Area	1990 (A)	1999 (B)	2020 (C)	Percent Increase (1999 to 2020)
Brea	34,284	40,483	49,700	23%
Region (D)	140,867	157,178	170,710	9%
Orange County	1,274,424	1,500,663	1,975,000	32%

- (A) Institute for Economic and Environmental Studies (IEES), 2002 and 1990 Census Transportation Planning Package
- (B) Claritas and Keyser Marston Associated Inc, 2001
- (C) Southern California Association of Governments (SCAG) 2001 RTP Projections
- (D) Region (North Orange County) includes Brea Fullerton, La Habra, Placentia, and Yorba Linda.



*The Brea Job Center provides a neutral site where day laborers may be hired by potential employers. Men and women with expertise in plumbing, landscaping, homemaking, painting and other skills are ready to work. Potential employers may drop by or call the Job Center to arrange for help in their home or business.*

## Redevelopment

The Brea Redevelopment Agency was formed to provide a tool to eliminate blighting conditions and ensure that the City's economic base would grow and remain healthy through new public improvements, commercial development, and affordable housing. Over the years, the Agency adopted and amended several project areas, resulting in numerous successful projects ranging from the Brea Mall and the Civic and Cultural Center to office parks, retail centers, residential projects and public improvements. Community-involved planning efforts such as the Brea Project and the Downtown Charette ensured that public input was included throughout any redevelopment process.

The Agency made significant progress during the later half of the 1990s by restoring a vibrant downtown commercial district, providing numerous affordable housing opportunities, and other economic development successes. Under the guidance of the Economic Development Department, the Agency looks forward to continuing to use redevelopment authority strategically to target improvements within project areas.



*New mixed-use development in Downtown Brea is so successful that potential tenants wanting to move into residential units above commercial space are placed on a waiting list.*

## Economic Development Challenges

Although Brea's economy exhibits good health, the City faces several challenges as it continues to grow in the future. The challenges, however, are not unique to other cities or the region:

- **Underutilized and outdated commercial shopping centers.** While larger commercial centers are currently enjoying successes within the retail market, older and outdated commercial centers are struggling because of secondary locations and loss of market share to newer and larger commercial centers within the City. An example of this issue is the Brea Marketplace, which has been struggling and will more than likely find it difficult to retain tenants, given the number of healthy, vibrant centers with better locations and higher visibility nearby. Due to the location of the center, adjacent to the Brea Mall, Cultural and Civic Center, and high-rise office buildings, this center is an ideal place for intense mix-use combining commercial, office and residential uses to continue the successes of Downtown Brea.
- **Shortage of Industrial Buildings.** Vacancy of industrial space within Brea is very low, and economists believe that Brea has a shortage of industrial buildings, given the City's prime location to regional transportation corridors. Land use policy plans for at least a 10 percent increase in industrial space square footage over the time horizon of the General Plan.
- **Automobile congestion.** Traffic congestion has plagued many Orange County cities and can be a deterrent for potential businesses choosing to locate to Brea. As population and employment grows, congestion will continue to be a foreseeable problem in the future. The Circulation section of this chapter describes the goals and policies designed to confront the problem now and in the future.

- **Housing availability.** Available and affordable housing must be accommodated as Brea continues to expand economically. In the 1990s, the Economic Development Department and the Redevelopment Agency participated in several projects that created new affordable housing opportunities. Brea has identified a need not just for affordable housing but also for homes priced above \$500,000 that establish a move-up market for executives and other high wage earners who wish to remain in Brea as their earning power increases.

## Goals and Policies

The Economic Development goals and policies call for continued economic successes and outline strategies the City will pursue keep Brea on track as a community that experiences a comfortable quality of life.

### Tax Base Revenue Growth

Economic growth can bring many benefits to the community, including jobs, housing, and new revenue. New growth will lead to higher tax revenue, thus benefiting residents and the community directly by enhancing many of the public services the City provides. The City will collaborate with the business community to facilitate growth, development, and infrastructure improvements that benefit residents and businesses alike.

<b>Goal CD-23</b>	<b>Encourage and facilitate activities that expand the City's revenue base.</b>
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Policy CD-23.1	Encourage a broad range of business uses that provide employment at all income levels and that make a positive contribution to the City's tax base.
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Policy CD-23.2	Provide opportunities for mixed-use, office, manufacturing, and retail development that respond to market and community needs in terms of size, location, and cost.
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Policy CD-23.3	Track retail trends, and tailor regulations to respond to market changes, maximize revenue, and maintain the appropriate business mix.
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- Policy CD-23.4 Encourage new development along highly visible corridors that is pedestrian oriented and includes a mixture of retail, residential, and office uses.
- Policy CD-23.5 Use the City's redevelopment authority to create opportunities for businesses to establish in Brea and bring high-skill and professional jobs and new revenue sources into the community.
- Policy CD-23.6 Examine options for the development of new infrastructure for new technologies and businesses that use those technologies.
- Policy CD-23.7 Recognize the need of the work force to have convenient access to safe, affordable, and quality child care.

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## Diversity

Brea understands that part of its economic stronghold stems from its employment diversity of office, retail, manufacturing, and industrial businesses. Retaining and expanding these businesses will continue the economic benefits the City maintains, as well as those seeking employment opportunities in Brea.

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<b>Goal CD-24</b>	<b>Maintain and expand the City's diverse employment base, including office, retail, manufacturing, and industrial businesses.</b>
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- Policy CD-24.1 Engage in activities that promote Brea as a good place to work and develop a business.
- Policy CD-24.2 Identify opportunities to assist underperforming office and employment centers to restructure their tenant mix, market assists, and improve individual customer bases to remain competitive in today's changing market place.
- Policy CD-24.3 Support training and educational opportunities in the City to develop a highly trained and professional work force, and encourage local business to hire local residents.

Policy CD-24.4 Work with organizations such as the Chamber of Commerce to implement marketing programs that identify economic opportunities in the City.

Policy CD-24.5 Continue to work with surrounding cities to strengthen North Orange County Regional Economic Development.

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### Expansion of Downtown Brea

To continue the successes of Downtown Brea, expanding similar uses in areas adjacent to downtown can help revitalize underutilized and struggling properties.

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<b>Goal CD-25</b>	<b>Extend the visual identity, mixture of land uses, and fiscal success of Downtown Brea into adjacent areas, including North/South Brea Boulevard.</b>
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Policy CD-25.1 Coordinate development of residential, office, small retail centers, and similar uses that would serve local residents and would also benefit from the high visibility and access from Downtown Brea.

Policy CD-25.2 Reconfigure parcels that have poor visibility from the street and are difficult to access by car or through walking.

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### Revitalization of Aging Business Centers

Brea recognizes the need to revitalize its older commercial areas and support reinvestment and business growth in these areas. Encouraging economic growth can also help meet identified community needs.

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<b>Goal CD-26</b>	<b>Revitalize aging business centers with uses that bring jobs, housing, and new revenues into the community.</b>
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Policy CD-26.1 Continue to use the City's redevelopment authority to facilitate revitalization of blighted and economically struggling business centers and neighborhoods.

Policy CD-26.2 Provide low-interest property rehabilitation loans to Brea businesses through Economic Development Department programs.

Policy CD-26.3 Explore opportunities for mixed-use development projects on sites historically supporting commercial centers.

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**Implementation  
Guide**



See Section V of the Implementation Guide for action programs.

## GROWTH MANAGEMENT

Given its central location at the crossroads of Orange, Los Angeles and San Bernardino counties, Brea experiences the both positive and negative effects of regional growth. As more people move into the area, they spend tax dollars at the Brea Mall and at shops and restaurants in Downtown, and they drive every weekday to major employment centers in the community. This activity brings increased congestion on the major arterials through Brea: Carbon Canyon Road, Imperial Highway, Lambert Road, and Valencia Road. Brea continues to work cooperatively with surrounding cities and counties to manage growth to minimize adverse effects on the City. More importantly, Brea looks locally to balance its own growth and ensure that the pace of new development and welcome economic growth does not outpace the ability of the City and the Brea-Olinda Unified School District to provide services and infrastructure necessary to achieve “livable community” goals.



*Infill housing development conserves the City's resources of undeveloped land, provides housing near employment opportunities, and often provides lower associated development costs.*

Future growth in Brea will occur as residential, commercial, and industrial infill, as well as new homes in limited hillside areas, are constructed. The outlying areas will require substantial investments in roads, water and sewer lines, and other infrastructure. Proper phasing of new development in concert with augmenting the capacities of infrastructure and public facilities, as identified in the Community Services and Community Resources Chapters, will ensure sufficient levels for service of residents and businesses.

To assure that new development does not negatively impact the existing levels of public services provided by the City, the Growth Management Element provides guidance in balancing provision of public facilities and infrastructure in a manner that will adequately serve current and future residents. In addition, planning for future growth is also required by Orange County Measure M, the Revised Traffic Improvement and Growth Management Plan approved in 1990 by Orange County voters. Measure M allocates funds to provide transportation facilities in Orange County. The funds raised by Measure M are returned to cities to be used for local and regional transportation projects. To qualify for funds, each city must comply with the Countywide Traffic Improvement and Growth Management Program, which requires adoption of local growth management policies.

## Context

Plans and programs adopted by Orange County and the Southern California Association of Governments have bearing on the scope and context of Brea's growth management policies.

### Orange County Revised Traffic Improvement and Growth Management Plan (Measure M)

Measure M, adopted by Orange County voters in 1990, established a one-half cent sales tax for countywide transportation improvements. All eligible jurisdictions receive 14.6 percent of the sales tax revenue based on factors such as the population ratio, Master Plan of Arterial Highways miles, and total taxable sales. To receive the funds, cities must have a local Growth Management Plan. Funds can be used for local projects, as well as ongoing maintenance of local streets and roads. This program was designed to create a process among Orange County jurisdictions to coordinate and implement traffic improvements and planning on a countywide basis.

To receive its allocations of Measure M funds, Brea must submit a statement of compliance with the growth management components addressing the following:

- 1) **Adoption of a Growth Management Element, or similar General Plan component**, that includes traffic Level of Service (LOS) standards. The general target standard should be LOS D for intersections, recognizing that lower LOS standards for certain intersections in urbanized areas may be appropriate if such affected roadways carry a high proportion of regional traffic (for example, Imperial Highway).
- 2) **Develop a Mitigation Program**. This program shall ensure that new growth pays its fair share of the costs associated with that growth. To meet the requirements, Brea is required to: a) adopt a development mitigation program to ensure that development is paying its share of the costs associated with that development; b) impose or continue a traffic impact mitigation fee; c) ensure that new revenues provided by a sales tax increase measure shall no be used to replace private developer funding which has been committed for a project.
- 3) **Participate in Inter-jurisdictional Planning Forums**. Brea, in cooperation with the City-County Coordination Committee, is required to participate in interjurisdictional forums to cooperate in easing cumulative traffic impacts and coordinating improvements in transportation and other facilities. Brea has been an ongoing participant in the Four Corner Study effort to define options for easing traffic congestion where Orange, Los Angeles, Riverside, and San Bernardino counties converge.
- 4) **Development of a Seven-Year Capital Improvement Program**. Brea must determine the capital projects needed to meet and maintain both traffic Level of Service and Performance Standards. The Capital Improvement Program shall include approved projects and an analysis of the costs of the proposed projects, as well as a financial plan for providing the improvements.
- 5) **Address Housing Options and Job Opportunities** as related to transportation demand on a city, sub-regional, and countywide basis. Through adoption and implementation of the Mixed Use I, II, and III General Plan land use designations, Brea creates opportunities for housing and jobs to be located in close proximity to each other, thereby reducing vehicle trips.

- 6) **Adopt a Transportation Demand Management Ordinance** or alternative mitigation to reduce single-occupancy automobile travel.

**Southern California  
Association of  
Governments**

The Southern California Association of Governments, or SCAG, has adopted a Growth Management Plan that applies to the six-county SCAG region (Orange, Los Angeles, San Bernardino, Riverside, Ventura, and Imperial counties). This plan recommends ways that cities and counties can redirect regional growth to minimize traffic congestion and improve environmental quality. A key goal of SCAG’s Growth Management Element is to better balance jobs and housing within subregions.

The South Coast Air Quality Management District supports the SCAG Growth Management Plan through implementation of the Air Quality Management Plan (AQMP). The AQMP mandates a variety of measures to reduce traffic congestion and improve air quality. To comply with the AQMP, Brea has adopted an Air Quality Implementation Plan to meet the requirements for local jurisdictions as identified in the regional AQMP. New development in the City must comply with the Air Quality Implementation Plan.

## Goals and Policies

Inherent in the Land Use and Infrastructure sections of this Chapter is the goal to balance growth with infrastructure and services. The following goals and policies tie together all development goals under the unifying theme of growth management, and indicate Brea’s commitment to continue to work with other jurisdictions to address regional concerns.

<b>Goal CD-27</b>	<b>Promote balanced growth with supporting public services infrastructure.</b>
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Policy CD-27.1     Integrate land use and transportation planning to provide adequate transportation system service standards.

Policy CD-27.2     Monitor and maintain service levels standards for public services and infrastructure.

Policy CD-27-3     Ensure that new development is in balance with the provision of services and/or funding.

Policy CD-27.4 Explore infill development opportunities wherever possible as open space becomes more limited.

Policy CD-27.5 Support programs that match Brea residents with local jobs to reduce long commutes.

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<b>Goal CD-28</b>	<b>Assist in the provision of adequate regional and local transportation facilities.</b>
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Policy CD-28.1 Cooperate with other agencies to address regional issues and opportunities related growth, transportation, infrastructure, and other planning issues.

Policy CD-28.2 Promote the expansion and development of alternative methods of transportation.

Policy CD-28.3 Encourage the development of housing within close proximity to jobs and services.

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**Implementation Guide** 

See Section VI of the Implementation Guide for action programs.

Chapter 3  
**HOUSING**

THE CITY OF  
BRE A  
GENERAL PLAN



# Chapter 3

## HOUSING ELEMENT

### 1. INTRODUCTION

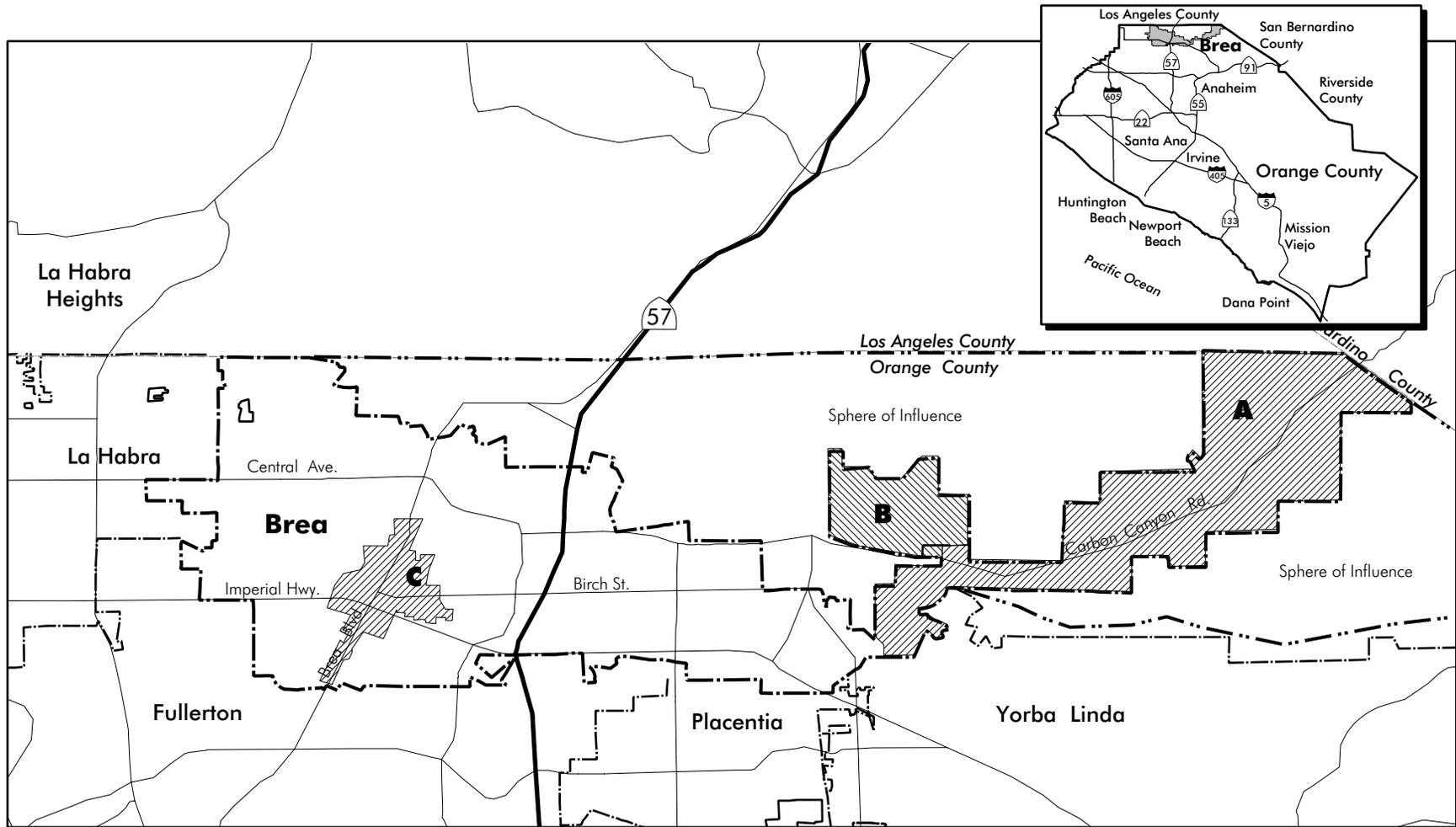


#### A. COMMUNITY CONTEXT

Located along the 57 Freeway among rolling hills in northeast Orange County (refer to Figure HE-1), Brea started as a small oil town and has grown to a community of more than 36,000 people today. Although incorporated in 1917, the 1950s and 1960s brought an avalanche of housing and new businesses into Brea. During this time, the population more than doubled every ten years. Significant population and employment growth continued well into the 1970s and 1980s, but leveled off in the 1990s as vacant land became increasingly scarce.

Demographic shifts are occurring in the City. As Brea continues to grow, the ethnic composition of the population is changing. In 1990, more than 77 percent of the Breans were white. The Asian and Hispanic shares of the population were 6 percent and 15 percent, respectively. While no updated census is available, enrollment data in the Brea-Olinda Unified School District (BOUSD) provides some insight into the racial/ethnic composition of Breans. In 1999, about 10 percent of the students in BOUSD are Asian and 22 percent are Hispanic. Also, the population is aging, with the senior population having increased to more than 3,000 residents, making up over 9 percent of the population in 1990. According to the Orange County Register, Brea's senior population is projected to triple by 2020.

Brea serves as an important retail and industrial center for the north Orange County area. It is home to numerous large corporations and offers a wide variety of employment opportunities. The City's strong tax base allows the citizens to enjoy a high level of public services rarely found in a community of similar size.



-  City Boundary
  -  Sphere of Influence
- Area Plan**
- A.** Carbon Canyon Specific Plan
  - B.** Olinda Ranch Specific Plan
  - C.** Brea Towne Plaza Specific Plan



Figure HE-1  
City of Brea

The City's strong employment base, high level of service, highly rated school system, and quality residential neighborhoods contribute to its attractiveness as a place to live, resulting in an ever tightening housing market. Housing costs in Brea continue to rank among the highest in north Orange County, second only to Yorba Linda. The median price of a single-family home is estimated at over \$230,000 and rent for a two-bedroom apartment ranges from \$750 to \$1,245 per month in 1999.

Brea offers a mix of housing choices. Of the 13,160 units in the City, 62 percent are single-family homes, 30 percent are multi-family units, and 7 percent are mobile homes and trailers. The majority of the housing stock is in good physical condition. There are, however, a few neighborhoods with visible signs of deterioration, reflecting in part that many homes are 30 years or older. The City recognizes that the preservation and improvement of the existing housing stock is an important issue, and has adopted a Neighborhood Enhancement Plan which sets forth policies and programs to specifically address the problem conditions found in six selected areas.

The majority of the 2,600+ dwelling units remaining to be developed in Brea will occur in the northern and eastern hillside areas within the Carbon Canyon and Olinda Ranch Specific Plan areas. The City will continue to provide opportunities for multi-family housing development both within Carbon Canyon and on in-fill sites in the downtown area. Over the years, the Redevelopment Agency has assisted in the development of various in-fill residential projects in downtown. It has also been active in using its set-aside funds for the preservation, improvement, and development of affordable housing in other parts of the City.

## B. ROLE OF HOUSING ELEMENT

Brea is faced with various important housing issues: a balance between employment and housing opportunities; a match between the supply of and demand for housing; preserving and enhancing affordability to provide housing for all segments of the population; preserving the quality of the housing stock; and providing new types of housing necessary to accommodate the demographic shifts. This Housing Element provides policies and programs to address these issues.

Brea's Housing Element is a five-year plan extending from 2000-2005, unlike other General Plan elements which typically cover a minimum ten-year planning horizon. This Housing Element identifies strategies and programs that focus on: 1) conserving and improving existing affordable housing; 2) providing adequate housing sites; 3) assisting in the development of affordable housing; 4) removing governmental and other constraints to the housing development; and 5) promoting equal housing opportunities.

The Housing Element consists of the following major components:

- An analysis of the City's demographic and housing characteristics and trends (Section 2)
- A summary of the existing and projected housing needs of the City's households (Section 3).
- A review of potential market, governmental, and environmental constraints to meeting the City's identified housing needs (Section 4).
- An evaluation of resources available to address Brea's housing goals (Section 5).
- A statement of the Housing Plan to address the City's identified housing needs, including housing goals, policies and programs (Section 6).

## C. DATA SOURCES

In preparing the Housing Element, various sources of information are consulted. The 1990 Census provides the basis for population and household characteristics. Although dated, the Census remains the most comprehensive and widely accepted source of information on demographic characteristics. In addition, 1990 Census data must be used in the Housing Element to ensure consistency with other Regional, State, and Federal housing plans. However, several sources of information are used to supplement and provide reliable updates of the 1990 Census.

- Population and demographic data is updated by the State Department of Finance, and 1998 school enrollment information from the State Department of Education;

- Housing market information, such as home sales, rents, and vacancies, is updated by City surveys and property tax assessor's files;
- Local and County public and nonprofit agencies are consulted for information on special needs populations, the services available to them, and gaps in the system;
- Lending patterns for home purchase and home improvement loans are provided through the Home Mortgage Disclosure Act (HMDA) database; and
- Housing condition information is provided by the City.

## D. PUBLIC PARTICIPATION

Opportunities for residents to recommend strategies, review, and comment on Brea's Housing Element are an important component of the housing element preparation program. Under this program, the Housing Breans Advisory Board, consisting of volunteer residents, review and provide input into the programs, strategies, and policies. The Planning Commission and City Council conduct a study session upon completion of the housing needs assessment and prior to formulation of the Element's policies and programs. After review of the draft Element by the State Department of Housing and Community Development (HCD), public hearings are held before the Planning Commission and the City Council. Notification is published in the local newspaper in advance of each hearing and copies of the draft Element are made available for public review at City Hall and public libraries. In addition, copies of the Element are sent to the Brea Olinda Unified School District.

In addition to the public participation program described above which involved the local community, the City also brought together an outside "Housing Resource Team" for a two-day workshop program in early 2000. The Resource Team was comprised of working professionals in housing related fields, and offered innovative and creative ideas and recommendations to the City for addressing its housing issues and needs. Among the many ideas generated, ten were selected as recommendations that would be most relevant and effective in Brea:

1. Explore reuse opportunities on obsolete commercial or industrial sites, especially obsolete strip retail centers.
2. Amend the City's Inclusionary Housing Ordinance.
3. Explore creation of a local Housing Trust Fund and/or non-profit housing corporation.
4. Seek out opportunities to facilitate the development of larger apartment units for low- and moderate-income families in mixed-use complexes.
5. Partner non-profits and affordable housing builders with for-profit developers.
6. Promote mixed-use development where housing is located adjacent to jobs, shopping, services, schools, and leisure opportunities.
7. Work with developers/operators to ensure that any senior housing and/or assisted living projects are appropriately designed and staffed, and offer necessary range of services for residents.
8. Meet some affordable housing demand by allowing extra rental unit on single-family lots (e.g. carriage units above garages), notwithstanding inclusionary requirements.
9. Explore more innovative parking solutions (e.g. tandem parking, garage lifts, use of driveway apron area, shared parking, carports, on-street parking).
10. Demand creative look at opportunities in hillsides; do not simply settle for smaller number of executive-style units.

These top recommendations of the Housing Resource Team are incorporated into the Housing Plan (Section 6) of the Element. A written report documenting this innovative process and the ideas/recommendations generated is attached as an appendix to the Housing Element.

## E. RELATIONSHIP TO OTHER GENERAL PLAN ELEMENTS

The City of Brea General Plan consists of eight elements: 1) Land Use; 2) Circulation; 3) Housing; 4) Open Space and Conservation (combined); 5) Park, Recreation and Human Services; 6) Safety; 7) Noise; and 8) Growth Management. This Housing Element builds upon the other General Plan elements and is consistent with the policies and proposals set forth by the Plan. Examples of inter-element consistency include: residential development capacities established in the Land Use Element are incorporated within the

Housing Element, and the discussion of environmental constraints in the Housing Element is based upon information from the Open Space and Conservation, and Safety elements.

## 2. HOUSING NEEDS ASSESSMENT



This Needs Assessment section of the Housing Element reviews and evaluates Brea's demographic, socio-economic, and housing characteristics and trends as a basis for determining the nature and extent of the City's specific housing needs.

### A. POPULATION CHARACTERISTICS

The characteristics of a city's population are important factors affecting the housing market in the community. Issues such as population growth, age, race/ethnicity, and employment all help determine the city's housing needs.

#### 1. Population Growth Trends

Brea grew tremendously prior to 1980. Many new housing developments and businesses located in the City between the 1940s and 1960s. The 1970s was a period of intensive development marked by the opening of the Orange (SR-57) Freeway and the construction of Brea Mall, a regional shopping center. Brea's growth in the post-1970s period has been relatively moderate. Between 1980 and 1990, the population grew by 18 percent, from 27,910 to 32,873. This growth was 7 percentage points lower than the countywide population increase of 25 percent. Table HE-1 and Figure HE-2 compare population changes in the 1980s and 1990s for Brea, the nearby cities, and the County. Brea's population growth in the 1980s was identical to that of Placentia, but higher than that of Fullerton and La Habra. Like other north Orange County cities, the increase in Brea's population was well below that experienced by Yorba Linda, the city with the highest growth percentage in north Orange County.

As of January 1999, the City had an estimated population of 36,388 (State Department of Finance), representing an increase of nearly 11 percent from 1990. Countywide, the population grew by 15 percent during the nine-year period. Among the five cities, the (percentage) increase in Brea's population is the second lowest in the 1990s. This implies that the City's population size has been

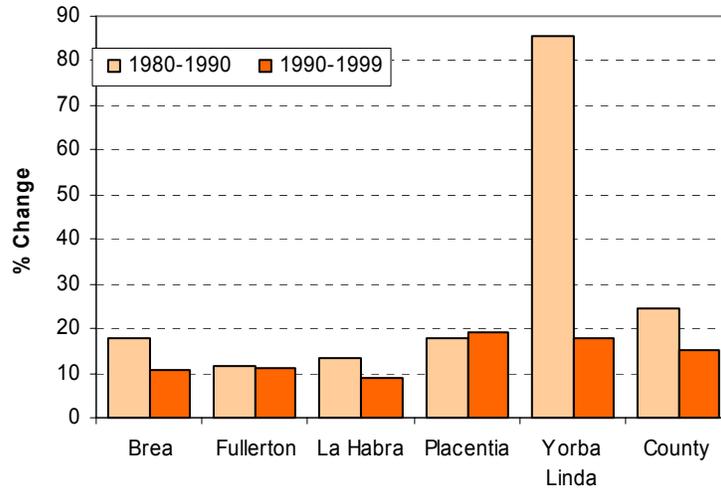
rather stable, especially when compared to Placentia and Yorba Linda which have experienced relatively high population growth in the 1990s.

**Table HE-1  
Population Growth Trends - 1980s and 1990s**

Jurisdiction	1980	1990	1999	1980 - 1990 % Change	1990 - 1999 % Change
<b>Brea</b>	27,910	32,873	36,388	17.8%	10.7%
Fullerton	102,235	114,144	126,757	11.6%	11.1%
La Habra	45,232	51,266	55,805	13.3%	8.9%
Placentia	35,037	41,259	49,127	17.8%	19.1%
Yorba Linda	28,251	52,422	61,839	85.6%	18.0%
Orange County	1,932,708	2,410,556	2,775,619	24.7%	15.1%

Sources: 1980 and 1990 Census  
State Department of Finance, January 1999

**Figure HE-2  
Population Growth - 1980s and 1990s**



Sources: 1980 and 1990 Census

## 2. Age Characteristics

Age is an important influence on housing demand because people of different age groups have different housing needs. Traditionally, the assumption has been that young adults (20 to 34 years old) prefer apartments, low- to moderate-cost condominiums, and smaller single-family units. Adults between 35 and 65 years old provide the major market for moderate- to high-end apartments, condominiums, and single-family homes. This segment of the population is more likely to have higher incomes and larger household sizes. The senior population (65 years and older) tends to demand low- to moderate-cost apartments and condominiums, group quarters, and mobile homes.

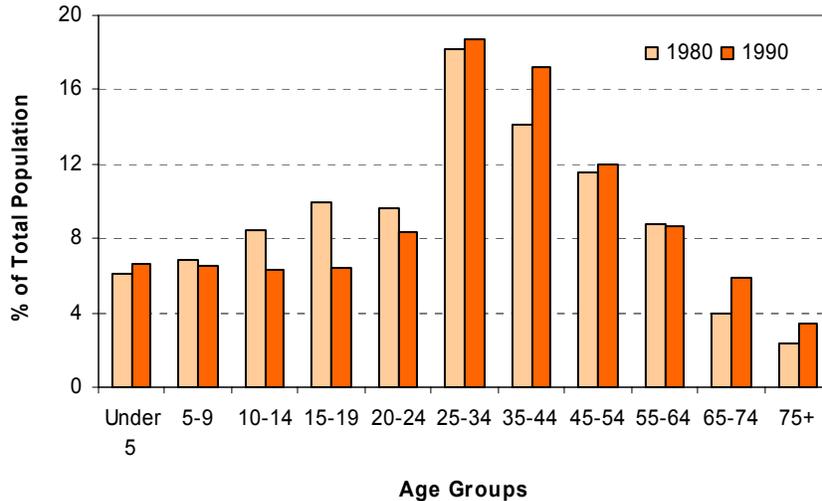
In 1990, the median age in the City was 33.5 years, 2.1 years higher than the County median of 31.4 years. Between 1980 and 1990, the share of Brea's population aged between 20 and 34 years old remained virtually unchanged at 27 percent. Adults between the ages of 35 and 64 made up a slightly higher proportion of the population in 1990 than in 1980. The senior population also grew: the share of the population aged 65 years and above increased by 3 percentage points during the ten-year period. More telling though is that the senior population increased by 73 percent, from 1,763 in 1980 to 3,052 in 1990. This indicates that special attention must be paid to the housing and supportive services needs of senior residents. Table HE-2 and Figure HE-3 show the age distributions of Brea's population in 1980 and 1990.

**Table HE-2  
Age Distribution - 1980 and 1990**

Age Group	1980		1990		County % of Total
	Persons	% of Total	Persons	% of Total	
Under 5 years	1,703	6.1	2,176	6.6	7.7
5-9	1,887	6.8	2,132	6.5	6.8
10-14	2,377	8.5	2,078	6.3	6.1
15-19	2,769	9.9	2,108	6.4	7.0
20-24	2,690	9.6	2,714	8.3	9.3
25-34	5,087	18.2	6,147	18.7	20.1
35-44	3,947	14.1	5,644	17.2	15.6
45-54	3,242	11.6	3,961	12.0	10.6
55-64	2,445	8.8	2,861	8.7	7.5
65-74	1,121	4.0	1,939	5.9	5.4
75+	642	2.3	1,113	3.4	3.8
Total	27,910	100.0	32,873	100.0	100.0
Median Age	30.7 years		33.5 years		31.4 years

Sources: 1980 and 1990 Census

**Figure HE-3  
Age Distribution - 1980 and 1990**



Sources: 1980 and 1990 Census

School enrollment in the Brea Olinda Unified School District increased in the 1990s, according to the California State Department of Education. Between 1992 and 1997, the total number of students enrolled in kindergarten to twelfth grade grew by about 13 percent, from 5,262 to 5,965. There was an increase in enrollment for all grades, with the greatest percentage growth in grade 6. An increase in enrollment over the years indicates that the school age population has grown steadily.

### 3. Race and Ethnicity

The nature and extent of a community's housing needs is, to a large extent, determined by the racial/ethnic composition of its population. The size and type of housing preferred, the neighborhood desired, and the ability to purchase or rent housing often vary by a person or household's racial or ethnic background. Table HE-3 compares the racial/ethnic composition of Brea's population in 1990 with that of the County's.

**Table HE-3  
Race and Ethnicity – 1990**

Race/ Ethnicity	Brea		Orange County	
	Persons	% of Total	Persons	% of Total
White	25,359	77.1%	1,554,501	64.5%
Asian	1,957	6.0%	240,756	10.0%
Hispanic	5,078	15.4%	564,828	23.4%
Black	332	1.0%	39,159	1.6%
Other	147	0.4%	11,312	0.5%
Total	32,873	100.0%	2,410,556	100.0%

Source: 1990 Census

Brea's population is predominantly white. As shown above, whites made up 77 percent of the City's population in 1990, comparing to 65 percent for the County. The Asian and Hispanic shares of the population were 6 percent and 15 percent, respectively. These figures are lower than those for the County; Asians and Hispanics together make up about a third of the County's population.

## 4. Employment

According to the 1990 Census, 19,628 Breans were in the labor force. (The labor force includes employed and unemployed persons aged 16 years and above.) This represents a labor force participation rate of 75 percent. As shown in Table HE-4, most of the residents were employed in two occupational categories: managerial and professional specialty (37 percent) and sales, technical, and administrative support (36 percent). The unemployment rate was 3.3 percent. According to the State Employment Development Department, Brea's unemployment rate in 1999 was very low at 1.8 percent, nearly 1 percentage point lower than the countywide rate.

**Table HE-4  
Occupation of Residents – 1990**

<b>Job Category</b>	<b>Number</b>	<b>% of Total</b>
Managerial/Professional	6,987	36.8%
Sales, Technical, Administrative (Support)	6,825	36.0%
Service Occupations	1,464	7.7%
Precision Production, Craft & Repair	1,841	9.7%
Operators, Fabricators, & Laborers	1,670	8.8%
Farming, Forestry, & Fishing	190	1.0%
Total Employed Persons	18,977	100.0%

Source: 1990 Census

Considering the relatively small population of the City, Brea had a large employment base in 1990. The City's employment base of 31,519 jobs was only slightly lower than its population of 32,873 (SCAG). Major employers in Brea include Bank of America, American Funds Service Company, American Suzuki Motor Corporation, Beckman Instruments, Ericsson Communications, and Avery Dennison. Data from SCAG show that the number of jobs has actually decreased in the 1990s; the projected figure for the year 2000 is 25,098. However, Brea's employment base is projected to increase dramatically (by 18,986 jobs) between 2000 and 2020. Should the predicted increase in jobs be realized, there will very likely be an accompanying increase in the demand for housing in the City. Thus, job growth is certainly a possible major stimulus to housing demand in the near future.

A general measure of the balance of a community's employment opportunities with the needs of its residents is through a "jobs-housing ratio" test. A balanced community would have a match between employment and housing opportunities so that most of the residents can also work in the community. SCAG data show that there were 22,015 jobs and 12,527 occupied housing units in Brea in 1994. These figures translate to a jobs-housing ratio of 1.76, which is higher than the County's ratio of 1.45 and indicates that the City's employment base is high relative to the size of its occupied housing stock. Table HE-6 compares Brea's jobs-housing ratio against that of the nearby cities and the County. As shown below, Brea has the highest jobs-housing ratio among cities in northeast Orange County.

**Table HE-5  
Jobs-Housing Ratio – 1994**

<b>Jurisdiction</b>	<b>Jobs-Housing Ratio</b>
<b>Brea</b>	1.76
Fullerton	1.75
La Habra	1.07
Placentia	0.85
Yorba Linda	0.42
County	1.45

Source: SCAG Baseline Projections, 1998

## B. HOUSEHOLD CHARACTERISTICS

The characteristics of households provide important information about the housing needs in a community. Income and affordability are best measured and examined at the household level, as are the special needs of certain groups, such as large-family households or female-headed households.

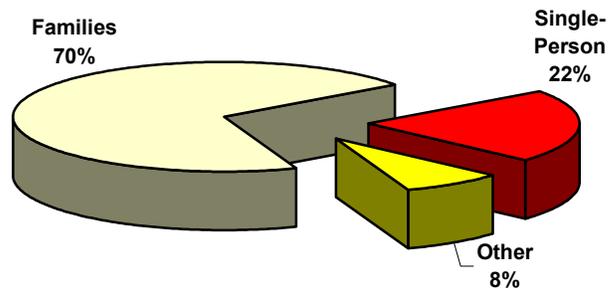
### 1. Household Composition and Size

The Census defines a "household" as all persons who occupy a housing unit, which may include single persons living alone, families related through marriage or blood, or unrelated persons sharing living quarters. Persons living in retirement or convalescent homes, dormitories, or other group living situations are not

considered households. Household characteristics are important indicators of the type and size of housing needed in a city.

According to the 1990 Census, about 70 percent of the 12,224 households in the City were families. This proportion is almost identical to that of Orange County as a whole, where 71 percent of all households consisted of families. Single-person households comprised 22 percent of all households in the City, while 8 percent of households made up of unrelated persons living together (“other”). Figure HE-4 shows the composition of households in Brea in 1990.

**Figure HE-4  
Household Composition – 1990**



Source: 1990 Census

Household size is defined as the number of persons living in a housing unit. A noticeable change in the average household size over time reflects a change in the household composition of a community. For example, a city's average household size will increase over time if there is a trend towards larger families. In a community with a growing number of elderly households, the average household size will usually decline.

Between 1980 and 1990, the average household size in Brea decreased from 2.81 to 2.68 persons per household. This decline, however, did not continue in the 1990s. According to the State Department of Finance, the estimated household size in 1999 is 2.85 persons. This figure is lower than the estimates for nearby cities and Orange County as a whole. Table HE-6 compares the average household size of Brea, its neighbors, and the County.

**Table HE-6  
Average Household Size – 1999**

<b>Jurisdiction</b>	<b>Persons Per Household</b>
<b>Brea</b>	<b>2.85</b>
Fullerton	2.94
La Habra	3.02
Placentia	3.27
Yorba Linda	3.28
County	3.04

Source: State Department of Finance, January 1999

## 2. Overcrowding

Overcrowding is a problem of misfit between a household and the housing unit. The State Department of Housing and Community Development (HCD) defines overcrowding as more than one person per room, excluding bathrooms, kitchens, hallways, and porches. Overcrowding occurs primarily because households “double-up” to afford high rents and because of a lack of available housing units of adequate size to accommodate a growing number of families with larger numbers of children.

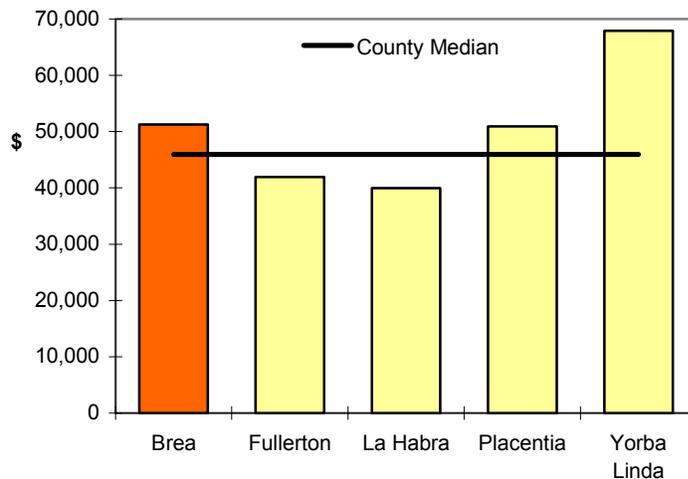
According to the 1990 Census, 629 or 5 percent of all households in the City were living in overcrowded conditions. The incidence of overcrowding is much lower in Brea than in the County (11 percent) and reflective of the City’s smaller household size. As was (and is) the case in most cities, overcrowding among renter-households in Brea was more prevalent than among owner-households. Only 2.4 percent of the owner-households were overcrowded, comparing to nearly 10 percent of renter-households. Lower income households have an overcrowding rate of 9 percent. Of the 265 lower income households living in overcrowding conditions, 238 are renters and 27 are owners.

The City has made an effort to address overcrowding in Brea. Subsequent to 1990, the Redevelopment Agency acquired and rehabilitated 51 deteriorated apartment units previously occupied by a total of 300 persons. Of these residents, 23 households remained in the project after rehabilitation and 15 purchased homes using relocation assistance provided through the Agency. The remaining households relocated to non-overcrowded units elsewhere.

### 3. Household Income and Income Distribution

Income is a major factor influencing the demand for housing and to a large extent, reflects the affordability of housing in a community. According to the Census, the median household income in Brea was \$51,253 in 1989, about 12 percent higher than the County median of \$45,922. Figure HE-5 shows median household income of Brea in comparison to the nearby cities and the County.

**Figure HE-5  
Median Household Incomes – 1989**



Source: 1990 Census

The State Department of Housing and Community Development has developed the following income categories based on the Median Family Income (MFI) of a Metropolitan Statistical Area (MSA):

- Very Low Income: 50 percent or less of the area MFI;
- Low Income: between 51 and 80 percent of the area MFI;
- Moderate Income: between 81 and 120 percent of the area MFI;
- Upper Income: greater than 120 percent of the area MFI.

The 1989 MFI of \$46,900 for the Anaheim-Santa Ana MSA was used to interpolate the City's income distribution from the Census according to the above income categories. Table HE-7 shows the

income distribution of Brea's population, while Table HE-8 provides household composition by income groups.

**Table HE-7  
Income Distribution – 1990**

<b>Income Level</b>	<b>Number of Households</b>	<b>% of Total</b>	<b>Cumulative %</b>
< \$5,000	214	1.7%	1.7%
\$5,000 - \$9,999	419	3.4%	5.2%
\$10,000 - \$14,999	535	4.4%	9.5%
\$15,000 - \$24,999	1,161	9.5%	19.0%
\$25,000 - \$34,999	1,587	13.0%	32.0%
\$35,000 - \$49,999	1,952	15.9%	47.9%
\$50,000 - \$74,999	3,358	27.4%	75.3%
\$75,000 - \$99,999	1,630	13.3%	88.6%
\$100,000 - \$149,000	1,091	8.9%	97.6%
\$150,000 +	300	2.5%	100.0%
<b>Total</b>	<b>12,247</b>	<b>100.0%</b>	

Source: 1990 Census

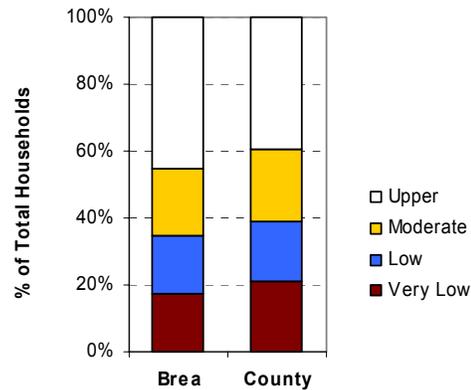
**Table HE-8  
Households by Income Groups – 1990**

<b>Income Groups (defined as % of the Area MFI)</b>	<b>Households</b>	<b>% of Total</b>
Very Low (<50%)	2,149	17.5%
Low (51 to 80%)	2,095	17.1%
Moderate (81 to 120%)	2,468	20.2%
Upper (120%+)	5,535	45.2%
<b>Total</b>	<b>12,247</b>	<b>100.0%</b>

Source: 1990 Census

The percentage of Brea households in the very low- and low income groups is slightly lower than that of households countywide. About 35 percent of the households in the City have very low or low incomes comparing to nearly 40 percent of the households in Orange County. Brea is predominantly a moderate and upper income community, with nearly two-thirds of its households in these income ranges. Figure HE-6 compares Brea’s household composition by income group with that of the County.

**Figure HE-6  
Household Composition by Income Groups – 1989**



Source: 1990 Census

## 4. Housing Affordability and Overpayment

Affordability measures the burden of monthly housing expenditures in relation to the residents’ incomes. State and federal standards state that a household overpays for housing if it spends 30 percent or more of its gross income on housing. Table HE-9 provides a profile of housing overpayment by household type and tenure.

**Table HE-9  
Housing Overpayment Profile**

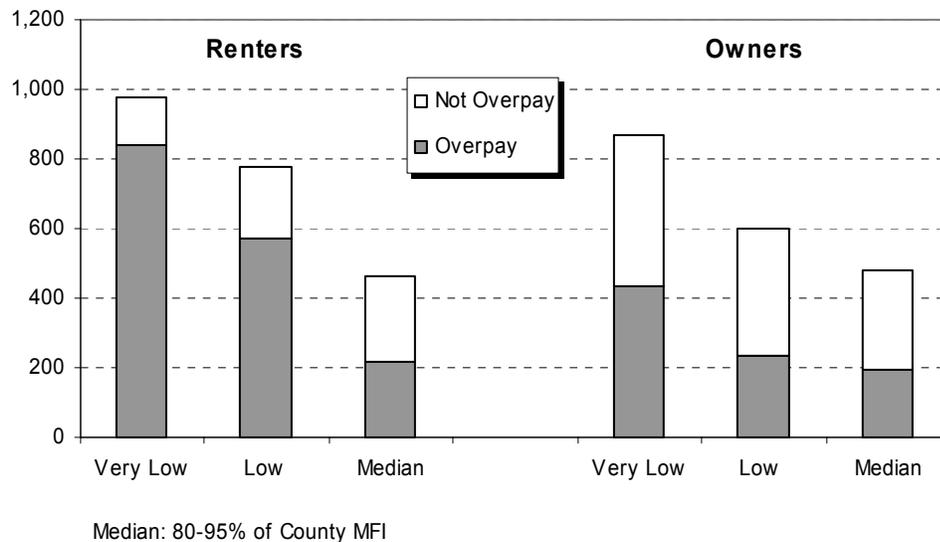
Household Type	% of All Households	% of Owner Households	% of Renter Households	% of Lower Income
Total	32%	28%	41%	68%
Seniors	37%	24%	70%	60%
Small Related	29%	29%	30%	64%
Large Related	31%	26%	45%	75%
Others	37%	30%	41%	83%

Source: CHAS Database, 1993

According to the Census, 32 percent (approximately 3,910) of the 12,224 households in Brea were spending 30 percent or more of their income on housing in 1990. Of these cost-burdened households, 47 percent were renters and 53 percent were owners. Overpayment varies widely by income, household type and size. Lower income (less than 80 percent of area MFI) households have an overpayment rate of 68 percent. Of the 1,940 lower income households overpaying, 1,328 are renters and 612 are owners. Like many cities in Southern California, the level of overpayment for senior renter households in Brea is high (70 percent), as exhibited in Table HE-9. Overpayment is also severe for large lower income households, with three-quarters of such households being cost-burdened.

Figure HE-7 shows housing overpayment statistics by tenure and income. Very low-income renters (less than 51 percent of area MFI) had a very high incidence of overpayment. Of the 977 renter households in this income category, 838 or 86 percent spent 30 percent or more of their income on housing. Overpayment is also severe for low-income households, for whom the rate of overpaying is 74 percent.

**Figure HE-7**  
**Overpaying Households by Tenure and Income – 1990**



Sources: 1990 Census; SCAG 1998 projections

## 5. Special Needs Groups

Certain segments of the population may have particular difficulties in finding decent, affordable housing because of their special needs and circumstances. These special needs groups, as defined by State housing element law, include the elderly, disabled persons, large households, female-headed households, farmworkers, and the homeless. Table HE-10 summarizes the numbers of households or persons in each of these special needs groups in the City.

**Table HE-10**  
**Special Needs Groups – 1990**

Special Need Group	Number of Persons or Households	% of Total Population or Total Number of Households
Elderly (65 years and older) households	1,879	15.4%
Living Alone	890	7.3%
Large (5 or more members) households	1,289	10.5%
Female-headed households	945	7.7%
With Children	433	3.5%
Disabled persons (16 years and over)	2,662	10.2%*
16 to 64 years old	1,664	6.4%
65 and over	998	3.8%
Farmworkers	190	1.0%**
Day Laborers	125	0.3%
Homeless persons	5	0.02%

Sources: 1990 Census; Community Services Department, City of Brea, 2000

\* Percent of total persons 16 years and older

\*\* Percent of total employed persons

### Elderly Persons

Most elderly households have special needs because of their relatively low, fixed incomes, physical disabilities/limitations, and dependency needs. Specifically, people aged 65 years and older have four main concerns:

- *Housing:* Many seniors live alone and tend to be renters.
- *Income:* People aged 65 and over are usually retired and living on a limited income.
- *Health care:* Seniors are more likely to be in ill-health and require hospital visits or stays.
- *Transportation:* Many of the elderly rely on public transportation. A significant number of seniors with disabilities who require alternative transportation (e.g. Handivan).

As shown in Table HE-10, about 15 percent of households in Brea were headed by elderly persons aged 65 and above in 1990. Of the 1,879 elderly households, 890 (47 percent) were elderly persons living alone. Elderly persons who live alone may have special needs for assistance with finance, home maintenance and repairs, and other routine activities. This is especially true of persons aged 75 years and older who are more likely to be the "frail elderly." Of the 2,889 persons aged 65 and above, about 6 percent lived below the poverty level.

According to the Brea Senior Policy/Issues Task Force, the City has a housing stock of 957 units for seniors as of 1998. This stock consists of 504 apartments (both at and below market rates), 429 mobile home units, and 24 units in residential care homes. Senior housing developments include Alterra Winwood Residential Manor (formerly Springhouse), Brea Woods, BREAL, Darnan Senior Apartments, Heritage Plaza, and Orange Villa. The Task Force states that more assisted living facilities are needed to meet the housing and supportive services needs of the City's growing senior population. At the present time, there is only one such facility in the City. Opened in 1995, Alterra Winwood has 98 units and provides the supportive services many seniors need, including meals, housekeeping, laundry services, and transportation. The City has a new senior apartment complex, Vintage Canyon, a 105-unit development which opened in 1999. Vintage Canyon offers many services to more active seniors with rental rates affordable to very low-income seniors.

The Brea Redevelopment Agency administers the Senior Subsidy Program to assist very low-income seniors with their housing expenditure. Began in 1989, the program currently provides 125 senior Breans with \$163 per month in rent subsidy. Participating seniors are allowed to use the subsidy towards rent at the dwelling of their choice.

The City has a Senior Center which serves as a meeting place and service center for seniors. Services or programs provided by the Center include a noon-time nutrition (lunch) program, bus service to the Center, health counseling, homemaker services, and recreation, education, and cultural activities. The City also has a "Meals on Wheels" program through which meals are delivered to senior residents with physical disabilities or limitations. A senior day care program for seniors requiring day time supervision has been created and is operated by Easter Seals.

## Disabled Persons

Physical and mental disabilities can hinder access to traditionally designed housing units (and other facilities) as well as potentially limit the ability to earn income. As shown in Table HE-10, 10.2 percent of Brea’s population aged 16 years and over were disabled in 1990. Disabled persons include those with (1) work disabilities, (2) mobility/self-care limitations, or (3) both work disabilities and mobility/self-care limitations. Of the 2,662 disabled residents in Brea, 998 or 37 percent were 65 years or older. This is further supporting evidence that the City needs to develop or assist in the development of more assisted living facilities for senior residents.

**Table HE-11  
Disability Status of Non-Institutionalized Persons**

Age \ Disability Status	With Work Disabilities only	With Mobility/Self-care Limitations	With both Work Disabilities and Mobility/Self-care Limitations	Total Disabled Persons
Persons 16 to 64 years old	1,002	385	277	1,664
Persons 65 years and over	405	89	504	998
Total	1,407	474	781	2,662

Source: 1990 Census

Most lower income disabled persons are likely to require housing assistance. Their housing need is further compounded by design and location requirements which can often be costly. For example, special needs of households with wheelchair-bound or semi-ambulatory individuals may require ramps, holding bars, special bathroom designs, wider doorways, lower cabinets, and other interior and exterior design features.

Housing opportunities for individuals with disabilities can be addressed through the provision of affordable, barrier-free housing. Currently, housing units that are accessible to the disabled are limited in supply. In addition to the development of new units, rehabilitation assistance can also be provided to disabled renters and homeowners to modify existing units to improve accessibility. In addition, accessible housing can be provided through senior housing developments.

## Large Households

Large households are defined as those with five or more members. The size of these households create special needs in that many cities lack adequately-size, affordable housing units for larger households. Large households, consisting mostly of families with a few children, often live in overcrowded conditions because of their lower incomes and the limited supply of large, affordable dwelling units.

According to the Census, there were 1,289 large households in Brea in 1990, of which 367 or 29 percent were renters. Over three-quarters of these large renter-households had housing problems which include overpayment, overcrowding, or living in sub-standard housing conditions.

Through its Community Center, the City provides youth and family services to meet the needs and concerns of families. Services include: parenting classes, counseling, tutoring for teens, youth employment, consumer education seminars, health screening, citizenship classes, information and referrals.

## **Female-headed Households**

Single-parent households often require special consideration and assistance as a result of their greater need for affordable housing and accessible day care, health care, and other supportive services. Female-headed households with children, in particular, tend to have lower incomes than other types of households. Because of their relatively low income, such households often have limited housing options and restricted access to supportive services.

According to the 1990 Census, there were 945 female-headed family households in the City, 433 (46 percent) of which had children under the age of 18. Female-headed families with children constituted about 3 percent of the City's households. Approximately 6 percent of the female-headed families were living below the poverty level in 1990. It is also important to point out that a total of 1,505 female householders were living alone; many of them are likely to be seniors.

## **Farmworkers**

Farmworkers are traditionally defined as persons whose primary incomes are earned through seasonal agricultural labor. They have special housing needs because of their relatively low income and the unstable nature of their job (having to move throughout the year from one harvest to the next). There were approximately 190 Brea residents employed in farming, forestry, and fishing occupations in 1990. These individuals accounted for only 1 percent of the City's total employed residents.

## **Day Laborers**

The City operates a Job Center where day laborers may be hired by potential employers. Approximately 125 individuals are registered at the Center, with between 40 and 50 laborers available for hire daily to perform a variety of work, including plumbing, landscaping, homemaking, painting and so forth. All of these registered laborers are Brea residents and the majority of them originated from Mexico. According to the Community Services Department, two of the laborers are homeless, but currently receive emergency assistance from the City.

## Homeless Persons

In 1990, 5 homeless persons were staying in emergency shelters in Brea, according to the Census. This is possibly an undercount. (It has been widely acknowledged that the methodology used in the Census to count or estimate the number of homeless was ineffective, thereby resulting in substantial undercounting of the homeless population.) The City Community Services Department provides homeless individuals and families with emergency assistance, including meal vouchers, transportation assistance, and referrals to social service agencies. According to the Department, 103 persons were assisted with emergency assistance in 1999, of which 50 received vouchers for lodging or rental assistance which enabled some residents to remain in their homes. The majority of those assisted were Caucasians between the ages of 30 to 45 years of age. Funding for the emergency assistance program originate from the City and three non-profit organizations: Active Christians Today, Brea Community Emergency Council and the Salvation Army.

There is currently a shelter for women and children in Brea. Operated by a Christian organization, this facility called the "Sheepfold" is located in a residential area in the central portion of the City. In addition, the City's Family Resource Center serves as an out-station for the Salvation Army. Table HE-12 lists the major homeless shelters and facilities in north Orange County. Aside from the facilities listed, the Community Services Department also refers homeless persons and families to the following shelters and organizations in other areas of Orange County (primarily in Santa Ana): Women's Transitional Living Center, Mercy House, YWCA, Sheepfold, and Human Options.

**Table HE-12**  
**Inventory of Homeless Services and Facilities - North Orange County**

<b>Organization</b>	<b>Beds and/or Services Provided</b>
City of Brea Community Services Department 695 Madison Way Brea, CA 92821 (714) 990-7776	Provides one-time only food, lodging, transportation and meals to homeless residents or those who are homeless in Brea.
Salvation Army Family Services 151 Viking Avenue Brea, CA 92621 (714) 225-1000	Provides emergency food, clothing, furniture vouchers, referrals, youth and adult programs, and shelter and utility assistance (when funds are available).
Sheepfold Central Brea	Provides shelter for women and children.
Anaheim Interfaith Shelter P.O. Box 528 Anaheim, CA 92815 (714) 774-8502	Provides 78 beds for families for up to 60 days. Supportive counseling and other services are provided.

**Table HE-12  
Inventory of Homeless Services and Facilities - North Orange County**

<b>Organization</b>	<b>Beds and/or Services Provided</b>
Ecumenical Service Alliance Service Center 311 W. South Street Anaheim, CA 92805 (714) 776-7510	Provides food bags, rental and utilities assistance, gas/bus vouchers, hotel vouchers, prescription assistance (when funds are available), job referrals, and employment counseling.
Fullerton Interfaith Emergency Services 244 E. Valencia, Room 16 Fullerton, CA 92634 (714) 680-3691	Services include food assistance, rental assistance, utilities (when funds are available), referrals, motel vouchers, and grocery vouchers.
H.I.S. House P.O. Box 1293 Placentia, CA 92670 (714) 993-5774	Provides 29 beds for families, individuals, and couples for up to 90 days. Services include job counseling and referrals, transitional housing, financial management, counseling, and life skill classes.
Lutheran Social Services 215 N. Lemon Street Fullerton, CA 92632 (714) 738-1058	Provides clothing, limited transportation, referrals, prescriptions, utilities and rental assistance (when funds are available), counseling and English as a Second Language (ESL) classes.
New Vista Shelter (Fullerton Interfaith Emergency Services) Fullerton, CA 92634 (714) 680-3691	Provides 48 beds for families with children for up to 75 days. Services include counseling, self-help classes, shelter programs, and life skill classes.

Sources: Orange County Homeless Issues Task Force, June 1999  
Orange County Rescue Mission, June 1999  
County of Orange Consolidated Plan 1995-1999, June 1995

## C. HOUSING STOCK CHARACTERISTICS

This section addresses various housing characteristics and conditions that affect the well-being of Brea residents. Housing factors analyzed include the following: housing stock and growth, tenure and vacancy rates, age and condition, and housing costs and affordability.

### 1. Housing Growth

Between 1980 and 1990, Brea's housing stock increased moderately (by nearly 13 percent), from 11,203 to 12,648 units.

In contrast, the County’s housing stock grew by 21 percent. Table HE-13 compares housing growth in Brea and selected cities in north Orange County. The growth in Brea’s housing stock in the 1980s is higher than that in Fullerton and La Habra, but significantly lower than that in Yorba Linda. Placentia, which experienced the same population growth (18 percent) as Brea in the 1980s, had a greater percentage increase in its housing stock than Brea.

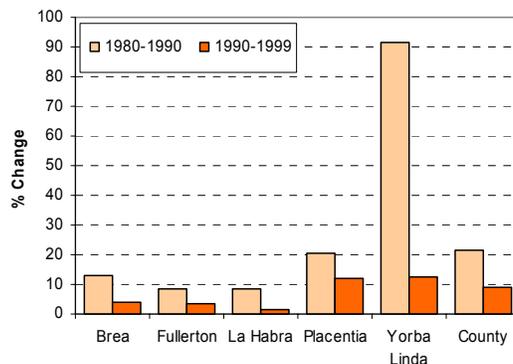
**Table HE-13  
Housing Growth: 1980s and 1990s**

Jurisdiction	1980	1990	1999	1980 to 1990 % Change	1990 to 1999 % Change
<b>Brea</b>	11,203	12,648	13,157	12.9%	4.0%
Fullerton	39,506	42,956	44,553	8.7%	3.7%
La Habra	17,194	18,670	18,964	8.6%	1.6%
Placentia	11,379	13,733	15,356	20.7%	11.8%
Yorba Linda	9,058	17,341	19,490	91.4%	12.4%
Orange County	721,514	875,072	954,882	21.3%	9.1%

Sources: 1980 and 1990 Census  
State Department of Finance, January 1999

In the 1990s, Brea’s housing growth is again greater than that in Fullerton and La Habra, but much lower than that in Placentia and Yorba Linda. The Redevelopment Agency played a key role in increasing the supply of housing in the City during this period. The majority of housing developed in the 1990s received some form of assistance from the Agency. Figure HE-8 compares housing growth in the 1980s and 1990s for Brea, the nearby cities, and the County.

**Figure HE-8  
Housing Growth - 1980s and 1990s**



Sources:  
1980 and 1990 Census  
State Department of Finance, January 1999

## 2. HOUSING TYPE AND TENURE

Table HE-14 provides a breakdown of the housing stock in Brea by unit type in 1990 and 1999. The composition of the housing stock in 1999 is essentially the same as that nine years ago. As was the case in 1990, single-family homes are the predominant housing type in 1999. Single-family homes make up 63 percent of the stock in 1999, while the multi-family share is 31 percent and mobilehomes comprise the remaining 7 percent. Specifically, 90 percent of the single-family homes are detached. The multi-family share of Brea's housing stock is relatively high in comparison to the nearby cities of Placentia and Yorba Linda, where the vast majority of housing is single-family homes and where apartments are limited in supply.

**Table HE-14  
Comparative Housing Unit Mix - 1990 and 1999**

Housing Type	1990		1999	
	No. of Units	% of Total	No. of Units	% of Total
Single-Family				
Detached	7,170	56.7%	7,440	56.5%
Attached	779	6.2%	779	5.9%
Total	7,949	62.8%	8,219	62.5%
Multi-Family				
2-4 Units	466	3.7%	504	3.8%
5+ Units	3,339	26.4%	3,540	26.9%
Total	3,805	30.1%	4,044	30.7%
Mobile Homes	894	7.1%	894	6.8%
Total Housing Units	12,648	100.0%	13,157	100.0%
Total Occupied	12,224	100.0%	12,715	--
Owner-Occupied	7,764	63.5%	N/A	N/A
Renter-Occupied	4,460	36.5%	N/A	N/A
Vacancy Rate (%)		3.35%		3.36%

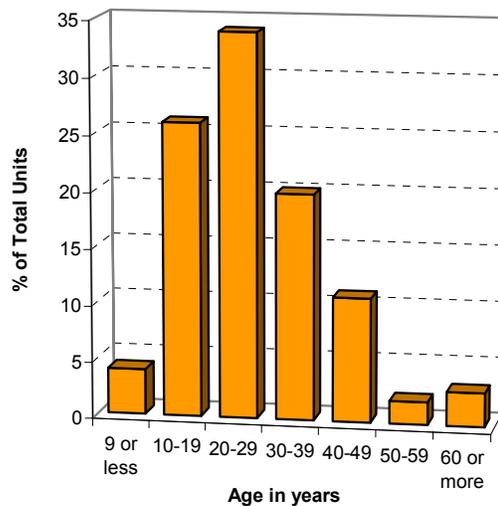
Sources: 1990 Census  
State Department of Finance, January 1999

In terms of tenure, nearly two-thirds of the units were owner-occupied in 1990, while about one third were renter-occupied. Given that the housing stock composition has not changed much since 1980, it is likely that the majority of the housing units in Brea is still predominantly owner-occupied. The City’s overall vacancy rate was 3.4 percent in 1990. Specifically, the vacancy rates for ownership and rental units were 0.8 percent and 5 percent, respectively. While this reflects a healthy vacancy rate for rentals, the low ownership vacancy rate indicates a tight for-sale housing market.

### 3. Age and Condition of Housing Stock

The age of housing is commonly used as a measure of when housing may begin to require repairs. Figure HE-9 shows the age distribution of the housing stock in Brea. In general, housing units over 30 years old are likely to have rehabilitation needs, including new roofing, foundation work, new plumbing, and so on. Approximately one-third of the housing stock in the City is 30 years or older (built before 1970), reflecting the Brea’s tremendous growth in the 1960s and indicating that some homes are likely to be in need of rehabilitation. The dramatic slowdown in housing production is also reflected in Figure HE-9; units 9 years old or younger (constructed since 1990) account for only about 4 percent of Brea’s housing stock.

**Figure HE-9  
Age Distribution of the Housing Stock – 1999**



Sources: 1990 Census  
State Department of Finance, January 1999

The majority of Brea's housing stock is in good condition. However, like most cities, the City does have a number of areas that are showing early signs of deterioration. To address these conditions, the City prepared a *Neighborhood Enhancement Plan* in 1998. This plan identifies six "Focus Neighborhoods" with signs of physical deterioration and other related problems (refer to Figure HE-10). Specifically, these neighborhoods exhibit the following problem conditions: deferred maintenance, dilapidation (such as peeling paint, wood rot, worn roofs), cracked asphalt driveways, and building code violations such as illegal garage conversions. Table HE-15 summarizes the characteristics and physical problem conditions of the six neighborhoods.

Neighborhoods where physical problem conditions are most severe (neighborhoods 2, 3, and 4) share two common characteristics: many of the households are renters and over 10 percent of the households are overcrowded (Table HE-15). To address the physical problems (as well as the economic and social conditions) found in the focus neighborhoods, new programs are proposed, including (but not limited to): assistance to non-profit organizations for the purchase and rehabilitation of multi-family units, an apartment inspection program, and a rehabilitation loan program for single- and multi-family properties in the focus neighborhoods. These programs are designed to build upon and complement the City's existing housing programs, including: the Deferred Payment Loans and Low Interest Loans for rehabilitating owner-occupied housing; Investor-Owner Loans for rehabilitating rental units; and Grants for rehabilitating mobilehomes.



Source: *Neighborhood Enhancement Plan, City of Brea (September 1998)*

1. Acacia Apartments
2. Walnut, South of Imperial Highway
3. South of Laurel Walk East Imperial Apartments
4. Pepperwood/Randolph Birch Street Apartments
5. Olive Avenue/57 Freeway
6. Tamarack/Mariposa



Figure HE-10  
Neighborhood Enhancement Areas

**Table HE-15  
Neighborhood Enhancement Areas**

<b>Neighborhood</b>	<b>Characteristics</b>	<b>Problem Conditions</b>
#1 Acacia Apartments	<ul style="list-style-type: none"> <li>- Land use: multi-family residential</li> <li>- 32 multi-family dwelling units</li> <li>- 11% of renter households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Old shake roofs (potential fire hazard)</li> <li>- Missing/bent window screens</li> <li>- Deteriorated carports</li> <li>- Deterioration due to overcrowding</li> <li>- Cracked asphalt driveways</li> </ul>
#2 Walnut, south of Imperial Highway	<ul style="list-style-type: none"> <li>- Land use: primarily residential</li> <li>- 156 single-family and 199 multi-family units</li> <li>- 33% owner-occupied; 67% renter-occupied</li> <li>- 18% of households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Deferred maintenance</li> <li>- Garage conversions</li> <li>- Deteriorated fencing</li> <li>- Deteriorated retaining walls</li> <li>- Cracked asphalt driveways</li> </ul>
#3 South of Laurel Walk/ East Imperial Apartments	<ul style="list-style-type: none"> <li>- Land use: primarily residential</li> <li>- 83 single-family and 39 multi-family units</li> <li>- 18% owner-occupied; 82% renter-occupied</li> <li>- 12% of households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Deferred/no maintenance</li> <li>- Deteriorating rental homes/apartments due to overcrowding</li> <li>- Deteriorated/unattractive fencing</li> </ul>
#4 Pepperwood/Randolph/ Birch Street Apartments	<ul style="list-style-type: none"> <li>- Land use: residential only</li> <li>- 177 single-family and 199 multi-family units</li> <li>- 56% owner-occupied; 46% renter-occupied</li> <li>- 12% of households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Lack of maintenance</li> <li>- Damaged exterior building material</li> <li>- Deteriorated fencing</li> <li>- Deteriorated roofs and eaves</li> <li>- Cracked asphalt driveways</li> </ul>
#5 Olive Avenue/57 Freeway	<ul style="list-style-type: none"> <li>- Land use: residential only</li> <li>- 69 single-family units</li> <li>- 4% of households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Chipped/peeling paint</li> <li>- Deteriorated roofing</li> <li>- Cracked asphalt driveways</li> </ul>
#6 Tamarack/Mariposa	<ul style="list-style-type: none"> <li>- Land use: residential only</li> <li>- 234 single-family units</li> <li>- 0.1% of households overcrowded</li> </ul>	<ul style="list-style-type: none"> <li>- Deferred maintenance (structures needing paint)</li> <li>- Deteriorated garage doors</li> <li>- Cracked asphalt driveways</li> </ul>

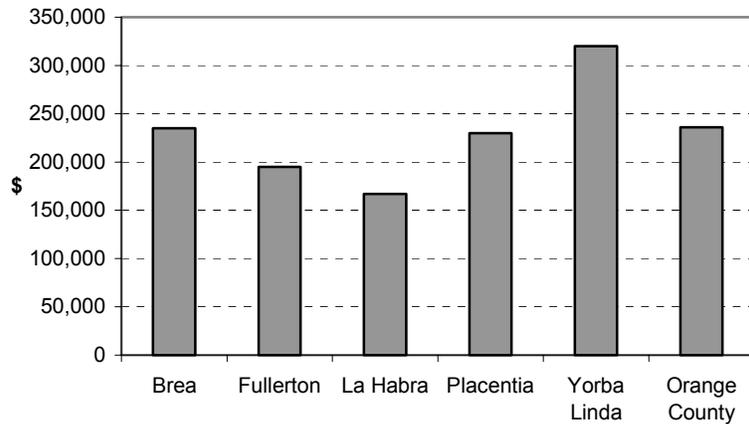
Source: Neighborhood Enhancement Plan, City of Brea, September 1998

## 4. Housing Costs

### Housing Prices

According to the 1990 Census, the median value of a housing unit in Brea was \$264,000, about 4 percent higher than the County median of \$252,700. More recent data from the California Association of Realtors (CAR) indicate that Brea has the second highest (behind Yorba Linda) median home price among the five cities in north Orange County, as shown in Figure HE-11. According to CAR, the median sales price of a home in Brea in the first quarter of 1999 was \$235,000.

**Figure HE-11**  
**Median Sales Prices – 1999**



Sources: California Association of Realtors, 1st Quarter 1999

Detailed housing market sales data for Brea are provided in Tables HE-16 and HE-17. The median sale prices of single-family homes and multi-family units are \$231,750 and \$113,500, respectively. The sales volume of single-family homes is fairly high between June 1998 and May 1999. Specifically, single-family homes with three to four bedrooms have been the most popular with home buyers: 325 or 79 percent of the 410 homes sold have three to four bedrooms. The multi-family housing market has not been particularly active, with only 38 sales between June 1998 and May 1999. Of the multi-family units sold, 22 or 58 percent have two bedrooms. Only five three-bedroom units were sold, likely indicating that these units are limited in supply.

**Table HE-16  
Prices of Single-Family Homes - June 1998 to May 1999**

<b>Number of Bedrooms</b>	<b>Median Price</b>	<b>Average Price</b>	<b>Price Range</b>	<b>Number of Units Sold</b>
1	\$145,000	\$145,000	n/a	1
2	\$161,000	\$177,964	\$112,000 - \$429,500	55
3	\$215,000	\$210,635	\$64,500 - \$432,000	170
4	\$270,000	\$275,304	\$142,000 - \$675,000	155
5	\$300,000	\$297,065	\$224,000 - \$396,000	23
6 or more	\$322,000	\$312,125	\$264,500 - \$340,000	4
<b>Totals</b>	<b>\$231,750</b>	<b>\$241,604</b>	<b>\$64,500 - \$675,000</b>	<b>410</b>

Source: California Market Data Cooperatives, June 1998 to May 1999

**Table HE-17  
Prices of Multi-Family Homes/Condominiums - June 1998 to May 1999**

<b>Number of Bedrooms</b>	<b>Median Price</b>	<b>Average Price</b>	<b>Price Range</b>	<b>Number of Units Sold</b>
1	\$85,000	\$83,727	\$59,500 - \$96,000	11
2	\$129,000	\$127,750	\$90,000 - \$168,500	22
3	\$175,000	\$176,700	\$164,000 - \$190,000	5
<b>Totals</b>	<b>\$113,500</b>	<b>\$135,673</b>	<b>\$59,500 - \$190,000</b>	<b>38</b>

Source: California Market Data Cooperatives, June 1998 to May 1999

## Housing Rental Rates

The 1990 median contract rent for a housing unit in the City was \$701, about 4 percent lower than the County median of \$728. Comparing to the nearby cities, Brea's median rent was the "median": it was higher than rents in Fullerton and La Habra, but lower than those in Placentia and Yorba Linda.

Table HE-18 is a comparison of current rental rates in Brea, Fullerton, and Placentia. Apartment rents in Brea are slightly higher than those in Fullerton and Placentia. In Brea, rents range from \$650 to \$895 for a one-bedroom apartment and from \$750 to \$1,245 for a two-bedroom apartment. There appears to be a shortage of three-bedroom apartments in the City, given the limited availability of such units listed for rent. There are three-bedroom condominiums or townhomes available for rent, but these units are rather limited in supply and costly to rent.

**Table HE-18  
Rental Rates - June 2000**

Housing Type and Number of Bedrooms	Range of Rental Costs		
	Brea	Fullerton	Placentia
<b>Apartments</b>			
1 Bedroom	\$650 - \$895	\$515 - \$845	\$575 - \$870
2 Bedrooms	\$750 - \$1,275	\$600 - \$1,200	\$645 - \$1,200
3 Bedrooms	n/a	\$850 - \$1,047	\$895 - \$1,400
<b>Condominiums &amp; Townhouses</b>			
2 Bedrooms	\$875 - \$1,230	\$950	\$1,100
3 Bedrooms	\$1,695	\$970	\$1,400 - \$1,550

Sources: SpringStreet.com, June 2000  
Apartments.com, June 2000  
Apartment List, City of Brea Community Center, May 2000

## Housing Costs and Affordability

Housing affordability can be inferred by comparing the cost of renting or owning a home in Brea with the maximum affordable housing costs to households of different income levels. Taken together, this information can reveal who can afford what size and type of housing as well as indicate the type of households that would likely experience overcrowding or overpayment.

The federal Department of Housing and Urban Development (HUD) conducts annual household income surveys for metropolitan areas across the country, including Orange County. These income surveys are adjusted for differences in the type and size of a family. HUD uses these income levels to determine the maximum amount that a household could pay for housing and their eligibility for federal housing assistance. According to HUD, the 1999 Area Median Family Income (MFI) for a four-person household in Orange County is \$68,300.

Tables HE-19 and HE-20 below provide the annual income for very low, low, and moderate-income households by the maximum affordable housing payment based on the federal standard of 30 percent of gross household income. Standard housing costs for utilities, taxes, and property insurance are also shown. From these income and housing cost information, the maximum affordable home prices and rents are determined.

**Table HE-19  
Affordable Homeownership Costs by Income Category  
Orange County – 1999**

<b>Income Group (defined as % of County MFI)</b>	<b>Median Income (4- person household)</b>	<b>Monthly Affordable Housing Cost</b>	<b>Property Taxes, Utilities, Home- ownership Insurance</b>	<b>Affordable Mortgage Payment</b>	<b>Maximum Affordable 30-Year Mortgage</b>	<b>Maximum Affordable Home Price</b>
Very Low (0-50% MFI)	\$34,150	\$854	\$350	\$504	\$72,082	\$79,300
Low (51-80% MFI)	\$47,800	\$1,195	\$400	\$795	\$113,701	\$125,100
Moderate (81-120% MFI)	\$81,950	\$2,049	\$400	\$1,649	\$235,841	\$259,400

Note: Calculation of affordable home sales price based on downpayment of 10 percent, annual interest of 7.5 percent, 30-year mortgage, and monthly payment of 30 percent of gross household Income. Median Family Income (MFI) for lower income households in Orange County are capped at the National Median Family Income.

**Table HE-20  
Affordable Rents by Income Category  
Orange County – 1999**

<b>Income Group (defined as % of Area MFI)</b>	<b>Median Income (4-person household)</b>	<b>Monthly Affordable Rent</b>	<b>Utility Allowance</b>	<b>Affordable Monthly Payment</b>
Very Low (0-50% MFI)	\$34,150	\$854	\$50	\$804
Low (51-80% MFI)	\$47,800	\$1,195	\$50	\$1,145
Moderate (81-120% MFI)	\$81,950	\$2,049	\$50	\$1,999

Note: Calculation of affordable rent is based on a monthly payment of 30 percent of gross household income. Median Family Income (MFI) limits for lower income

Given that the maximum affordable home price for a **very low-income** (0 to 50 percent of MFI) four-person household is only \$79,300, the household cannot find an affordable home of sufficient size in Brea. In terms of the rental market, there are two-bedroom apartments affordable to very low-income households with monthly rents below \$800. However, the supply of these units is extremely limited. Market rents in newer apartment complexes are well above levels affordable to very low-income households, indicating a need to provide some form of subsidy or affordable housing incentives to bring rents down to affordable levels.

The best homeownership option for low-income (51 to 80 percent of MFI) households is a two-bedroom condominium. However, this type of unit appears to be limited in supply: only 22 two-bedroom multi-family homes or condominiums were sold between June 1998 and May 1999. With an affordable monthly payment of \$1,145, a low-income household should be able to rent a two-bedroom apartment or condominium in Brea. There are very few three-bedroom apartments in the City and those available are most likely to be too high-priced for low-income households. And while three-bedroom condominiums are in greater supply, rent levels are beyond the level of affordability of low-income households.

A **moderate-income** (81 to 120 percent of MFI) household should be able to afford to purchase a home of adequate size in Brea. With a maximum affordable home price at \$259,400, moderate-income households can afford the median-priced three-bedroom single-family home as well as any condominium in the City. However, the required downpayment and/or closing costs can be obstacles to home purchase for moderate-income households. In terms of rental options, moderate-income households can afford the full range of rental units available in Brea.

## 5. Assisted Housing At Risk of Conversion

State law requires the Housing Element to identify, analyze and propose programs to preserve housing units that are currently restricted to low-income housing use and will become unrestricted and possibly lost as low-income housing. Based on a review of federal and State subsidized housing inventories, there are currently no "Assisted Housing Projects" in Brea, as defined by State Government Code Section 65583(a)(8). There are, however, City-assisted multi-family rental housing in Brea. As illustrated by Table HE-21, the Redevelopment Agency has played an active role in the provision of affordable housing. The Agency has subsidized a total of nine multi-family rental projects, providing 107 very low-income, 86 low-income, and 94 moderate-income units. These developments have relatively long affordability periods and none is at risk of conversion to market rate for the next ten years.

**Table HE-21  
City-Assisted Multi-family Rental Housing Projects**

<b>Project Name and Address</b>	<b>Number of Affordable Units</b>	<b>Year Affordability Period Ends</b>	<b>Source of Assistance</b>	<b>Tenant Type</b>
Brea Woods Senior Apartments 195 W. Central Avenue (151 units total)	36 - Low <u>39</u> - Mod 75 - Total	2014	Density Bonus	Seniors
BREAL 111 N. Orange Avenue (30 units total)	30 - Very Low	2023	Redevelopment Agency - Housing Set-Aside	Seniors
Civic Center Apartments 647-669 Birch Street (30 units total)	14 - Very Low <u>2</u> - Low 16 - Total	2029	Redevelopment Agency - Housing Set-Aside; County HOME funds	Families
Darnan 212 S. Orange Avenue (28 units total)	28 - Low/Mod	2019	Redevelopment Agency - Housing Set-Aside	Seniors
Foursquare (9 units total)	5 - Very Low <u>4</u> - Low 9 - Total	2038	Redevelopment Agency - Redevelopment funds (non-housing set-aside)	Families
Imperial Terrace 430 W. Imperial Highway (37 units total)	11 - Very Low <u>7</u> - Low 18 - Total	2026	Redevelopment Agency - Housing Set-Aside; HOME funds	Families
Orange Villa 137 N. Orange Avenue (31 units total)	9 - Low	2019	Redevelopment Agency; Density Bonus	Seniors
Tamarack Pointe Villas 330 W. Central Avenue (48 units total)	3 - Low <u>2</u> - Mod 5 - Total	2026	Redevelopment Agency - Housing Set-Aside; Inclusionary Ordinance	Families
Town & Country 800 S. Brea Boulevard (122 units total)	25 - Low <u>25</u> - Mod 50 - Total	2024	Redevelopment Agency - Housing Set-Aside; tax- exempt revenue bonds	Families
Vintage Canyon Apartments 955 N. Brea Boulevard (105 units total)	105 - Very Low	2029	Low-income housing tax credits; County HOME funds; density bonus; parking reduction	Seniors
Walnut Village 523 S. Walnut Avenue (47 units total)	47 - Very Low	2038	Redevelopment Agency - Housing Set-Aside; Low- income housing tax credit proceeds; County HOME funds	Families

Source: Affordable Housing Monitoring Report, Redevelopment Services Department, City of Brea, 2000

### 3. SUMMARY OF HOUSING NEEDS



This section of the Housing Element is a summary of the major housing need categories in terms of the various income groups as defined by Federal and State law. It includes the City's share of regional housing need as provided by the Southern California Association of Governments' (SCAG) Regional Housing Needs Assessment (RHNA). As summarized in Table HE-22, the areas of greatest housing assistance need in Brea include the following:

#### 1. Households Overpaying for Housing

About 31 percent of all households in Brea spent 30 percent or more of their income on housing. Of these 3,822 households, 47 percent (1,796) were renters and 53 percent (2,026) were owners. Very low-income households experienced a very high incidence of overpayment: 86 percent of these households overpaid for their housing. About 160 large renter-families and 423 elderly households overpaid for housing in 1990.

#### 2. Special Needs Groups

Certain segments of the population may experience more difficulties finding decent, affordable housing because of their special needs and circumstances. The Housing Needs Assessment documents the following special needs groups:

- 1,879 households headed by elderly persons, 47 percent of which were elderly persons living alone;
- 2,662 persons aged 16 and above with physical disabilities, of which 998 (or 37 percent) were 65 years or older;
- 945 female-headed households, 46 percent of which had children (under 18 years old). Approximately 6 percent of female-headed households were living below the poverty level;
- 1,289 large (5 or more members) households, comprising 11 percent of Brea households;
- About 103 persons received emergency assistance from the City's Community Services Department in 1999.

### 3. Age and Condition of Housing Stock

As of 1999, about 36 percent of the housing stock in Brea is 30 years or older, when most housing units typically begin to have repair and/or rehabilitation needs.

A number of neighborhoods are showing signs of deterioration. Six "Focus Neighborhoods" with signs of physical deterioration and other related problems have been identified by the City and are the focus of Brea's Neighborhood Enhancement Program. Specifically, these neighborhoods exhibit the following problem conditions: deferred maintenance, dilapidation (such as peeling paint, wood rot, worn roofs), cracked asphalt driveways, and building code violations such as illegal garage conversions.

### 4. Housing Costs and Affordability

Recent sales data indicate that the median sale prices of single-family homes and condominiums in Brea are \$231,750 and \$113,500, respectively. Given these relatively high prices, very low- and low-income households have very few home ownership opportunities in the City. Moderate-income households have a better chance of purchasing adequately-sized homes: they can afford a three-bedroom single-family home or condominium.

In terms of rentals, there are two- to three-bedroom apartments affordable to very low- and low-income households. However, the supply of these units is limited, while the demand for them is very high. A moderate-income household is able to rent the largest unit available on the market: a three-bedroom condominium. It should be noted, however, that these units are rather scarce.

### 5. Overcrowded Households

About 5 percent (629) of the total households lived in overcrowded conditions (more than 1.01 persons per room). Like most cities, overcrowding among renter-households in Brea was more prevalent than among owner-households. Only 2.4 percent (189) of the owner-households were overcrowded, comparing to nearly 10 percent (440) of renter-occupied households.

## 6. Regional Housing Needs Assessment (RHNA)

Regional growth needs are defined as the number of units that would have to be added in each jurisdiction to accommodate the forecasted growth in the number of households by different income categories between January 1, 1998 and June 30, 2005, as well as the number of units that would have to be compensated for anticipated demolitions and changes to achieve an “ideal” vacancy rate. SCAG recently released the RHNA figures for the 1998-2005 period. The RHNA for Brea for this period is 1,053 additional dwelling units. This total growth need is allocated to four income categories: very low-income, low-income, moderate-income, and upper-income. Table HE-22 shows the household growth divided into the various income categories.

**Table HE-22  
Summary of Existing and Projected Housing Needs**

<b>Overpaying Households</b>		<b>Special Needs Groups</b>	
Total	3,822	Elderly Households	1,879
Renter	1,796	Elderly Living Alone	890
Owner	2,026	Disabled Persons	2,662
		Female Headed Households with Children	945 433
		Large Households	1,285
		Renter	344
		Owner	941
<b>Overcrowded Households</b>		<b>Household Growth: January 1998 - June 2005</b>	
Total	629	Total Construction Need	1,052
Renter	440	Very Low Income	203
Owner	189	Low Income	136
		Moderate Income	212
		Upper Income	502

Sources: 1990 Census  
Community Services Department, City of Brea, 1999  
SCAG Regional Housing Needs Assessment, 1999

## 4. HOUSING CONSTRAINTS



The provision of adequate and affordable housing is constrained by a number of factors. This section assesses the various market, governmental, and environmental factors that may serve as a potential constraint to housing development and improvement in Brea.

### A. MARKET CONSTRAINTS

#### 1. Construction Costs

The cost of building materials is a major cost associated with constructing a new housing unit. In the current Southern California real estate market, the cost of building materials is the single factor having the most impact on the cost of building a new house. These costs are estimated to account for 40 to 50 percent of the sales prices of a new home. Typical residential construction costs range from approximately \$60 to \$80 per square foot for a single-family home and from \$40 to \$75 per square foot for multi-family construction. According to the 1999 Orange County Affordable Housing Strategy, residential construction costs have increased dramatically over the last decade “to the point where a typical two-bedroom apartment costs around \$150,000 to develop” (p. 58).

A reduction in amenities and the quality of building materials (above a minimum acceptability for health, safety, and adequate performance) could result in lower sales prices. In addition, prefabricated factory-built housing may provide for lower priced housing by reducing construction and labor costs. Another factor related to construction costs is the number of units built at one time. As the number increases, overall costs generally decrease as builders are able to take advantage of the benefits of economies of scale. This type of cost reduction is of particular benefit when density bonuses are used for the provision of affordable housing.

#### 2. Land Costs

A key component of the total cost of housing is the price of raw land and any necessary improvements. The diminishing supply of land available for residential construction combined with a fairly

high demand for such development has served to keep the cost of land relatively high, particularly in Orange County cities. High and rapidly increasing land costs in the County have resulted in homebuilders developing more and more high-priced homes in order to preserve profits, according to the County's Housing Strategy. In Brea, residential land costs vary from \$11 to \$17 per square foot, depending on the location, zoning, and size of the property.

### 3. Availability of Mortgage and Rehabilitation Financing

The availability of financing affects a person's ability to purchase or improve a home. Interest rates are determined by national policies and economic conditions, and local governments can do little to affect these rates. Jurisdictions can, however, offer interest rate write-downs to extend home purchasing opportunities to a broader economic segment of the population. In addition, government insured loan programs may be available to reduce mortgage downpayment requirements.

Under the Home Mortgage Disclosure Act (HMDA), lending institutions are required to disclose information on the disposition of loan applications and the income, gender, and race of loan applicants. As shown in Table HE-23, of the 789 applications for conventional mortgage loans to purchase homes in Brea in 1997, about 70 percent were originated (approved by lenders and accepted by applicants). The overall denial rate was 14 percent, while 16 percent of the applications were withdrawn, closed for incompleteness, or not accepted by the applicants. As expected, the denial rate for low-income applicants was the highest among the three income groups at 23 percent. However, it is interesting to note that 20 percent of applicants for conventional mortgages in Brea were low-income. This likely reflects the fact that the City operates a first-time homebuyer program, which provides silent second mortgages (considered "conventional" loans) to low- and moderate-income households.

**Table HE-23  
Disposition of Home Purchase Loans**

Applicant Income	Conventional Loans				Government-Assisted Loans			
	Total Applications	% Originated	% Denied	% Other*	Total Applications	% Originated	% Denied	% Other*
Low-Income (< 80% MFI)	156	55.1%	23.1%	21.8%	25	72.0%	20.0%	8.0%
Moderate-Income (80-119% MFI)	270	68.1%	15.2%	16.7%	24	83.3%	8.3%	8.4%
Upper Income (=/> 120% MFI)	334	79.3%	9.0%	11.7%	8	87.5%	12.5%	0.0%
Not Available	29	51.7%	17.2%	31.1%	4	75.0%	0.0%	25.0%
Total	789	69.7%	14.2%	16.1%	61	78.7%	13.1%	8.2%

Source: Home Mortgage Disclosure Act (HMDA) data, 1997

\* "Other" includes applications approved but not accepted, files closed for incompleteness, and applications withdrawn.

Brea has a relatively small pool of government-assisted loans, such as FHA, VA and FmHA, given the sales price limits under these programs. Among the 61 applications for government-assisted loans, the denial rate for low-income applicant households was also higher than that for moderate- and upper income applicants. However, it should be noted that low-income households have a much better chance of getting a government-assisted loan than a conventional loan. Over 70 percent of the applications for government-assisted loans by low-income households were originated, comparing to just 55 percent for conventional loans.

Table HE-24 shows the disposition of conventional and government-assisted home improvement loans. Compared to mortgage loans, home improvement loans were much more difficult to secure, as is typical in most jurisdictions. The overall approval rates for conventional and government-assisted home improvement loans were very low: only 50 percent and 33 percent, respectively. Even for government-assisted loans, the denial rate for low-income applicants was extremely high (70 percent). The relatively high denial rates for low- and moderate-income households support the importance of City sponsored rehabilitation assistance for these households. Increasing the availability of financing sources for households to improve or rehabilitate their homes is important to improving the overall quality of housing in Brea.

**Table HE-24  
Disposition of Home Improvement Loans**

Applicant Income	Conventional Loans				Government-Assisted Loans			
	Total Applications	% Originated	% Denied	% Other*	Total Applications	% Originated	% Denied	% Other*
Low-Income (< 80% MFI)	35	42.9%	51.4%	5.7%	10	20.0%	70.0%	10.0%
Moderate-Income (80-119% MFI)	23	34.8%	34.8%	30.4%	11	36.4%	36.4%	27.2%
Upper Income (>=/ > 120% MFI)	71	60.6%	23.9%	15.5%	22	36.4%	31.8%	31.8%
Not Available	2	0.0%	100.0%	0.0%	3	33.3%	33.3%	33.4%
<b>Total</b>	<b>131</b>	<b>50.4%</b>	<b>34.4%</b>	<b>15.2%</b>	<b>46</b>	<b>32.6%</b>	<b>46.3%</b>	<b>21.1%</b>

Source: Home Mortgage Disclosure Act (HMDA) data, 1997

\* "Other" includes applications approved but not accepted, files closed for incompleteness, and applications withdrawn.

## B. GOVERNMENTAL CONSTRAINTS

Aside from market factors, actions by the City can have an impact on the price and availability of housing. Land use controls, site improvement requirements, building codes, fees, and other local programs intended to improve the overall quality of housing may serve as potential constraints to the development and improvement of housing.

### 1. Land Use Controls

The Land Use Element of the General Plan sets forth the City's policies for guiding local development and growth. These policies, together with existing zoning regulations, establish the amount and distribution of land uses within the City. The General Plan provides for five residential land use categories, as shown in Table HE-25.

**Table HE-25  
Residential Land Use Designations**

<b>Residential Land Use Category</b>	<b>Corresponding Zoning District(s)</b>	<b>Dwelling Units per Acre</b>	<b>Description</b>
Rural	R1-H and RH	0.2	Very low density development in areas where the topography is generally steep with limited area suitable for development.
Hillside	R1-H and RH	1.0 - 2.0	Low density development in hillside areas, where grading is minimized to retain slope stability and to protect scenic resources.
Low Density	R-1	1.0 - 6.0	Single family homes developed on conventional lots.
Medium Density	R-1 (5,000) and R-2	6.1 - 12.0	Multiple family housing including existing mobile home parks, condominiums and apartments.
High Density	R-3 and R-3-I	12.1 - 24.89	Multiple family housing consisting of condominiums and apartment developments.

Source: Land Use Element, City of Brea General Plan, June 1986

In addition to the base zone districts identified above, the City has adopted several specific plans for areas requiring focused planning efforts, including Olinda Ranch, Carbon Canyon, Towne Plaza, and Birch Hills. (Additionally, the proposed Tomlinson specific plan is currently under consideration.) Each of these specific plans has its own unique zoning districts and set of development standards tailored to guide development in that particular area. These specific plans fall within the overall development framework defined by the Land Use Element.

## **Growth Management**

In response to the passage of Measure M in 1990, the City adopted a Growth Management Element as part of its General Plan. The Element aims to ensure that growth and development are balanced with the City's ability to provide an adequate circulation system pursuant to the requirements of the Revised Traffic Improvement and Growth Management Ordinance, also known as Measure M. The Element does not serve as a constraint to housing development. Unlike growth control measures enacted by some jurisdictions, Brea's Growth Management Element does not limit housing growth to an established maximum level.

## 2. Residential Development Standards

Brea's Zoning Ordinance provides for the following residential zones:

- Single Family Residential - Hillside (R1-H)
- Hillside Residential (RH)
- Single Family Residential (R-1)
- Single Family Residential (R-1 (5,000))
- Multiple Family Residential (R-2)
- Multiple Family Residential (R-3 and R-3-I)

The development standards for these zones are summarized in Table HE-26. Brea's standards for dwelling unit size establish opportunities for a diverse housing mix. These standards range from 450 square feet for a studio unit in the R-3 district to 2,500 square feet (for a single-story unit) in the RH zone. Small lot single-family subdivisions as well as townhomes are provided for in the R-1 (5,000) zone, with minimum unit sizes of 800 square feet. These standards, when viewed in the context of the entire community, offer a positive balance of home sizes.

Apart from the maximum density permitted by the Zoning Ordinance, two standards that may affect the number of units that can be constructed on a given lot are maximum lot coverage and height limits. The Zoning Ordinance restricts the amount of lot area that may be covered by development to 50 percent in R-2 and 60 percent in R-3 zones. Three-story (35') building heights are permitted in R-2 and R-3 zones, with additional height of up to 75' permitted in R-3 zones based on distance from R-1, R-2 of RH zone districts. The maximum building envelope achievable under the lot coverage and height requirements does not serve as a constraint to achieving close to the maximum permitted densities in the City's R-2 and R-3 zone districts. For example, the City has a current development application for 48 units on a 2.07 acre R-3 parcel, translating to 23.8 units per acre, or 96 percent of the maximum permitted density.

**Table HE-26  
Residential Development Standards**

<b>Standards</b>	<b>R1-H</b>	<b>RH</b>	<b>R-1</b>	<b>R-1 (5,000)</b>	<b>R-2</b>	<b>R-3</b>
<b>Primary Housing Type</b>	Single family dwellings	Cluster-type and estate- type single family dwellings	Single family dwellings	Single family dwellings, townhomes	Low-density multiple family dwellings	High-density multiple family dwellings, i.e. apartments & condominiums
<b>Minimum Lot Area</b>	Varies, depending on slope & lot frontage	<i>Cluster-type</i> Corner lots-7,000 sq.ft. Reverse corner lots-7,500 sq.ft. <i>Estate-type</i> 20,000 sq.ft.	7,200 sq.ft.	5,000 sq.ft.	9,000 sq.ft.	R-3 - 10,000 sq.ft. R-3-l - 7,000 sq.ft.
<b>Maximum Building Coverage</b>	None	None	35%	50%	50%	60%
<b>Minimum Dwelling Unit Size</b>	1,800 sq.ft.	<i>Cluster-type</i> Single story- 2,500 sq.ft. Split level-1,800 sq.ft. <i>Estate-type</i> Single story- 2,500 sq.ft. Split level 2,000 sq.ft.	1,000 sq.ft.	800 sq.ft.	800 sq.ft.	Bach - 450 sq.ft. 1 BR - 650 sq.ft. 2 BR - 800 sq.ft.
<b>Maximum Building Height</b>	30 ft.	None	18 - 30 ft.	35 ft.	35 ft.	35 - 75 ft.
<b>Density - Maximum Dwelling Units</b>	1.0 unit/lot*	1.0 unit/acre (average unit density)	1.0 unit/lot (max. 6 units/acre)	1.0 unit/lot (max. 8.7 units/acre)	12.0 units/acre	24.9 units/acre

Source: Zoning Ordinance, City of Brea, December 1996

\* Greater than 1.0 unit per lot permitted where planned unit techniques are employed.

The Zoning Ordinance calculates parking requirements by unit type, and on a per bedroom basis for multi-family units, as illustrated in Table HE-27. Carports for multi-family units are permitted, serving to reduce development costs in contrast to the provision of separate garages. In addition, to facilitate the development of affordable housing and to reflect the actual parking demands created by these developments, the City has granted parking reductions for senior housing, loft housing and affordable housing in general.

**Table HE-27  
Parking Requirements**

<b>Unit Type</b>	<b>Parking Requirement</b>
Single-family	2 covered spaces for each unit
Single-family: attached	2 covered spaces for each unit, plus 0.5 uncovered spaces for each dwelling unit
Two or more units on a building site/ lot	Bachelor unit = 1.5 spaces per unit* 1-Bedroom units = 1.75 spaces per unit* 2-Bedroom units = 2 spaces per unit* 3-Bedroom or more dwelling units = 2.5 spaces per unit**  * of which 1 space shall be located within a garage or 3-sided carport ** of which 2 spaces must be covered for each dwelling unit, plus 0.5 off-street parking spaces must be provided for each bedroom in excess of 3.
Five or more units on a building site/ lot	In addition to the required number of spaces stated above for multi-family units: 0.2 guest parking space shall be provided for each dwelling unit. A maximum of 25 percent of the required uncovered spaces may be of compact car size, provided such spaces are clearly and individually marked.

Source: Zoning Ordinance, City of Brea, December 1996

In order to specifically facilitate the development of affordable housing, the City utilizes two primary regulatory mechanisms: Development Agreements and the Affordable Housing Ordinance.

**Development Agreements**

The City makes extensive use of development agreements as a tool to provide regulatory relief from density, parking and other multi-family development standards in exchange for public benefits, including but not limited to affordable housing. For example, the City has entered into 10 development agreements to provide for multi-family housing in R-3 districts which exceed the maximum density standard of 25 dwelling units per acre. These projects have provided for 609 senior and 62 family units. The average densities for the senior and family projects are 50 and 78 units per acre, respectively.

**Affordable Housing Ordinance**

The purpose of the Affordable Housing Ordinance is to expand the affordable housing stock in proportion with the overall increase in residential units in the City through establishment of standards to encourage the development of low- to moderate-income housing. Adopted in 1993, the Ordinance requires developers of residential projects with 20 or more units to provide 10 percent of the units for affordable housing, provided that the City determines the development of the affordable units does not place an economic burden on the developers. To offset the costs of developing affordable units, the City offers the following incentives:

- Density bonus;
- Flexible development standards, such as a reduction in unit square footage, on-site requirements, and off-site improvements;
- Deferral of development impact fees;
- Use of Building Code alternatives;
- Assistance in application for public funds, such as rent subsidies, bond financing, community development block grants (CDBG);
- Redevelopment set-aside funds; and
- Any other lawful means of offsetting the costs of providing affordable units

All affordable units developed under this Ordinance must remain as units affordable to lower and moderate-income households for 30 years. The 30-year period begins upon initial sale or rental of the units. Since adoption of the ordinance, only four projects have met the 20-unit threshold which triggers implementation of the Ordinance, including Vintage Canyon, Tamarack Pointe, Arbors, and Laurel Walk. With the economy in the late 1990s again robust and several of the City's specific plans now becoming active, the Affordable Housing Ordinance will again come into play. To provide even greater flexibility in the way the City's affordable housing requirements are fulfilled, the Ordinance allows for payment of an in-lieu fee, described below.

*In-Lieu Fees:* The Affordable Housing Ordinance provides the developers of for-sale housing with the option to fulfill the inclusionary housing requirement by paying an in-lieu fee to the City. The in-lieu fees collected are deposited in the Affordable Housing Trust Fund to be used exclusively for the development or maintenance of housing affordable to low- and moderate-income households. The fee is calculated by subtracting the current market value for comparable units from the established maximum affordable home price for moderate-income households. The formula included in the Ordinance calls for the City to calculate the fee each time a developer wishes to apply for the in-lieu fee option. To date, of the four projects which met the 20-unit threshold of the inclusionary housing ordinance, none has opted to pay the in-lieu fee, and all have provided the affordable units on site. Given development activity projected to occur in Brea's hillsides, it is likely many of these hillside projects will opt to pay the in-lieu fee rather than provide affordable units on-site due to the high cost of developing in the hillsides.

### 3. Provisions for a Variety of Housing Types

Housing element law specifies that jurisdictions must identify adequate sites to be made available through appropriate zoning and development standards to encourage the development of various types of housing for all economic segments of the population, including multi-family rental housing, factory-built housing, mobile homes, emergency shelters, and transitional housing. Table HE-28 summarizes housing types permitted in each of the residential zone districts in Brea.

**Table HE-28**  
**Housing Types by Residential Zone Category**

Housing Types Permitted	Residential Zone						
	R-1	R1-H	RH	R-1 (5,000)	R-2	R-3	
Conventional Single Family	✓	✓	✓	✓	✓	✓	
Cluster Single Family	✓	c	✓		✓		
Estate Single Family		✓	✓				
Planned Unit Development	✓	c		✓	✓		
Second Units	✓	c	c				
Condominiums			c		✓	✓	
Mobile Home Parks		c	c		c	c	
Multiple Family					✓	✓	
Townhouses			c	✓	✓	✓	
Care Facility: 6 or fewer persons	✓	✓		✓	✓	✓	
Care Facility: 6 or more persons						c	

Source: Zoning Ordinance, City of Brea, December 1996

✓ = Permitted c = Conditionally Permitted

#### Second Units

The Zoning Ordinance permits second dwelling units by right in the R-1 Zone and conditionally permits second units in the R1-H and RH zones. Specifically, in the R-1 Zone, one second dwelling unit is allowed per lot provided that: (1) the lot contains one existing single-family dwelling unit; (2) the occupant of either the existing dwelling unit or the second unit is the owner of both units; (3) the second unit meets the development standards for such a unit in the R-1 Zone; and (4) the second unit does not cause the existing dwelling to conflict with the minimum standards applicable to the single family zone on which it is located. The City has processed and approved numerous applications for the development of second units.

**Mobile Homes/  
Manufactured  
Housing**

About seven percent of the housing stock in Brea consists of mobile home units. Mobile home parks are conditionally permitted in the following zones: R1-H, RH, R-2, and R-3. Mobile homes used as residences, other than those located within an authorized mobile home park, are prohibited in all residential zones. There are currently five mobile home parks in the City: Bandera Estates, Crestmont, Hollydale, Lake Park Brea, and Rancho Brea.

Pursuant to State law, manufactured housing placed on a permanent foundation in are permitted in all residential zones. Such housing is subject to the same development standards and design review criteria as stick-built housing as set forth by the zoning district.

**Multi-Family  
Rental Housing**

Multi-family housing make up nearly one-third of the housing stock in the City. The Zoning Ordinance provides for multiple family developments in the R-2 and R-3 multiple family residential zones. The maximum dwelling unit densities for the R-2 and R-3 zones are 12 and 25 units per acre, respectively. A developer may receive a density bonus from the City to offset its costs of providing affordable housing units.

**Transitional  
Housing and  
Emergency Shelters**

Small-sized boarding homes and residential care facilities (for 6 or fewer persons) are permitted in the following zones: R-1, R1-H, R-1 5,000, R-2, and R-3. Residential care facilities for more than 6 persons are conditionally permitted in the following zones: R-3 (Multiple-Family Residential), C-P (Commercial, Administrative, and Professional Office), C-N (Neighborhood Commercial), C-C (Major Shopping Center), C-G (General Commercial), and C-RC (Commercial Recreation).

The Sheepfold Shelter for women and children currently operates in Brea. According to the City's Community Services Department, there have been some discussions (but no formal plans) by a local church and the operator of "Sheepfold" to open another shelter for women and children in Brea.

## **4. Development and Planning Fees**

The Development Services Department collects various fees from developers to cover the costs of processing permits and providing necessary services and infrastructure. An analysis conducted by the City in the early 1990s indicated the City's fees constituted only about six percent of the total development costs in multi-family development. As of this writing, fees for staff time spent on a project are based on three levels of hourly rate service: \$97 per

hour for “Management Staff,” \$73 per hour for “Technical Staff,” and \$63.25 per hour for “Inspection Staff.” Typically, most work is performed by technical and inspection staff. The Department requests a deposit at the time it begins to process a project based on an estimate of how much time will be spent. If the project ends up requiring more time, an invoice is processed for additional funds. If the project is completed before the entire deposit is used, a refund is processed. The City adopted the fee deposits as a measure to save costs for smaller and less complicated developments, in contrast to an average fee being applied to all projects. Table HE-29 is a list of deposit amounts charged by the City.

**Table HE-29  
Development Deposits**

<b>Fee Type</b>	<b>Fees</b>
Appeal Processing	Residents: \$50 flat fee Developers: \$1,000 deposit
Carbon Canyon Development Review	\$1,500 deposit
Certificate of Occupancy	\$30 fee
Conditional Use Permit	\$2,000 deposit
Development Agreement	\$5,000 deposit
Environmental Impact Report Review	\$2,000 deposit
General Plan Amendment	\$2,000 deposit
Negative Declaration	\$2,000 deposit
Plan Check	\$1,000 deposit
Planned Unit Development	\$3,750 deposit
Subdivision Map	\$2,000 deposit
Zone Change	\$2,000 deposit
Zone Variance	\$2,000 deposit
Zone Ordinance Amendment	\$2,000 deposit

Source: Development Services Department, City of Brea, August 1996

Not all projects are processed using the hourly rate formula. For inspection of small projects, flat fees are charged.

New residential development projects in Brea are subject to various impact fees, which include the Traffic Impact Fees, Water Impact Fees, Fire Impact Fees, and Dispatch Impact Fees. Established in 1995, these fees apply to new developments in the City and annexed portions of its Sphere of Influence. The City is currently in the process of re-evaluating the structure of impact fees. As a result of this analysis, the City may modify fees to better reflect the actual costs of providing services.

**Traffic Impact Fees**

“Traffic Impact Fees” are required, in part, by Orange County’s Measure M, a transportation initiative passed in 1990. These are fair-share based fees designed to offset or mitigate the traffic impacts caused by new developments. In some cases, a developer may be required to make certain traffic improvements in addition to, or in-lieu of paying traffic impact fees. However, the total cost of traffic improvements and/or fees will not exceed the development’s fair-share toward mitigating its own impacts. The amount of fees varies depending upon the type and location of a project. The required fees must be paid prior to the issuance of any building permits. Table HE-30 is a schedule of the traffic impact fee.

**Table HE-30  
Traffic Impact Fee Schedule by  
Residential Land Uses**

Land Use Category	Fee District			
	City west of Orange Freeway	City east of Orange Freeway	Western portion of Sphere of Influence	Eastern portion of Sphere of Influence
Low Density Residential (Up to 6 d.u. per acre)	\$693 per d.u.	\$2,683	\$1,841	\$5,693
Medium Density Residential (7 to 12 d.u. per acre)	\$561	\$2,171	\$1,490	\$4,606
High Density Residential (13 d.u. per acre +)	\$428	\$1,659	\$1,138	\$3,519

Source: Development Services Department, City of Brea, 1999

**Water Impact Fees**

“Water Impact Fees” are assessed to ensure that adequate water infrastructure and facilities are provided to new residential projects. In some cases, a developer may be required to make certain water improvements in addition to, or in-lieu of paying water impact fees. However, the total cost of water improvements and/or fees will not exceed the development’s fair-share of providing the water infrastructure or facilities. The amount of fees a developer pays ranges from \$2,876 to \$8,764 per dwelling unit, depending upon a project’s location. The fees only apply to three designated areas in the Sphere of Influence.

**Fire Impact Fees**

“Fire Impact Fees” are one-time fees established to fund the cost of building a fire station and purchasing new fire engines and equipment required to serve new development. The amount of fees vary depending upon a project’s location. The fees for the eastern and western portions of the Sphere of Influence are \$166.32 and \$1,388.35 per dwelling unit, respectively. New

developments within the City are not subject to fire impact fees because the infrastructure and equipment for fire services are already in-place.

### Dispatch Impact Fees

“Dispatch Impact Fees” are assessed to provide upgrades to the Police and Fire dispatch systems to ensure that new developments are provided with the necessary public safety services. The fee is \$132.50 for each dwelling unit.

Recognizing that the above impact fees may discourage new development, the City does allow the deferral of these fees as a means to offset the costs of providing affordable housing units for developers.

## 5. Building Codes and Enforcement

In addition to land use controls, local building codes also affect the cost of housing. The City has adopted the Uniform Building Code (UBC) which establishes the minimum standards for new construction. While the City may impose more stringent standards, it cannot adopt any which are below those of the UBC. No standard has been adopted above the minimum standards of the UBC.

## 6. Local Processing and Permit Procedures

The processing time needed to obtain development permits and required approvals is often cited as a prime contributor to the high cost of housing. Additional time may be necessary for environmental review, depending on the location and nature of a project.

Brea’s development process can be summarized in the following seven steps. All of these steps may not be necessary depending on the nature of a project.

### Screen Check

This process is a free service that allows a developer to meet with staff from various City departments before actually filing an application that initiates the development process. Screen Check is particularly helpful in projects requiring a Conditional Use Permit (CUP). City staff help developers by assessing proposed projects, discussing concerns, and explaining any special requirements of the projects. Screen Check can potentially save developers time and money by addressing potential concerns at an early stage thereby avoiding delays later.

**Application  
Submittal**

The “Planning (or Application) Submittal” process is when a developer submits an application, a deposit (amount depending on the complexity of the proposed project), and other necessary forms, such as “Environmental Information” or “Hazardous Waste and Substance Site Statement”.

**Plan Review**

The next phase, the “Plan Review” process, begins after the application plans and required deposit have been submitted. A planner is assigned to serve as the developer’s liaison helping to expedite the permit process and coordinating any department reviews required. During this phase, the developer’s application submittal is distributed to the appropriate City departments for review and comment. A review of the environmental issues associated with the proposed project (as required by the California Environmental Quality Act (CEQA)) will also be completed at this time. All discretionary projects (those requiring Planning Commission approval) require an Environmental Review.

**Planning  
Commission  
Approval**

If a project is determined to require discretionary action, it will be scheduled on the Planning Commission’s agenda and public notice will be given. Property owners within 300 feet of the proposed project will be notified. The developer is strongly encouraged to work out potential problems with neighboring residents or business owners before the Commission meeting.

A developer may appeal most Planning Commission actions to the City Council. In some instances, the project may require City Council approval. Still, other projects may never go to either the Commission or the Council. The level of review is dictated by the location and use of a project.

**Plan Check**

This step verifies that all building, fire, mechanical, plumbing, and electrical code requirements are fulfilled in compliance with Brea’s Building and Zoning codes. The developer’s application will be reviewed by the City’s Engineering, Planning, Fire, and Building divisions. Any requirements or problems identified during the Plan Review phase should now be completed or rectified. Once the application submittal is complete, the City performs the Plan Check. The Plan Check turnaround times are as follows:

Minor repair projects	10 days
Tenant improvements, room additions	15 days
Full development projects	90 days

Once Plan Check is completed, the developer will need to submit final plans with any revisions required by the City Council, Planning Commission or City Staff. Final plans must be submitted in order to receive the building permit(s).

**Building Permit**

A building permit is required before construction can begin. Throughout construction, regular inspections are required. The final inspection will involve all of the appropriate City departments.

**Occupancy Permit**

Once the final inspection is completed, the developer needs to secure an occupancy permit and pay the appropriate fees prior to occupancy. Buildings or structures cannot be used or occupied until the building official has issued a "Certificate of Occupancy."

## C. ENVIRONMENTAL AND INFRASTRUCTURE CONSTRAINTS

Brea is subject to the following environmental and infrastructure constraints. Carbon Canyon, in particular, is an area with high risks of being affected by these environmental hazards. The geology and soils of the canyon area present constraints upon development due to the presence of the Whittier Fault zone, landslides and related slope stability issues.

### 1. Seismic Hazards

The City is likely to experience ground shaking associated with the active and potentially active fault systems in the surrounding area. The most severe ground shaking would result from earthquake activity on the Whittier fault zone. The Whittier fault is active and cuts across Brea and the Sphere of Influence diagonally (northwest/southeast orientation).

## 2. Slope Stability - Landslides and Subsidence

Stability of natural slopes in the rugged Carbon Canyon area is a significant concern. The numerous landslides that have occurred in this area attest to its stability hazards. There is some potential for erosion and slope instability related to stream activity along major canyons and drainage courses. Liquefaction may also occur in areas of Carbon Canyon and along the major drainages of Brea Creek and Fullerton Creek where perched water may exist.

## 3. Flooding

All major tributary courses throughout the City are areas of potential flooding. Specifically, Carbon Canyon stream and its tributaries, Telegraph Canyon Creek and Soquel Canyon Creek, which flow into the Carbon Canyon Dam area, exhibit some hazardous flooding during high intensity storms. Flooding may also occur in areas surrounding the Carbon Canyon Dam and at the Orange County reservoir should there be dam failure. There is currently an inundation map to guide development in areas that would be affected by dam failure.

## 4. Public Services and Facilities

Future development in the Carbon Canyon area will result in the need for additional infrastructure (roads, facilities providing water and power etc.) and public safety facilities including police, fire and paramedic services. These additional services and facilities will be funded by developers who are required to pay various impact fees. However, as mentioned previously, developers may have the fees deferred as an incentive to provide affordable housing in their residential developments.

## 5. HOUSING RESOURCES



Various resources available for the development, rehabilitation, and preservation of housing are available in Brea. The section begins with an overview of the availability of land resources or residential sites for future housing development, and an evaluation of the City's ability to provide adequate sites to address the its identified share of future housing needs. This section also presents the financial resources available to support the provision of affordable housing in the community, and specifically identifies funding amounts for redevelopment set-aside, CDBG and HOME funds available during the five year planning period. Finally, the section concludes with a discussion of opportunities for energy conservation.

### A. AVAILABILITY OF SITES FOR HOUSING

An important component of the Housing Element is the identification of sites for future housing development, and evaluation of the adequacy of these sites in fulfilling the City's share of regional housing needs (as determined by SCAG). Table HE-31 shows the City's residential development potential by General Plan land use category. Figure HE-12 identifies the general location of vacant and underutilized sites available for residential development.

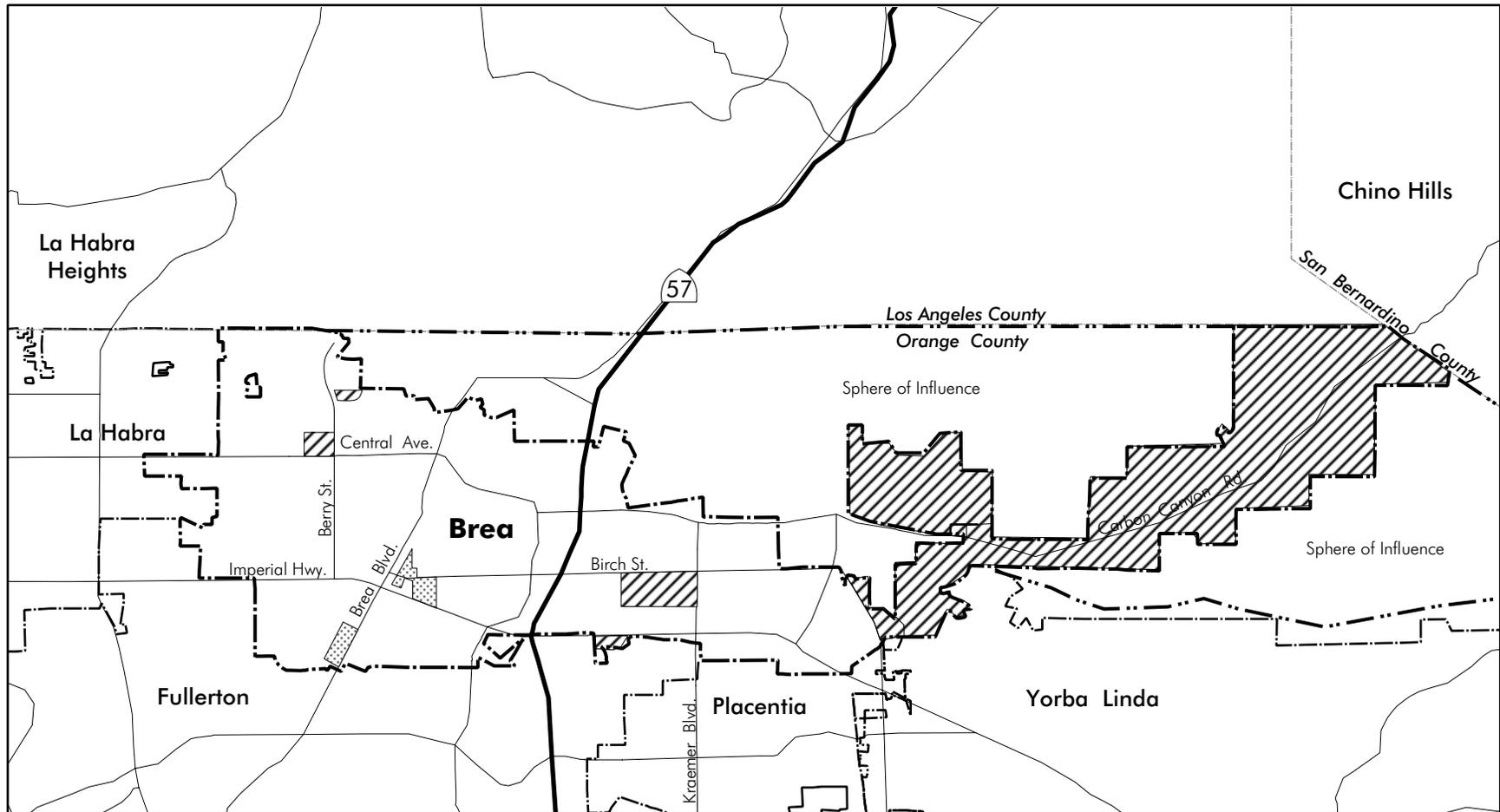


Figure HE-12  
Future Residential Sites

**Table HE-31  
Residential Development Potential**

<b>Land Use District</b>	<b>Maximum Residential Density (du/ac)</b>	<b>Vacant Acreage</b>	<b>Under-utilized Acreage</b>	<b>Net Dwelling Units</b>
Rural (RH & R1-H)	1 du/5 ac	882	-	88
Hillside (RH & R1-H)	1 du/ac	-	-	-
Low Density (R-1)	6 du/ac	571	-	1,567
Medium Density (R-1 (5,000) & R-2)	12 du/ac	52	-	459
High Density (R-3)	25 du/ac	47	11	531
<b>Total</b>		1,552	11	2,645

Source: Development Services Department, City of Brea, 1999

- Notes:
1. Assumes realistic development potential in Carbon Canyon is approximately half of the 2,061 total units called for in the 1986 Specific Plan, estimated at 1,029 units (88 Rural, 483 Low Density, 115 Medium Density, and 343 High Density).
  2. Underutilized acreage reflects key developable parcels in Brea Towne Plaza Specific Plan yielding, a potential net increase in 188 units.

As shown in Table HE-31, the City can accommodate 2,645 net new dwelling units based on existing zoning, General Plan, and Specific Plan designations. The 1,655 units in the “Rural” and “Low Density” districts will most likely be single-family homes, while the 990 units in the “Medium Density” and “High Density” zones will be in multi-family housing developments. Two large specific plans in the eastern hillside areas of the City - Carbon Canyon and Olinda Ranch - will comprise a significant portion of the City’s future growth. Olinda Ranch allows for 639 single-family units, and has begun development.

The 1986 Carbon Canyon Specific Plan calls for approximately 2,000 units in this area, although in all likelihood this significantly overstates the level of development which can realistically be expected. The City has received a preliminary development application for a sizable portion of Carbon Canyon known as Canyon Crest which proposes approximately half the units entitled under the Specific Plan. The City has been undertaking infrastructure studies in Carbon Canyon and the Sphere of Influence to better assess supportable development in the area. Given these considerations, the residential sites analysis contained in Table HE-31 assumes Carbon Canyon will build out at half the residential development potential assumed by the Specific Plan.

While Table HE-31 identifies potential for 990 new multi-family units in medium and high density areas, the City has a strong history of providing increased densities in exchange for affordable units. Through development agreements, the City has provided

developers with incentives such as density bonuses as well as relaxed development standards (e.g. lower parking requirements, smaller unit sizes). The City has approved numerous residential developments (most of which are senior apartments) that have densities well above the 25 units per acre standard established for the High Density or R-3 zone, as well as mixed use housing including loft housing above retail uses.

In addition to vacant sites, future housing units can also be accommodated on underutilized lots or lots on which developments are not built-out to the maximum density level permitted. The Brea Towne Plaza Specific Plan area, in particular, is an area designed for high density housing and contains a significant number of parcels developed with only one or two units. The Towne Plaza Specific Plan provides density incentives for the consolidation of underutilized parcels in this area. A parcel specific analysis identifies potential for a net increase of 188 high-density units in the Towne Plaza area on key underdeveloped sites.

### Comparison of Site Inventory with RHNA

Brea has a regional housing growth need (RHNA) of 1,052 new units for the 1998-2005 period. Given that the City can accommodate residential growth of over 2,640 units, it can certainly accommodate 1,052 units in this period. The more important issue, however, is whether the City has adequate sites for the provision of units for very low-, low-, and moderate-income households. The State Department of Housing and Community Development (HCD) uses the following general affordability and development density guidelines for urban areas to evaluate the adequacy of available sites to fulfill a city's share of regional housing needs:

- Very Low-Income - 25 du/ac
- Low-Income - 18 du/ac
- Moderate-Income - 8 du/ac

Housing units built in Brea since January 1998 can be subtracted from the City's RHNA. Since January 1998, a total of 144 housing units have been developed in the City, including 6 single-family homes, a triplex (3 units), 30 loft apartments and 105 senior apartment units. Given the market sales and rent data presented earlier in the needs assessment, the following income distribution can be assumed for these units: 5 of the single-family homes are at upper-income sales prices; one single-family home, the triplex,

and loft apartments are at prices affordable to moderate income households; and the senior apartment units are at rent levels affordable to very low-income households. The Redevelopment Agency provided funding support for both the loft apartments and the senior housing project (Vintage Canyon), ensuring long term affordability to moderate- and very low-income households, respectively. Thus, the RHNA figures for the City can be reduced to the following:

**Table HE-32**  
**1998-2005 Regional Housing Growth Needs compared to**  
**Units Constructed Between 1998 and 1999**

<b>Income Category</b>	<b>Regional Housing Needs (RHNA)</b>	<b>Number of New Units Constructed</b>	<b>Remaining Units Needed</b>
Very Low	203	105	98
Low	136	0	136
Moderate	212	34	178
Upper	502	5	497
<b>Total</b>	<b>1,052</b>	<b>144</b>	<b>908</b>

Sources: 1999 SCAG Regional Housing Needs Assessment  
Development Services Department, City of Brea, 1999

Given the City's zoning designations and the maximum allowable densities and based on HCD's density thresholds, units affordable to very low-income and low-income would need to be located in the High Density or R-3 zone. As displayed in Table HE-31, a total of 531 units can be accommodated in areas designated High Density. This high density growth potential adequately fulfills the remaining need for 234 very low- and low-income units. The City's remaining housing needs for moderate-income households are for 178 units. With residential sites to accommodate 459 additional units in the Medium Density (R-2) zone, the City also has adequate residential development capacity to address its remaining moderate-income housing needs.

### **Availability of Public Facilities and Services**

As an urbanized community, the majority of Brea has already in place all of the necessary infrastructure to support future development. All land currently designated for residential use is served by sewer and water lines, streets, storm drains, and telephone, electrical, and gas lines. To assure that future development can be served in less developed parts of Brea, the City is in the process of updating its master plans for sewer and water services. Also, the City collects various impact fees from developers to cover the costs of providing necessary services and infrastructure. These fees apply to new developments in the City and annexed portions of the Sphere of Influence.

## B. FINANCIAL RESOURCES

There are a variety of existing and potential funding sources available for affordable housing activities in Brea. They include the following:

### 1. Redevelopment Housing Set-Aside

Redevelopment housing set-aside funds are one of the primary sources of financing used for the preservation, improvement, and development of affordable housing. As required by the California Redevelopment Law, Brea's Redevelopment Agency sets aside 20 percent of all tax increment revenue generated from the redevelopment project area for the purpose of increasing and improving the community's supply of housing for low- and moderate-income households. This set-aside is placed in a separate Low- and Moderate-Income Housing Fund. This fund has funded Agency-assisted affordable housing programs and projects in Brea, such as the senior rental subsidy program, the homebuyer assistance program, the Birch View and Habitat condominium projects, and the multi-family rental developments listed in Table HE-21 (Needs Assessment). Interest earned on money in the Fund, and repayments from loans, advances or grants are returned to the Fund and used to assist other affordable housing projects and programs.

Housing units developed using the Agency's housing set-aside funds must remain affordable to the targeted income group for the longest feasible period of time and not less than 15 years for rental housing and 10 years for ownership housing. However, there are provisions that allow for a term of less than 10 years if the Agency receives a fair return on invested funds.

So long as the expenditure directly serve to increase, improve, or preserve the supply of low- and moderate-income housing, Redevelopment Law allows for a broad range of uses for the Housing Fund. These uses include, but not limited to the acquisition of land or buildings, construction of buildings, rehabilitation of buildings, subsidies, and on-site and off-site improvements.

The Housing Fund may be used outside the redevelopment project area (but must remain within the City limits), upon a finding by the City Council that use outside the project area will be of benefit to the redevelopment project. The Housing Fund must be expended on housing that is available at affordable housing cost

to households whose incomes do not exceed the low- and moderate-income level for the area.

The City of Brea is projected to have approximately \$3.7 million in redevelopment set-aside funds available for housing activities during this Housing Element cycle (2000-2005). Annual contributions range from \$289,000 to \$1 million per year.

## 2. Community Development Block Grant (CDBG) Funds

Through the CDBG program, the federal Department of Housing and Urban Development (HUD) provides funds to local governments for funding a wide range of community development activities for low-income persons. The CDBG program provides formula funding to larger cities and counties, while smaller cities (less than 50,000 population) generally compete for funding that is allocated to and administered by the state or county. As a smaller city, Brea receives its allocation of CDBG funds through the Orange County Housing and Community Development Department.

The CDBG program is very flexible in that the funds can be used for a wide range of activities. The eligible activities include, but not limited to: acquisition and/or disposition of real estate or property, public facilities and improvements, relocation, rehabilitation and construction (under certain limitations) of housing, homeownership assistance, and clearance activities. The City currently allocates 70 to 75 percent of its CDBG funds to housing activities.

## 3. HOME Investment Partnership Program

Established by the Federal government in 1990, the HOME program is designed to improve and/or expand a jurisdiction's affordable housing stock. Unlike the CDBG program, HOME funds can only be used for affordable housing activities. Specifically, HOME funds can be used for the following activities which promote affordable rental housing and lower-income homeownership: building acquisition, new construction, reconstruction, moderate or substantial rehabilitation, homebuyer assistance, and tenant-based assistance.

There are fairly strict requirements governing the use of the funds. Two major requirements are that HOME funds must be: (1) used for activities that target certain income groups (lower income families in particular) and (2) matched 25 percent by non-federal sources (such as City redevelopment funds).

The City anticipates receiving approximately \$1.2 million in CDBG and HOME funds from the County during the 2000-2005 period, averaging \$250,000 per year.

## 4. Tax Exempt Multi-family Revenue Bonds

The construction, acquisition, and rehabilitation of multi-family rental housing developments can be funded by tax exempt bonds which provide a lower interest rate than is available through conventional financing. Projects financed through these bonds, which can be issued by the Redevelopment Agency, are required to set aside 20 percent of the units for occupancy by very low-income households or 40 percent of the units to be set aside from households at 60 percent of the area median income. Tax exempt bonds for multi-family housing may also be issued to refinance existing tax exempt debt, which are referred to as a refunding bond issue.

In 1985, the Agency issued multi-family revenue bonds to finance the 124-unit Town and County Apartments project on South Brea Boulevard. A Regulatory Agreement requires that 20 percent of the units (25 of 124) be reserved for lower income households at affordable rents. This project also included a refunding bond issue in 1994 and additional housing set-aside funds to secure a total of 50 affordable units for lower income households. The Redevelopment Agency issued tax exempt 501(c)(3) bonds for non-profit acquisition of the Rancho Brea mobile home park and financed \$395,000 in park improvements using housing set-aside funds.

## 5. Housing Agreements

The Redevelopment Agency assists in the development of new affordable housing units by entering into Disposition Development Agreements (DDA) or Owner Participation Agreements (OPA) with developers. DDAs or OPAs may provide for the disposition of Agency-owned land at a price which can support the development of units at an affordable housing cost for low- and moderate-income households. These agreements may also provide for

development assistance, usually in the form of a density bonus or the payment of specified development fees or other development costs which cannot be supported by the proposed development.

Land disposition projects to date include (but not limited to) Arbors, Laurel Walk, Laurel Creek, Birch View, and Ash Street Cottages. The City has also provided development assistance (density bonus and/or land disposition) to the following: Brea Woods, Darnan, Orange Villa, and three projects with Habitat for Humanity.

## 6. Housing In-Lieu Fee

The City allows the payment of in-lieu fee to fulfill the inclusionary housing requirement under the Affordable Housing Ordinance. The Planning Commission decides, on a case by case basis, whether a developer can pay in-lieu fees instead of providing the required affordable housing units. The in-lieu fees collected are deposited in an Affordable Housing Trust Fund to be used exclusively for the development or maintenance of housing affordable to low- and moderate-income households.

## 7. Low Income Housing Tax Credit (LIHTC)

Created by the 1986 Tax Reform Act, the LIHTC program has been used to encourage the construction and rehabilitation of low-income rental housing. It allows investors an annual tax credit over a ten-year period, provided that the housing meets the low-income occupancy requirements: a minimum of 20 percent of the units must be affordable to households at 50 percent of area median income (AMI), or 40 percent affordable to those at 60 percent of AMI. The total credit over the ten-year period has a present value equal to 70 percent of the qualified construction and rehabilitation expenditures. This program has limitations which affect the amount of tax credits received, such as a ten-year requirement for housing being purchased, a minimum rehabilitation expenditure for rehabilitation projects. The tax credit is typically sold to large investors at a syndication value. The rehabilitation of Walnut Village Apartments (on Walnut Avenue) and the construction of the Vintage Canyon senior apartment complex were funded in part by LIHTC proceeds.

## 8. Mortgage Credit Certificates (MCC)

The MCC program is designed to help low- and moderate-income first-time homebuyers qualify for conventional first mortgage loans by increasing the homebuyer's after-tax income. The MCC is a direct tax credit for eligible homebuyers equal to 20 percent of the mortgage interest paid during the year. The other 80 percent of mortgage interest can still be taken as an income deduction. Brea is a participating city in the County-run MCC program and has issued 36 MCCs to date.

## 9. Section 8 Rental Assistance

The federal Section 8 program provides rental assistance to low-income persons in need of affordable housing. There are two types of subsidies under Section 8: certificates and vouchers. A certificate pays the difference between the fair market rent and 30 percent of the tenants' monthly income, while a voucher allows a tenant to choose housing that may cost above the fair market rent figure, with the tenant paying the extra cost. The voucher also allows the tenant to rent a unit below the fair market rent figure with the tenant keeping the savings.

The Orange County Housing Authority (OCHA) coordinates and administers Section 8 rental assistance on behalf of the unincorporated areas and the 28 cities that do not have their own housing authorities (exceptions are Anaheim, Garden Grove, and Santa Ana). According to the County's Affordable Housing Strategy, there are approximately 6,700 households in extremely low-income, low-income, and special needs groups receiving Section 8 assistance. Like many other urban jurisdictions, the demand for rental assistance in Orange County is very high. There are at least 3,200 applicants on a waiting list for Section 8 assistance and the average waiting time is four years.

## 10. California Housing Finance Agency (CHFA) Programs

CHFA administers a number of ownership and rental housing programs funded with the proceeds of tax-exempt bonds. The single-family programs provide low- and moderate-income first-time homebuyers with below-market interest rate mortgages and/or second mortgages to assist with downpayment. The rental programs provide financing for the construction of apartment units, including second mortgages from CHFA's Housing Trust Fund.

## 11. Fannie Mae Downpayment Assistance Program

The Federal National Mortgage Association, better known as “Fannie Mae,” offers a program that provides second mortgages to homeowners. This second mortgage can serve as the downpayment and closing costs on home purchases. Under this program, a city or county is required to post a reserve fund equal to 20 percent of an amount that Fannie Mae then makes available for such second mortgages. Resources are leveraged at a minimum ratio of 5:1.

The Brea Redevelopment Agency also administers a Homebuyer Assistance Loan Program using housing set-aside funds for direct downpayment assistance lending. Over 80 households have been assisted to date.

## C. OPPORTUNITIES FOR ENERGY CONSERVATION

As residential energy costs rise, increasing utility costs reduce the affordability of housing. The City has many opportunities to directly affect energy use within its jurisdiction. Title 24 of the California Administrative Code sets forth mandatory energy standards for new development, and requires adoption of an “energy budget”. The following are among the alternative ways to meet these energy standards.

**Alternative 1:** The passive solar approach which requires proper solar orientation, appropriate levels of thermal mass, south facing windows, and moderate insulation levels.

**Alternative 2:** Generally requires higher levels of insulation than Alternative 1, but has no thermal mass or window orientation requirements.

**Alternative 3:** Also is without passive solar design but requires active solar water heating in exchange for less stringent insulation and/or glazing requirements.

In turn, the home building industry must comply with these standards while localities are responsible for enforcing the energy conservation regulations. Some additional opportunities for energy conservation include various passive design techniques. Among the range of techniques that could be used for purposes of reducing energy consumption are the following:

- Locating the structure on the northern portion of the sunniest portion of the site;
- Designing the structure to admit the maximum amount of sunlight into the building and to reduce exposure to extreme weather conditions;
- Locating indoor areas of maximum usage along the south face of the building and placing corridors, closets, laundry rooms, power core, and garages along the north face;
- Making the main entrance a small enclosed space that creates an air lock between the building and its exterior; orienting the entrance away from prevailing winds; or using a windbreak to reduce the wind velocity against the entrance.

## Southern California Edison Customer Assistance Program

Southern California Edison (SCE) offers a variety of energy conservation services under Customer Assistance Programs (CAP). These services are designed to help low-income households, senior citizens, permanently disabled, and non-English speaking customers control their energy use. All CAP participants must meet the federally-established income guidelines. Most of the services offered are available free of charge.

## Southern California Gas Company Involvement Program

The Southern California Gas Company offers an energy conservation service known as the Community Involvement Program (CIP). Provided free of charge, this service provides weatherization for the homes or apartments of low-income families, provided they meet the federally-established income guidelines.

## 6. HOUSING PLAN

Sections 2 through 5 of the Housing Element establish the housing needs, opportunities and constraints in the City of Brea. The following Housing Plan section first evaluates the accomplishments of the last adopted housing element and then presents the City's five-year Housing Plan. This Plan sets forth Brea's goals, policies, and programs to address the identified housing needs of the City.

### A. EVALUATION OF ACCOMPLISHMENTS UNDER ADOPTED HOUSING ELEMENT

Under State Housing Element law, communities are required to assess the achievements under their adopted housing programs as part of the five-year update to their housing elements. These results should be quantified where possible (e.g. the number of units that were rehabilitated), but may be qualitative where necessary (e.g. mitigation of governmental constraints). The results should then be compared with what was projected or planned in the earlier element. Where significant shortfalls exist between what was planned and what was achieved, the reasons for such differences must be discussed.

Brea's prior Housing Element was adopted in 1989, and sets forth a series of housing programs with related objectives for the following topic areas: conservation and improvement of the housing stock; provision of adequate affordable housing; provision of adequate housing sites; removal of governmental constraints; and promotion of equal housing opportunities. This section reviews the current appropriateness of these programs, the effectiveness of the Element, and the progress in implementation since 1989. Housing programs implemented since 1989 and not included in the previous Housing Element are also evaluated.

# 1. Conservation and Improvement of the Housing Stock

A major issue addressed in the 1989 Housing Element is the conservation and improvement of existing affordable housing. Specifically, the Element focused on upgrading the City's single-family residential neighborhoods and conserving the rental housing stock.

## 1989 Housing Element Programs

### Single-Family Rehabilitation Program

The Single-Family Rehabilitation Program has been providing low-interest loans to low- and moderate-income homeowners for the repair or rehabilitation of their homes. This program is funded by Federal CDBG and HOME grants, and Redevelopment set-aside funds. The City's 1989 objective was to provide financial assistance for the improvement of 13 housing units per year (65 units in five years). Brea has exceeded this goal, providing approximately 15 to 20 loans annually.

### Condominium Conversion Regulations

As a means of protecting the existing stock of apartment units from conversion to condominiums, the 1989 Housing Element called for the establishment of condominium conversion regulations. The Zoning Ordinance now provides criteria for the conversion of existing multi-family housing to condominiums. Residential condominium conversions are permitted in appropriate zoned districts within the City subject to the issuance of a conditional use permit. A proposed condominium or condominium project may be approved, conditionally approved, or disapproved by the Planning Commission.

### Section 8 Rental Assistance (Certificate/Voucher) Program

The Section 8 program provides rental subsidies to very low-income households as a means of providing affordable rental housing not otherwise provided by the private market. A total of 51 households in Brea currently receive Section 8 vouchers from the Orange County Housing Authority (OCHA), the agency administering the program for the City.

## New Programs

### Rental Rehabilitation and Assistance Program

Approved by the City Council and Redevelopment Agency in 1997, the Rental Rehabilitation Assistance Program provides rehabilitation and refinancing loans to qualifying owners of rental properties. The loans are funded by Federal HOME grants and the City's redevelopment housing set-aside funds. To date, the program has provided loans to two rental developments in the City: Imperial Terrace and Civic Center Apartments. Imperial Terrace offers 37 units, 18 of which are provided at affordable rents to very low- and low-income households. The Civic Center

Apartments has 30 units, 16 of which are for very low- and low-income households.

**Multi-family Housing  
Acquisition/  
Rehabilitation**

The acquisition and rehabilitation of aging apartment complexes is a way to provide quality affordable housing to very low- and low-income households. The Redevelopment Agency acquired the 51-unit Walnut Village Apartments in an effort to provide for the rehabilitation and preservation of these apartments at rents affordable to very low-income households. The Agency then sold the units to a non-profit housing corporation, which coordinated the rehabilitation, maintenance, and management of the units. The project construction/ rehabilitation was completed in 1997, and provides 47 units of very low-income family housing. There is a 40-year affordability term on the units.

**Neighborhood  
Enhancement Plan**

Adopted in 1998, Brea's Neighborhood Enhancement Plan identifies six focus areas in the City with signs of physical deterioration and other related problems, and proposes programs to provide for neighborhood upgrading. Programs proposed as part of the Plan include (but are not limited to): assistance to non-profit organizations for the purchase and rehabilitation of multi-family units, an apartment inspection program, and a targeted rehabilitation loan program for single- and multi-family properties in the focus neighborhoods.

In the first year since adoption of the Enhancement Plan, the Redevelopment Agency has started marketing a rehabilitation loan program specifically in the six identified neighborhoods and has had positive results. A mailing campaign to promote the program has generated a high number of inquiries. Between July and December 1998, five (5) new housing rehabilitation loans were provided to families living in the six focus areas. In addition, two neighborhood outreach meetings have been held with input received from the residents, followed by "Neighborhood Beautification Days" complete with clean-up volunteers and multiple free trash bins to help residents remove debris and upgrade their neighborhood.

## **Provision of Adequate Affordable Housing**

Since the adoption of the 1989 Housing Element, the City has implemented various programs to increase homeownership and rental opportunities for low- and moderate-income households.

## 1989 Housing Element Programs

### Affordable Housing Ordinance

An Affordable Housing Ordinance was adopted in 1993. It requires developers of residential projects with 20 or more units to provide 10 percent of the units for housing affordable to low- and moderate-income households, provided that the City determines the development of the affordable units does not render the project economically infeasible. Since its adoption, four housing developments have been subject to the Ordinance, and have provided a total of 120 affordable units (106 very low, 3 low, and 11 moderate), as shown in Table HE-33. The developers of these projects received various incentives to off-set the cost of developing the affordable units.

**Table HE-33**  
**Developments Providing Affordable Units Under the Affordable Housing Ordinance**

<b>Project Name (completion date)</b>	<b>Type of Units</b>	<b>Number of Units (affordable units)</b>
Tamarack Pointe (1996)	Family Apartments	48 (3 Low, 2 Moderate)
The Arbors (1996)	Single-family Homes (ownership)	27 (5 Moderate)
Laurel Walk (1998)	Single-family Homes (ownership)	25 (1 Very Low, 4 Moderate)
Vintage Canyon (1999)	Senior Apartments	105 (all Very Low)

Source: Redevelopment Agency, City of Brea, 1999

### Housing Trust Fund

The 1989 Housing Element proposed the completion of a study regarding establishment of a "Housing Trust Fund." The study was completed and an "Affordable Housing Trust Fund" was created in 1993, in conjunction with the adoption of the Affordable Housing Ordinance. Under the Ordinance, a developer may be allowed to pay in-lieu fees instead of providing the required affordable units. The in-lieu fees collected are deposited in the Affordable Housing Trust Fund to be used exclusively for the development or maintenance of housing affordable to low- and moderate-income households. To date, there have been no contributions to the Trust Fund. The four developments that have met the 20-unit threshold have provided affordable units as required under the Ordinance.

### Non-profit Housing Development: Habitat Housing Program and Additional Non-Profit Partnerships

The 1989 Housing Element establishes a quantified objective for the development of 45 affordable housing units through the Habitat Housing Program. Beginning in 1994, the City has worked closely with Habitat for Humanity of Orange County to develop ownership housing for very low-income families. A total of 10 units in three projects have been completed through the program in Brea. The first project, Habitat 1 (104 S. Flower), was completed in 1994 and consisted of 4 condominium units. Completed two years later in 1996, Habitat 2 (275 S. Poplar) is another condominium development occupied by 5 very

low-income families. The third project, a single-family home, was constructed with alternative building materials and finished in 1998.

While Brea fell short of its development objective of 45 units through this particular program, it is important to note that the City has implemented other housing programs since 1989 that have increased homeownership opportunities for low- and moderate-income households. These programs include the Homebuyer Assistance Program, Mortgage Credit Certificate (MCC) program, and Development Assistance (all of which are discussed below). The City has also worked with two additional non-profit housing corporations: La Habra Neighborhood Housing Services (NHS) and the Caritas Corporation. NHS acquired South Walnut Apartments and rehabilitated the units with assistance from the Agency. The Agency also facilitated the sale of Rancho Brea Mobile Home Park (100 spaces) to Caritas. Through these efforts, 47 very low-income units were secured at South Walnut and 63 very low- to moderate-income units were secured at Rancho Brea.

<p><b>New Programs</b></p> <p><b>Homebuyer Assistance Program</b></p>	<p>Funded with the City’s redevelopment housing set-aside funds, the Homebuyer Assistance Program provides 30-year second mortgages to help low- and moderate-income families purchase homes in Brea. The second mortgage helps the buyer bridge the gap between the first mortgage the buyer can afford and the purchase price of the home. The City initiated the program in 1994 and has assisted over 80 households.</p>
<p><b>Mortgage Credit Certificate (MCC) Program</b></p>	<p>The MCC program is designed to help low- and moderate-income first-time homebuyers qualify for conventional first mortgage loans by increasing their after-tax income. Since the City began participating in this County-run program in 1994, a total of 41 MCCs have been issued, 16 of which were issued in conjunction with loans from the Homebuyer Assistance Program.</p>
<p><b>Senior Subsidy Program</b></p>	<p>Begun in 1989 and funded by redevelopment set-aside funds, Brea’s Senior Subsidy Program offers rent subsidies to very low-income senior households. Participating seniors are allowed to apply the subsidy towards rent at the dwelling of their choice. The program currently provides approximately 120 very low-income seniors with \$171 per month each in rent subsidies.</p>
<p><b>Tax Exempt Multi-Family Revenue Bonds</b></p>	<p>In 1994, the Redevelopment Agency refinanced the bond issue on the 122-unit Town and Country Apartments which was set to expire in 1996. By doing so, it preserved 25 affordable housing units. The Agency also provided a loan to the property owner to finance property improvements and to secure an additional 25</p>

affordable units, thus bringing the total number of affordable units to 50. The affordability term on the units is 30 years. The income mix of the units is: 25 units for very low- and low-income households, and 25 for median/moderate-income households.

**Housing Agreements:  
Land Disposition and  
Development  
Assistance**

The Redevelopment Agency has assisted in the development of new affordable housing by entering into Disposition and Development Agreements (DDA) with developers. These agreements provided for the disposition of Agency owned-land at a price which can support the development of units at an affordable housing cost for low- and moderate-income households. The agreements also provided for development assistance in the form of a density bonus. Between 1985 and 1999, the Agency assisted in the production of 483 affordable units. Tables HE-34 and HE-35 include Agency-assisted projects developed since 1989.

**Table HE-34  
Affordable Housing Projects Supported Through Land Disposition (1989-1999)**

<b>Project Name (completion date)</b>	<b>Type of Units</b>	<b>Number of Units (affordable units)</b>
Laurel Creek (1990)	Condominiums	30 (3 Moderate)
Habitat Homes (1993, 1995, 1998)	Condominiums	10 (10 Very Low)
Birch View (1994)	Condominiums	25 (5 Very Low, 1 Low, 14 Moderate)
Ash Street Cottages (1995)	Townhomes	4 (2 Moderate)
The Arbors (1996)	Single-family Homes (ownership)	27 (5 Moderate)
Foursquare Church (1998)	Rental Units	9 (5 Very Low, 4 Low)
Laurel Walk (1998)	Single-family Homes (ownership)	25 (1 Very Low, 4 Moderate)

Source: Redevelopment Agency, City of Brea, 1999

Projects that included land disposition by the Redevelopment Agency are shown in Table HE-34. In addition to these developments, the Agency has also supported affordable housing development through direct financial assistance and/or density bonuses, as illustrated in Table HE-35.

**Table HE-35  
Affordable Housing Projects Supported Through Direct  
Financial Assistance or Development Incentives (1989-1999)**

<b>Project Name (completion date)</b>	<b>Type of Units</b>	<b>Number of Units</b>
Vintage Canyon (1999)	Senior Apartments	105 (All Very Low)
Downtown Loft Apartments-Building A (30 units completed in 1999, 32 pending)	Loft Apartments	62 (8 Low, 25 Moderate)

Source: Redevelopment Agency, City of Brea, 1999

Completed in 1999, Vintage Canyon is 105-unit senior housing complex developed by USA Properties in partnership with Riverside Charitable Corporation, a non-profit housing organization. Vintage Canyon is considered a unique development in that all of its units are for very low-income senior residents. In addition, the complex provides residents with transportation to medical appointments and day trips, recreational activities, on-site medial screening, and food delivery programs at the community room.

The Downtown redevelopment project includes 30 loft apartments with 25 units with affordable housing restrictions. Soon to be completed, Building A of the Downtown Loft Apartments will contain a total of 32 one-bedroom units, of which 8 are set-aside for low-income households. These 32 loft apartments are being built above retail spaces on Birch Street and have an affordability term of 30 years.

**Housing Breans  
Advisory Board**

The Housing Breans Advisory Board was formed in August 1993 to provide community input into the City's housing efforts. The Board has five members and is comprised of a cross-section of the community. The Board has provided valuable input on the review of affordable housing projects and programs, and has supported the City's efforts to provide quality housing opportunities to all economic segments of the community.

**Looking Forward: A  
Senior Policy Report**

In 1999, the Brea Senior Policy/Issues Task Force completed Looking Forward, a report that studies the demographic and policy impacts of the City's growing senior citizen population. An important issue addressed in the report is senior housing. The City recognizes that as Brea's senior population increases in size, it is important to provide for a variety of housing choices available to seniors of every income level. The major recommendations of the Task Force include: encouraging assisted living facilities and nursing home developments; investigating creative housing solutions such as co-housing; requiring new senior developments

to participate in “joint use” of facilities and services; and including an affordable housing component in all senior developments.

**Mobile Home Policy**

Adopted in 1995, the City’s Mobile Home Policy presents various policies and programs to assist in preserving and enhancing mobile home park living in Brea. It is formulated in recognition of the fact that many senior citizens, young families, and fixed income households are residing in mobile home parks and overpaying for their housing. The Policy identifies the problem of separate ownership of the land and dwelling unit which creates competing interests and conflict between the residents and park owners. The major recommendations of the Policy include: allowing the use of the existing first-time home buyers program for the purchase of mobile home park pads; providing financial assistance to mobile home park owners who agree to rehabilitate the park or to provide amenities to upgrade the living environment; offering mediation services between mobile home park owners and tenants; and providing information to park residents regarding programs, such as the State of California Mobile Home Ombudsman’s Office.

**Non-profit Acquisition  
and Management of  
Mobile Home Parks:  
Rancho Brea Mobile  
Home Park**

With the Mobile Home Policy in place, the Redevelopment Agency issued bonds and provided a loan to assist the Caritas Corporation, a non-profit organization, to purchase the 100-space Rancho Brea Mobile Home Park. Caritas purchased the park with the overall intention of improving living conditions, stabilizing rents, making necessary park improvements, and increasing residential participation. Sixty-three of the units in the park are now occupied by very low- to moderate-income families. Specifically, there are 24 very low-, 15 low-, and 24 moderate-income households living in Rancho Brea. The occupancy of the park went from 80 percent prior to Agency involvement to 100 percent when the sale and park improvements were completed.

## **Provision of Adequate Housing Sites**

A key component to satisfying the housing needs of all segments of the community is the provision of adequate sites for different types, sizes and prices of housing.

**1989 Housing  
Element Programs**  
  
**Market and Land Use  
Study**

In 1990, the City completed a comprehensive market and land use study to determine the existing and future potential for residential, retail, office, and recreational development within the City. This study has assisted the City in defining the highest and best use of its remaining land resources. Specifically, the study provided background information to the City Council to guide future decisions on: defining the character of downtown Brea, specifically the Brea Boulevard and Imperial Highway area; determining land

use appropriate for the vacant Tonner Canyon area; and defining the extent of future development permitted in its hillsides.

### Specific Plans

The 1989 Housing Element called for the implementation of specific plans as a means to “provide housing in adequate locations with convenient access to employment opportunities, community facilities and adequate services.” Specific plans were adopted in 1995 to guide the development of the Olinda Heights and Birch Hills areas. Both plans provide for the development of the two areas as primarily residential communities. Birch Hills has the capacity to accommodate a maximum of 475 dwelling units, with nearly 60 percent of the area acreage dedicated to residential use. Olinda Heights can accommodate 662 units, with 40 percent of the area acreage designated for residential use. Both Birch Hills and Olinda Heights provide for low (3.2 du/ac) and medium (7.7 du/ac) density residential uses.

The City also previously adopted specific plans for the Carbon Canyon and Brea Towne Plaza areas. Adopted in 1986, these plans recognize the unique characteristics of the areas and provide policies to guide their development. Carbon Canyon has an area of 1,758 acres, of which three-quarters is designated for single-family and multi-family residential uses. A total of 2,360 units can be accommodated in the area. The Brea Towne Plaza Specific Plan is rather unique in comparison to the City’s other specific plan areas in that this plan is concerned with the redevelopment and revitalization of a downtown area, rather than the development of vacant lands. Approximately 55 percent of the Towne Plaza area is zoned for medium to high density residential developments; the area has the capacity to support a total of 2,800 dwelling units. Loft apartments are currently being developed above retail spaces on Birch Street and ownership townhomes along Brea Boulevard in the downtown.

## Removal of Governmental Constraints

### 1989 Housing Element Programs

#### Affordable Housing Ordinance

As mentioned previously, the City has adopted an Affordable Housing Ordinance. The Redevelopment Agency can approve various incentives to off-set the costs of developing affordable units as required by the Ordinance. They include: density bonus, flexible development standards, deferral of development impact fees, use of building code alternatives, assistance in applying for public funds, redevelopment set-aside funds, and any other lawful means. The provision of incentives such as density bonuses have resulted in an increase in the supply of affordable housing in Brea.

## New Program

### Screen Check

The Development Services Department provides “Screen Check,” a free service that allows a developer to meet with staff from various City departments before actually filing an application that initiates the development process. The Screen Check process can potentially save developers time and money by addressing potential concerns at an early stage thereby avoiding delays later.

## Promotion of Equal Housing Opportunities

### 1989 Housing Element Program

### Fair Housing Program

One of the goals in the 1989 Housing Element is to promote housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry, national origin or color. To achieve this goal, the City participates with the Orange County Fair Housing Council and implemented a system of referring discrimination complaints to the proper agencies.

## Summary of Housing Accomplishments

Since the adoption of the previous Housing Element in 1989, the City of Brea has made significant progress in achieving its housing goals. The City’s major accomplishments include the following:

- Establishing the Homebuyer Assistance Program in 1994 and providing assistance to 80+ households;
- Providing rental subsidies to 125 very low-income seniors through the Senior Subsidy Program;
- Assisting in the provision of 180+ affordable housing units through provision of land disposition, regulatory incentives, and/or direct financial assistance;
- Providing 15 to 20 rehabilitation loans annually to low- and moderate-income single-family homeowners;
- Adopting the Affordable Housing Ordinance and establishing the Affordable Housing Trust Fund;
- Forming the Housing Breans Advisory Board to provide community input into the City’s housing efforts;
- Adopting the Neighborhood Enhancement Plan to proactively address the problem of physically deteriorating neighborhoods;
- Creating the Rental Rehabilitation and Assistance Program and upgrading two rental properties containing 67 units;
- Refinancing the bond issue on the 122-unit Town and Country Apartments and providing 50 rent-restricted units to very low- and low-income households; and

- Adopting specific plans for the development of the Olinda Heights and Birch Hills areas as primarily residential communities

**Comparison of 1989-1994 RHNA with Units Constructed during 1989-1997**

According to the 1989 Housing Element, the City had a total regional housing need (RHNA) of 1,600 units to be produced between 1989 and 1994, including 237 very low-, 285 low-, 362 moderate-, and 716 upper-income units. While the RHNA originally covered the 1989 to 1994 planning period, the RHNA has been extended through December 1997 based on direction from the State Department of Housing and Community Development to reflect the revised housing element cycle. Housing developed as of January 1998 is applied to the City's future RHNA for the 1998-2005 period.

Recognizing that the production goals established by the 1989-1994 RHNA were unrealistically high based on historic levels of development, the City established a more attainable objective of constructing 800 new housing units, with the same income mix as the RHNA. Table HE-36 summarizes the City's RHNA and Housing Element objectives, and provides a comparison with actual levels of production.

**Table HE-36  
1989-1994 RHNA, 1989 Housing Element Objectives,  
and Units Constructed during 1989-1997**

<b>Income Category</b>	<b>1989-1994 RHNA</b>	<b>1989 Housing Element Objectives</b>	<b>Units Constructed 1989-12/1997</b>
Very Low	237 (15%)	118 (15%)	113 (20%)
Low	285 (18%)	143 (18%)	53 (9%)
Moderate	362 (23%)	181 (23%)	107 (19%)
Upper	716 (45%)	358 (45%)	305 (52%)
<b>Total</b>	<b>1,600</b>	<b>800</b>	<b>578</b>

Sources: Development Services Department, City of Brea, September 1999 Redevelopment Agency, City of Brea, September 1999

Based on a review of Building Department records, a total of 578 units have been developed in the City between 1989 and 1997. This shortfall in housing production in contrast to both the RHNA and the Housing Element objectives is primarily the result of the economic recession which impacted most of the State in the early to mid 1990s. The RHNA was developed prior to the recession and assumed that the economic prosperity experienced in the late 1980s would continue in the 1990s. In actuality, housing

construction activities were significantly lower than the levels projected by SCAG throughout the region.

Because all of the housing projects developed in Brea since 1989 have received some type of assistance from the Redevelopment Agency, it is possible to determine the distribution of the units by income category. While Brea fell short in total production, the City supported a higher proportion of very low-income units than specified by the RHNA. A comparison between actual production figures and the 1989 objectives indicate that the City was extremely close to meeting its objective for very low-income households (only 5 units short) despite the recession, reflecting the active involvement of the Redevelopment Agency in the provision of affordable housing. Units for very low- to moderate-income households accounted for nearly half of the total units developed since 1989.

## B. GOALS AND POLICIES

This section of the Housing Element contains the goals and policies the City intends to implement to address Brea's identified housing needs and other important housing related issues.

### Housing Conservation and Maintenance

**GOAL 1.0** To maintain and improve the quality of existing housing and residential neighborhoods in Brea.

**Policy 1.1** Undertake comprehensive revitalization efforts in the City's Neighborhood Enhancement Areas to improve the quality of housing in these areas.

**Policy 1.2** Continue to provide rehabilitation and home improvement assistance to low- and moderate-income households, seniors, and the disabled.

**Policy 1.3** Preserve and improve the quality of affordable rental housing by providing rehabilitation and refinancing assistance to owners of rental properties, including single-family homes rented to low-income tenants.

- Policy 1.4** Cooperate with non-profit housing providers in the acquisition and rehabilitation of older apartment complexes, and maintenance as long term affordable housing.
- Policy 1.5** Promote increased awareness among property owners and residents of the importance of property maintenance to long-term housing quality.
- Policy 1.6** Conserve existing affordable rental housing through placement of long term affordability controls on assisted housing, and through provision of rent subsidies to bring down the cost of market rate units.

## Provision of Housing

- GOAL 2.0** **To assist in the provision of adequate housing to meet the needs of the community. Establish a balanced approach to meeting housing needs that includes the needs of both renter and owner households.**
- Policy 2.1** Use financial and/or regulatory incentives where feasible to encourage the development of affordable housing.
- Policy 2.2** Maintain the Inclusionary Housing Ordinance to ensure that new housing development provide a percentage of units affordable to low- and moderate-income households or pay an in-lieu fee to support the provision of affordable housing.
- Policy 2.3** Provide homeownership assistance to low- and moderate-income households.
- Policy 2.4** Support the provision of rental housing to accommodate large families.
- Policy 2.5** Facilitate the development of senior housing with supportive services.
- Policy 2.6** Encourage the construction of apartment complexes with strong on-site management.
- Policy 2.7** Promote the City's Affordable Housing Programs with employers in Brea.

**Policy 2.8** Explore creation of Brea Housing Trust Fund and/or Brea non-profit housing corporation.

**Policy 2.9** Partner non-profit organizations and affordable housing builders with for-profit developers.

## Provision of Adequate Housing Sites

**GOAL 3.0** To provide adequate housing sites through appropriate land use, zoning, and specific plan designations to accommodate the City's share of regional housing needs.

**Policy 3.1** Continue to provide opportunities for in-fill housing development in downtown Brea as part of the City's overall revitalization strategy for the area.

**Policy 3.2** Facilitate development of a range of housing types in the City's specific plan areas, including Birch Hills, Brea Towne Plaza, Carbon Canyon, and Olinda Heights.

**Policy 3.3** Identify available in-fill lots for future housing development opportunities.

**Policy 3.4** Pursue phased annexation of the City's Sphere of Influence to provide additional growth opportunities consistent with infrastructure capacities.

**Policy 3.5** Explore reuse opportunities on obsolete commercial or industrial sites, especially obsolete strip retail centers.

**Policy 3.6** Promote mixed-use development where housing is located adjacent to jobs, shopping, services, schools, and leisure opportunities.

**Policy 3.7** Explore creative residential development opportunities in the hillsides.

## Removal of Governmental Constraints

**GOAL 4.0** To mitigate any potential governmental constraints to housing production and affordability.

**Policy 4.1** Offer financial and/or regulatory incentives where feasible to off-set or reduce the costs of developing affordable housing.

**Policy 4.2** Periodically review City regulations, ordinances, and residential fees to ensure that they do not constrain housing development.

**Policy 4.3** Maintain the City's expedited and coordinated permit processing system.

**Policy 4.4** Pursuant to State law, continue to provide zoning provisions which allow second residential dwelling units on single-family lots as a means to meet some affordable housing demand.

**Policy 4.5** Explore more innovative parking solutions, such as tandem parking, garage lifts, shared parking, to ensure that parking requirements do not constrain housing development.

## Equal Housing Opportunity

**GOAL 5.0** To promote equal opportunity for all residents to reside in the housing of their choice.

**Policy 5.1** Continue to enforce fair housing laws prohibiting discrimination in the building, financing, selling or renting of housing on the basis of race, religion, family status, national origin, physical handicap or other such circumstances.

**Policy 5.2** Promote housing which meets the special needs of elderly persons, the disabled, and those in need of transitional housing such as the homeless.

**Policy 5.3** Encourage the provision of adequate housing to meet the needs of families of all sizes.

**Policy 5.4** Continue to provide opportunities for public input and representation in the City's housing efforts through the Housing Breans Advisory Board.

## C. HOUSING PROGRAMS

The goals and policies contained in the Housing Element address Brea's identified housing needs, and are implemented through a series of housing programs offered through the Development Services Department and Redevelopment Agency. Housing programs define the specific actions the City will undertake to achieve the stated goals and policies. According to Section 65583 of the State Government Code, a city's housing programs must address the following five major areas:

- Conserving the existing supply of affordable housing;
- Assisting in the provision of housing;
- Providing adequate sites to achieve a variety and diversity of housing;
- Removing governmental constraints as necessary; and
- Promoting equal housing opportunity

Brea's housing plan for addressing unmet needs, removing constraints, and achieving quantitative and qualitative objectives is described in this section according to the above five areas. The housing programs described on the following pages include existing programs as well as several new programs which have been added to address the City's unmet housing needs. The program summary (Table HE-37) included at the end of this section specifies for each program the following: five-year objective(s), funding source(s), and agency responsible for implementation of the program.

## CONSERVING THE EXISTING SUPPLY OF AFFORDABLE HOUSING

Conserving and improving the housing stock is an important goal for Brea. Approximately one-third of the housing stock is 30 years or older, the age when most homes start to have rehabilitation needs. By adopting the Neighborhood Enhancement Plan, the City has taken a pro-active approach to conserving its current housing stock. The six focus neighborhoods identified by this Plan as evidencing physical deterioration are now specifically targeted for City housing improvement assistance.

### 1. Neighborhood Enhancement Plan

Adopted in 1998, Brea's Neighborhood Enhancement Plan sets forth a comprehensive program to address deteriorated housing conditions in six focus neighborhoods, identified previously in Figure HE-10. The Plan proposes the following programs within the neighborhood focus areas: 1) assistance to non-profit organizations for the purchase and rehabilitation of multi-family units; 2) specific programs based on the "critical need" found within the neighborhood; and 3) a rehabilitation loan program for single- and multi-family properties. An initial community outreach event is conducted in each neighborhood to receive input from the community and provide information on available programs, followed by a neighborhood clean-up day. Implementation of programs under the Neighborhood Enhancement Plan is critical to improving the quality of housing in the City.

**Five-year Objective:** The City will undertake program activities identified in the Neighborhood Enhancement Plan to facilitate neighborhood upgrading in the six focus areas.

### 2. Single-Family Rehabilitation Program

Brea offers several housing rehabilitation programs for lower income (80 percent MFI) single-family homeowners, including low interest loans, deferred payment loans, and rebates. The maximum loan amount is \$35,000, although loan amounts typically vary from \$15,000 to \$25,000. Homeowners may receive loans to address the following types of problems: painting,

plumbing, leaks, sagging ceiling, electrical problems, termite damage, damaged floor tile and carpeting, poor drainage, crumbling chimney, roof leaks and related improvements. The City may also provide funding for room additions to alleviate overcrowded conditions. The City conducts a focused marketing of housing rehabilitation programs in the Neighborhood Enhancement areas both through the initial outreach events, and through targeted mailing of program brochures.

**Five-year Objective:** The City will continue to provide rehabilitation loans to qualified lower income homeowners. The objective is to issue eighteen (18) loans annually.

### 3. Rental Rehabilitation and Assistance Program

The Rental Rehabilitation and Assistance Program provides rehabilitation and refinancing loans to qualified owners of rental properties and mobile home parks. The loans are funded by Federal HOME grants and redevelopment housing set-aside funds. Typical loan amounts range between \$10,000 to \$20,000 per affordable unit with fixed interest rates and 30-year terms. These loans make it possible for apartment and/or mobile home park owners to adhere to the highest standards of housing maintenance. In return for the loans, property owners must: (1) maintain affordable rent levels for a portion of the units as determined by the Redevelopment Agency; (2) provide property maintenance covenants; and (3) provide and maintain a significant level of affordability for the long term.

**Five-year Objective:** The City will continue to inform owners of apartments and mobile home parks about the Rental Rehabilitation Program. The objective is to rehabilitate twenty-five (25) units annually and to focus the program in the six Neighborhood Enhancement areas especially for single-family rental properties with low-income tenants.

### 4. Multi-family Housing Acquisition and Rehabilitation

A key program in Brea's overall strategy to provide affordable housing to lower income households is through the acquisition and rehabilitation of aging and/or deteriorating apartment complexes. Under this program, the Agency acquires a problem apartment complex and then sells the project to a non-profit housing provider

which in turn coordinates the rehabilitation, maintenance, and management of the project as long term affordable housing. The Redevelopment Agency has demonstrated that this is a successful program and is integral to improving the City's aging stock of multi-family rental units.

**Five-year Objective:** The City will identify apartments in need of rehabilitation, and cooperate with non-profit housing corporations to acquire and rehabilitate the units. Projects will be focused in the Neighborhood Enhancement areas to contribute to revitalization efforts in these neighborhoods.

## 5. Senior Subsidy Program

As a means of addressing the affordable housing needs of its senior renter population, Brea has established a rent subsidy program for very low-income seniors. Seniors participating in the program are allowed to apply the subsidy towards rent at the dwelling of their choice. Presently, the program provides approximately 120 seniors with \$171 per month each in rent subsidies. As a testimony to the high demand for rental assistance, there are currently close to 100 seniors on the waiting list under the program.

**Five-year Objective:** The City's objective is to provide rental subsidies to approximately 120 to 130 seniors annually.

## 6. Section 8 Rental Assistance Program

The Section 8 Rental Assistance Program extends rental subsidies to very low-income households who spend more than 30 percent of their gross income on housing. Rental assistance not only addresses housing affordability, but also overcrowding by allowing families that may be "doubling up" to afford their own housing. The Orange County Housing Authority coordinates Section 8 rental assistance on behalf of the City, with 51 households currently participating in the program.

**Five-year Objective:** The City's objective is to continue current levels of Section 8 rental assistance, and to direct eligible households to the County program.

## ASSISTING IN THE PROVISION OF HOUSING

To enable more households to attain homeownership in Brea, the City offers two mortgage assistance programs: the Homebuyer Assistance Program and the Mortgage Credit Certificate (MCC). These programs are very important given that housing prices in Brea rank among the highest in north Orange County. The Redevelopment Agency actively works with both non-profit and for profit developers in the production of affordable for-sale and rental housing. The City has also adopted an Affordable Housing Ordinance to ensure that future market-rate housing developments provide a portion of units affordable to low- and moderate-income households.

### 7. Homebuyer Assistance Program

The Homebuyer Assistance Program is funded by redevelopment housing set-aside funds, and provides 30-year second mortgages to help low- and moderate-income families purchase homes in Brea. The second mortgage provided helps the homebuyer to bridge the gap between the first mortgage the buyer can afford and the purchase price of the home. Participants make no payments for the first five years of the loan. Instead of collecting interest, the Redevelopment Agency shares in the appreciation of the homes when it is sold. To be eligible, families must meet the specified income requirements and be able to pay a 5-percent downpayment on their home. The maximum sales price for a home through the program in 1999 is \$248,000. The maximum price of a home purchased in conjunction with the Mortgage Credit Certificate program (described below) is \$204,937 for an existing home and \$229,883 for a new home.

**Five-year Objective:** The City's objective is to provide loans to fifteen (15) households annually through the Homebuyer Assistance Program.

### 8. Mortgage Credit Certificate

The Mortgage Credit Certificate (MCC) program is a federal program that allows qualified first-time homebuyers to take an annual credit against federal income taxes of up to 15 percent of the annual interest paid on the applicant's mortgage. This enables homebuyers to have more income available to qualify for a mortgage loan and make the monthly mortgage payments. The

value of the MCC must be taken into consideration by the mortgage lender in underwriting the loan and may be used to adjust the borrower's federal income tax withholding. The MCC program has covenant restrictions to ensure the affordability of the participating homes for a period of 15 years. MCCs can be used in conjunction with the Homebuyer Assistance Program.

**Five-year Objective:** Through the County, the City will continue to provide MCCs to qualified first-time homebuyers. The objective is to provide four (4) MCCs per year depending on availability.

## 9. Affordable Housing Development

For-profit and non-profit housing developers play an important role in providing affordable housing in Brea. The City has provided land, regulatory incentives, and direct financial assistance to a number of developers to provide both ownership and rental housing to low- and moderate-income households. In recent years, the Redevelopment Agency has worked with The Olson Company, a for-profit developer, on a number of affordable housing developments. Non-profits active in Brea include Habitat for Humanity of Orange County, Neighborhood Housing Services of Orange County (NHS), and the Caritas Corporation.

**Five-year Objective:** The City will continue to provide financial and regulatory incentives to private developers to increase the supply of affordable housing in Brea. The City will focus a portion of Agency assistance towards rental projects which meet the needs of very low- and low-income renters, and large families.

## 10. Affordable Housing Ordinance

In 1993, the City adopted the Affordable Housing Ordinance which requires developers of residential projects with 20 or more units to allocate 10 percent of the units for housing affordable to low- and moderate-income households (provided that the City determines the provision of the affordable units does not make the project economically infeasible). The Redevelopment Agency can approve various financial or regulatory incentives to off-set the costs of developing the affordable units required by the Ordinance. These incentives include: density bonus, flexible development standards, deferral of development impact fees, use of building code alternatives, assistance in applying for public funds, redevelopment set-aside funds, and any other legal means. At the discretion of the City, a developer may be allowed to pay

in-lieu fees instead of providing the required affordable units to be deposited into an Affordable Housing Trust Fund.

Since adoption of the Ordinance in 1993, four projects have met the 20-unit threshold, all of which opted to provide the required affordable units on-site. With development now beginning to occur in the City's hillsides, it is likely many of these projects will opt for payment of the in-lieu fee due to the high cost of development in hillside locations.

**Five-year Objective:** The City will continue to utilize the Affordable Housing Ordinance as a tool to integrate affordable housing within market rate developments, or alternatively, to generate fees in support of affordable developments in off-site locations.

## 11. Housing Breans Advisory Board

The Housing Breans Advisory Board was formed to provide community input into the City's housing efforts. Specifically, the Board has provided valuable input on the review of affordable housing projects and programs, and has supported the City's efforts to provide quality housing opportunities to all economic segments of the community. The Board currently has five members, but may be expanded to include seven members.

**Five-year Objective:** The City will continue to conduct regular meetings to involve the Housing Breans Advisory Board in the City's housing efforts.

## PROVIDING ADEQUATE HOUSING SITES

A major element in meeting the housing needs of all segments of the community is the provision of adequate sites of all types, sizes and prices of housing. The City's General Plan, Zoning Ordinance, and specific plans dictate where housing may locate, thereby affecting the supply of land available for residential development.

## 12. Land Use Element

The Land Use Element of the General Plan provides for a variety of housing types in Brea, with densities ranging from one unit per five acres to 25 dwelling units per acre. Higher densities can be achieved through the City's density bonus provisions, senior housing policy, and through development agreements. The Land Use Plan provides for a net increase in over 2,600 units within the current City limits, including 531 high density units. This residential development potential is more than adequate to address the City's share of regional housing needs, which are identified as 1,052 dwelling units between 1998 and 2005. In addition to a variety of development assistance programs, the City maintains an inventory of available sites for future residential development.

**Five-year Objective:** The City will continue to provide appropriate land use designations and maintain an inventory of sites suitable for residential development, coupled with incentives for the development of affordable housing, to address its share of regional housing needs.

## 13. Specific Plans

The majority of future housing growth within the City's current boundaries will occur within several specific plan areas: Birch Hills, Olinda Heights, Carbon Canyon, and Brea Towne Plaza. Birch Hills and Olinda Heights have the capacity to accommodate 475 units and 662 units, respectively, according to the specific plans adopted in 1995. Carbon Canyon, the largest of the specific plan areas, can accommodate over 1,000 units, including 343 high-density units. The Brea Towne Plaza specific plan provides for in-fill housing opportunities in the downtown area. The Towne Plaza area has the capacity to support 2,800 dwelling units, over half of which will be in medium to high density residential developments.

**Five-year Objective:** The City will ensure that the specific plans are properly implemented to provide housing units of varying types and costs.

## 14. Annexation of Areas in Sphere of Influence

Brea has a sizable Sphere of Influence. The annexation of these areas will increase the City's capacity to accommodate future housing growth. In addition, since any new housing development will be subject to the City's Affordable Housing Ordinance, which requires 10 percent affordable units in projects with 20 or more units, additional affordable housing will be provided in the Sphere of Influence. If developers choose not to provide the required units, then they will have the option of paying an in-lieu fee, resulting in additional funds for affordable housing production.

**Five-year Objective:** The City will pursue phased annexation of the Sphere of Influence to provide additional growth opportunities consistent with infrastructure capacities.

## REMOVING GOVERNMENTAL CONSTRAINTS

Under current State law, the Housing Element must address, and where legally possible, remove governmental constraints affecting the maintenance, improvement, and development of housing. The following programs are designed to mitigate government constraints on residential development and facilitate the development of a variety of housing.

## 15. Regulatory and Financial Assistance

Brea has strong track record in providing regulatory and/or financial incentives to facilitate the development of affordable housing. In addition to direct financial assistance, the City provides the following incentives to for-profit and non-profit developers to off-set or reduce the costs of providing affordable housing: density bonus, deferral of development impact fees, flexible development standards, use of building code alternatives, assistance in applying for public funds as well as any other viable means. Where direct Agency assistance is involved, the City typically enters into a Development Agreement which provides regulatory relief from density, parking, and other multi-family development standards in exchange for affordable housing. The City also uses the Administrative Adjustment Process to provide

flexibility in height, setback, open space, parking and other requirements.

**Five-year Objectives:** The City will continue to offer regulatory and/or financial incentives to facilitate the provision of affordable housing by private developers. The City will periodically review all regulations, ordinances, and residential fees related to housing rehabilitation and/or construction to assess their impact on housing costs, and revise as appropriate.

## 16. Efficient Project Processing

The City is committed to a streamlined development process, and has adopted several procedures to facilitate project processing. The Development Services Department provides “Screen Check,” a free service designed to save developers time and money. Screen Check allows a developer to meet with staff from various City departments before actually filing an application that initiates the development process. Through this service, a developer can become aware of the City’s concerns about a project and address them early prior to starting the development process.

Another way the City assists developers in project processing is through the Guaranteed Second Opinion Program. Developers are given the opportunity to ask for a second opinion on how a regulation or policy is interpreted at any point during the permit process.

**Five-year Objective:** The City will continue to offer streamline development processing, and will periodically review departmental processing procedures to ensure efficient project processing.

## PROMOTING EQUAL HOUSING OPPORTUNITIES

To adequately meet the housing needs of all segments of the community, the housing plan must include program(s) that promote housing opportunities for all persons regardless of race, religion, sex, family size, marital status, ancestry, national origin, color, age, or physical disability.

## 17. Fair Housing Program

The City uses the services of the Orange County Fair Housing Council for fair housing outreach and educational information, fair housing complaints, tenant/landlord dispute resolution, and housing information and counseling. The role of the Council is to provide services to jurisdictions, agencies, and the general public to further fair housing practices in the sale or rental of housing. Brea advertises the fair housing program through placement of fair housing services brochures at the public counter, and through the City's newsletter.

**Five-year Objective:** The City will continue to promote fair housing practices, and provide educational information on fair housing to the public. The City will refer fair housing complaints to the Orange County Fair Housing Council.

**Table HE-37  
Housing Implementation Program Summary**

<b>Housing Program</b>	<b>Program Goal</b>	<b>Five-Year Objective(s)/ Time Frame</b>	<b>Funding Source(s)</b>	<b>Responsible Department or Agency</b>
<b>Conserving the Existing Supply of Affordable Housing</b>				
1. Neighborhood Enhancement Plan	Conserve and improve housing in the six focus neighborhoods.	Undertake program activities identified in the Plan.	CDBG; HOME; Redevelopment Set-Aside Funds	Redevelopment Services
2. Single-Family Rehabilitation Program	Provide loans to lower income homeowners to help them rehabilitate their homes.	Issue 18 loans annually.	CDBG; HOME Redevelopment Set-Aside Funds	Redevelopment Services
3. Rental Rehabilitation and Assistance Program	Provide rehabilitation and refinancing loans to owners of rental properties and mobile home parks.	Inform apartment and mobile home park owners about the program. Rehabilitate 25 units annually.	HOME; Redevelopment Set-Aside Funds	Redevelopment Services
4. Multi-Family Acquisition and Rehabilitation	Upgrade deteriorated apartment buildings, and provide affordable rental housing to very low- and low-income households.	Identify apartments in need of rehabilitation, and work with non-profits to acquire and rehabilitate the units.	Redevelopment Set-Aside Funds	Redevelopment Services
5. Senior Subsidy Program	Provide rental subsidies to very low-income seniors.	Assist a minimum of 120 to 130 seniors annually.	Redevelopment Set-Aside Funds	Redevelopment Services
6. Section 8 Rental Assistance Program	Provide rental subsidies to very low-income households.	Continue current levels of rental assistance and direct eligible households to the County program.	HUD	Orange County Housing Authority
<b>Assisting in the Provision of Housing</b>				
7. Homebuyer Assistance Program	Expand homeownership opportunities for lower and moderate-income homebuyers.	Provide loans to 15 households annually.	Redevelopment Set-Aside Funds	Redevelopment Services
8. Mortgage Credit Certificate (MCC)	Expand homeownership opportunities for lower and moderate-income homebuyers.	Provide 4 MCCs annually based on availability.	Federal Tax Credits	Redevelopment Services
9. Affordable Housing Development	Facilitate affordable housing development by for-profit and non-profit housing developers/corporations.	Provide financial and regulatory incentives and focus portion of assistance towards rental projects for lower income renters and large families.	Redevelopment Set-Aside Funds; Affordable Housing Trust Funds	Redevelopment Services
10. Affordable Housing Ordinance	Integrate housing affordable to low- and moderate-income households within market rate developments.	Continue to implement Ordinance to achieve affordable units, and generate in-lieu fee revenues.	Depending on incentives	Redevelopment Services

<b>Housing Program</b>	<b>Program Goal</b>	<b>Five-Year Objective(s)/ Time Frame</b>	<b>Funding Source(s)</b>	<b>Responsible Department or Agency</b>
11. Housing Breans Advisory Board	Provide for community input in the City's housing efforts.	Continue to conduct regular meetings to involve the Board in affordable housing activities.	None required - Volunteer Board	Redevelopment Services
<b>Providing Adequate Residential Sites</b>				
12. Land Use Element	Provide adequate sites for future housing development.	Maintain an inventory of residential sites and provide appropriate land use designations to fulfill regional housing needs.	General Fund	Development Services
13. Specific Plans	Ensure the provision of a variety of housing in the future.	Ensure specific plans are properly implemented to provide housing of varying types and costs.	General Fund	Development Services
14. Annexation of Areas in Sphere of Influence	Increase the City's capacity to accommodate future housing growth.	Pursue phased annexation of the Sphere of Influence.	General Fund	Development Services
<b>Removing Governmental Constraints</b>				
15. Regulatory and Financial Assistance	Facilitate the production of affordable housing through provision of regulatory and financial assistance.	Continue to offer incentives for affordable housing. Periodically review all regulations, ordinances, and fees.	General Fund	Development Services
16. Efficient Project Processing	Provide efficient development processing procedures.	Continue to offer streamline development processing, and periodically review procedures.	General Fund	Development Services
<b>Promoting Equal Housing Opportunities</b>				
17. Fair Housing Program	Further fair housing practices in the community.	Continue to promote fair housing practices. Provide educational information on fair housing to the public. Refer fair housing complaints to the Orange County Fair Housing Council.	None required	Development Services; Orange County Fair Housing Council

**Five-Year Goals Summary:**

TOTAL UNITS TO BE CONSTRUCTED: 1,052 (203 Very Low, 136 Low, 212 Moderate, 502 Upper)

TOTAL UNITS TO BE REHABILITATED: 215 (150 Very Low, 65 Low)

TOTAL UNITS TO BE CONSERVED: Conserve 51 very low-income (Section 8) rent subsidies and 120 very low-income senior subsidies.

Chapter 4  
**COMMUNITY  
RESOURCES**

THE CITY OF  
BRE A  
GENERAL PLAN



# Chapter 4

# COMMUNITY RESOURCES

PARKS AND OPEN SPACE • TRAILS • WILDLIFE HABITAT • SCENIC RESOURCES •  
WATER CONSERVATION AND QUALITY • AIR QUALITY • HISTORIC  
RESOURCES



## VISION

Imagine Brea, where resources are valued and preserved for future generations of Breans to experience and enjoy. Where residents prize parks, open spaces, wildlife habitat, recreational trails, water resources, air quality, and the City's historic buildings and monuments for their intrinsic value. Imagine parks, trails, scenic resources, and wildlife habitat areas that provide recreational and ecological benefits, as well as a source of aesthetic pleasure. Imagine water that is ample, clean, and pure, and used efficiently so that demands today do not compromise the needs of tomorrow. Imagine improvements in regional air quality. Imagine a place where distinctive historical qualities and buildings continue to link Brea's past to its future.

Brea will work to preserve and protect existing resources, and to capture new resources as they become available. The City will enhance its recreational facilities and look for ways to expand parks and trails throughout Brea. With assistance from interested organizations, creative solutions will be discovered and implemented to preserve sensitive habitat areas. The hills that establish the dramatic visual backdrop to the City will continue in this context, with significant ridgelines and other formations remaining undeveloped. Natural stream courses and important watershed areas will be protected from erosion and urban pollutants. Owners of historic properties will have incentives to maintain and enhance these properties, and all Breans will

## *Imagine Brea*

recognize the value these historic resources lend to the community.

The goals and policies in this Community Resources Chapter focus on the enhancement of community qualities that distinguish Brea. These resources contribute tremendously to the quality of life in Brea and allow residents to enjoy and experience features not found in many urban environments.

Photo credit: Hills for Everyone 2002



*View from Carbon Canyon Road of canyon along northern boundary of Chino Hills State Park.*

## PARKS AND OPEN SPACE

Parks and open spaces historically have provided passive greenery in urban areas, providing a refuge from the concrete environment with large green lawns, gardens, tall trees, and lakes and ponds. Many parks today, to meet the demands of a changing demographic, now serve as a location for active recreation, with sports and play facilities. As land continues to be developed and paved, and economic resources restricted or diverted, parks and open space have become increasingly difficult to acquire and fund. Nevertheless, parks and open space represent major, important resources in Brea, meeting both active and passive recreation needs and providing a source of aesthetic beauty. Carbon Canyon Regional Park and Chino Hills State Park create extensive stretches of scenic open space, but at the same time offer places where people can picnic, hike, throw a Frisbee, or ride a bike.



*The Plunge at  
City Hall Park.*

### Context

**Parks** Brea benefits from an assortment of neighborhood, community, and regional parks distributed throughout the community. Passive open space and park areas account for five percent of the land area within the corporate City limits. Chino Hills State Park alone, a natural wilderness park with hiking, biking, and equestrian trails, encompasses 3,400 acres.

#### **Classification and Standards**

The Brea park system consists of public neighborhood parks, community parks, and school grounds with joint-use agreements. Carbon Canyon and Craig regional parks, although located at least partially within Brea, are owned and maintained by Orange County.

**Passive/Casual and Active Parks**

Passive/casual parks do not include facilities for “active recreation” such as tennis courts, soccer fields, or softball diamonds. They are intended as public green space where city dwellers can escape from the rush of urban life. Passive parks include such amenities as large open green spaces, meadows, wandering pathways, ponds, and gardens.

Active parks, on the other hand, include a variety of facilities for recreation. Baseball and softball diamonds, basketball courts, horse shoe rings, football fields, playground, and swimming pools are examples of facilities often found in active parks.

The current trend cities have followed is providing recreational facilities, but many other cities have recognized the importance of creating passive recreation spaces as well. While passive parks do not contain active recreational facilities, they can also entertain active activities such as kite flying, pick-up football games, Frisbee, and other activities on a non-programmed basis. The task for cities is to provide a balance for both passive and active recreational parks that meet the demands of their residents.

The City owns, operates, and maintains 12 parks primarily designed and used for active recreation. Joint-use agreements with the Brea-Olinda Unified School District for use of school grounds and play areas provide residents with an additional 58.0 acres of park area. Table CR-1 lists all parks within the City limits, including associated facilities. Numbers listed with each park in the table correspond to the locations identified on Figure CR-1.

**Pocket Park.** A pocket park is small, neighborhood-oriented facility used to address limited or isolated recreational needs. Such parks usually range in size from 2,500 square feet and one acre. Woodcrest Park, located in the northwestern area of Brea, provides a small play area for children. Pocket parks like Woodcrest can be built on small, challenged properties and fill a localized park niche.

**Neighborhood Park.** Neighborhood and local parks, by definition, are specifically oriented toward residents within a one-half mile radius. These parks can encompass one to ten acres, with some sports fields and facilities usually geared toward the recreation needs of children.

**Community Park.** Community parks, as traditionally defined, vary from 10 to 60 acres in size and serve residents within a 3-mile radius of their homes. Tri-City is the only community park that serves Brea residents, but is not located in Brea. Located in Placentia less than a quarter mile from the City limits, Tri-City Park is maintained by the cities of Brea, Placentia, and Fullerton.

**School Park.** School parks, or joint-use parks, combine the resources of a school facility and a park. School parks are unique because they provide recreational needs in an efficient and cost-effective manner. Due to their size, school parks can offer broader amenities than a traditional neighborhood park, often more comparable to a community park, with baseball fields, basketball courts, tennis courts, and playgrounds.

**Regional Park.** Regional parks meet the recreational needs of a regional population and usually include a wide variety of active and passive recreation improvements. The environment in which the park is located influences the facilities available. Regional parks in urban areas can have a distinctive sports field focus, whereas a park within a natural open space area may offer only hiking or picnicking facilities. Night lighting of activity areas increases the availability and utilization of courts and playing field facilities.

As noted above, the two regional parks in Brea – Carbon Canyon Regional Park and Craig Regional Park – are County parks. Carbon Canyon Regional Park covers 124 acres, with 60 acres designed for recreational uses and the remaining acreage maintained as natural open space, including a grove of Coastal Redwoods. This open space area provides a transition to the adjacent Chino Hills State Park. The park contains hiking and equestrian trails, and offers guided nature walks and fishing.

Craig Regional Park, located between the SR-57 freeway and College State Boulevard, has 129 acres of rolling hills, baseball fields, open space, and a formal rose garden. The park is partially in Brea (14 acres), while the remaining portion of the park lies within the city of Fullerton.

**Other Recreation Areas.** Chino Hills State Park covers approximately 13,000 acres in Orange, Riverside, and San Bernardino counties. The park largely consists of natural open space and thus provides wildlife habitat value as well as open space for people. Over 60 miles of trails allow hikers, equestrians, and bicyclists to enjoy native southern California oak woodlands, grasslands, and riparian areas, as well as views of the surrounding urban areas and mountains.

**Table CR-1  
Park and Recreation Facilities**

Number	Park Name	Location	Amenities	Acres
City-Owned/Maintained Facilities				
<i>Mini or Pocket Parks</i>				
1	Woodcrest Park	Woodcrest Ave. at Puente St.	Barbeques, Children's play area, and picnic tables.	0.5
<i>Neighborhood Parks</i>				
2a	Arovista Park	Imperial Hwy at Berry St.	Children's play area, barbeques, picnic tables, restrooms, ball diamonds, soccer fields, volleyball court, amphitheatre, concession stand, football field, and basketball courts	15.0
2b	Arovista Skate Park (located in Arovista Park)	Imperial Hwy at Berry St.	"Street" and "freestyle" features with a rails, kidney pool, pyramid, table top, blank wall, ¼ pipes, and spectator seating area.	10,000 sq. ft.
3	City Hall Park	401 S. Brea Blvd.	Olympic-size swimming pool (Brea Plunge), restrooms, barbeques, Children's play area, basketball courts, and gazebo.	4.0
4	Greenbriar Park	Greenbriar Rd. and Associated Rd.	Barbeques, Children's play area, picnic tables, and restrooms.	3.0
5	Tamarack Park	520 N. Tamarack Ave.	Restrooms, barbecues, Children's play area, ball diamond, soccer fields, concession stand, football field, and basketball courts.	5.6
6	Brea Golf Course (municipal)	501 West Fir St.	Nine-hole course with driving range.	26.0
7	Tomlinson Park	Berry St./Central Ave.	Children's play area, picnic tables, and restrooms.	4.1

**Table CR-1  
Park and Recreation Facilities**

<b>Number</b>	<b>Park Name</b>	<b>Location</b>	<b>Amenities</b>	<b>Acres</b>
<b>School Parks (A,B)</b>				
8	Brea Jr. High School Park	400 N. Brea Blvd.	Children's play area, ball diamonds, soccer fields, volleyball court, tennis courts, concession stand, football field, and basketball courts	13.0
9	Country Hills School/Park	180 N. Associated Rd.	Restrooms, barbeques, Children's play areas, picnic tables, ball diamonds, soccer fields, tennis courts, concession stand, and football field.	6.0
10	Fanning Elementary School			
11	Lagos De Moreno Park/ Laurel Elementary School	200 S. Flower Ave.	Barbeques, Children's play area, and picnic tables.	1.5
<b>Community Parks</b>				
12	Tri-City Park (maintained by the Cities of Brea, Fullerton and Placentia)	Kraemer Blvd. at Golden Ave.	Restrooms, barbeques, Children's play area, picnic tables, bicycle trails, lake, and fishing.	40.0
Non-city Owned/Maintained Regional Parks and Facilities				
<b>Regional Parks (C)</b>				
13	Carbon Canyon Regional Park	Carbon Canyon Rd. (1 mile east of Valencia Blvd.)	Tennis courts, Children's play area, volleyball, ball diamond, multipurpose fields, hiking and biking trails, guided nature tours, picnic tables, and fishing.	124.0
14	Ted Craig Regional Park	3300 State College Blvd. (partially in Brea)	Restrooms, barbecues, Children's play area, picnic tables, ball diamonds, soccer fields, tennis courts, volleyball, racquetball courts, amphitheater, concession stands, bicycle trails, football field, basketball courts, and fishing.	129.0
<b>Other Recreation</b>				
15	Chino Hills State Park	Partially in Orange, Riverside, and San Bernardino Counties	Trails for hikers, mountain bikers, and equestrians, camping, and 13,000 acres of wilderness.	3,400 (D)
16	Birch Hills Golf Course		18 hole executive golf course.	

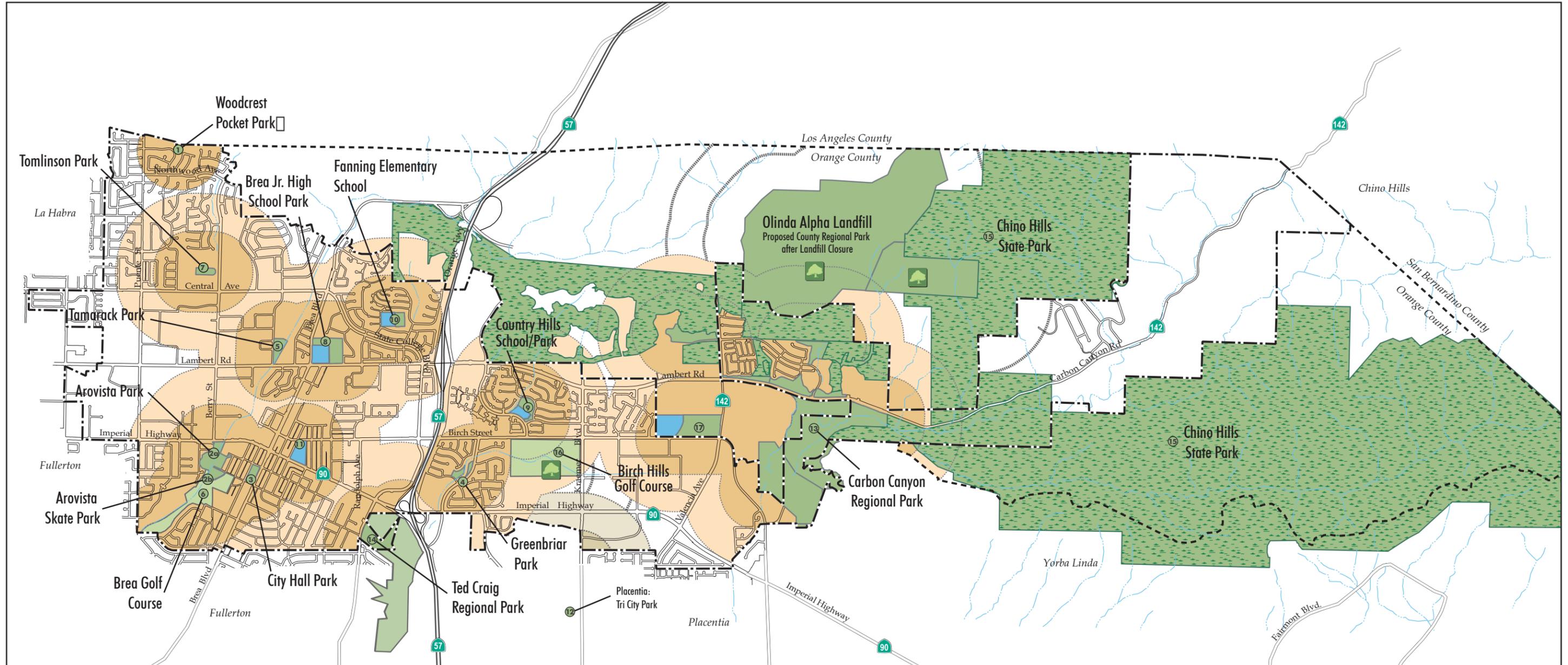
Source: City of Brea and Chino Hills State Park General Plan, February 1999

Note: (A) Joint use agreement with Brea-Olinda Unified School District.

(B) Maintained by the Olinda-Brea Unified School District

(C) Maintained by the County of Orange

(D) 3,400 acres are located in the Brea and Brea's Sphere of Influence, with the remaining acres located in the City of Chino Hills.



Source: City of Brea; Cotton/Bridges/Associates, 2002. □

0 3,000 6,000 9,000 12,000 Feet  
0 0.5 1 1.5 2 Miles

**Legend**

- City Boundary
- Sphere of Influence
- Creeks, Streams, and Drainage Channels

**Parks and Recreation Facilities**

- Parks and Recreation Facilities
- Open Space
- Golf Course
- Public Facilities (Schools)
- Potential Future Parks

**Park Names**

- Woodcrest Park
- Arovista Park
- City Hall Park
- Greenbriar Park
- Tamarack Park
- Brea Golf Course
- Tomlinson Park
- Brea Junior High School Park
- Country Hills School/Park
- Fanning Elementary School
- Lagos De Moreno Park
- Tri-City Park
- Carbon Canyon Regional Park
- Ted Craig Regional Park
- Chino Hills State Park
- Birch Hills Golf Course
- Brea Sports Park

**Parks and Recreation Service Areas\***

- Brea Parks**  
1/4 and 1/2 mile radii
- Tri-City Park**  
1/4 and 1/2 mile radii  
(Located in the City of Placentia)
- Potential Future Parks**  
1/4 and 1/2 mile radii

\* Note: Pocket/mini park radius is 1/4 mile.

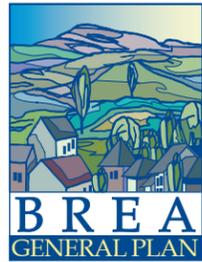


Figure CR-1  
Parks and  
Open Space Plan





*A children's play structure at Arovista Park.*

### **Recreation Programs**

Recreation programs are an integral component of an overall park and recreation system. Programs are addressed in the Community Services Chapter.

### **Need and Service Areas**

Brea has established a service standard of 5.0 acres of park and recreation facilities per 1,000 residents. Based on the January 1, 2002 population of 36,857 residents and park acreage of approximately 331 acres within the planning area, the ratio is 9.0 acres of parkland for every 1,000 residents. With the regional parks, Brea currently meets a high service standard. General Plan land use policy set forth in the Community Development Chapter could allow for up to 51,307 residents within the planning area. If the current park acreage remained constant, the ratio would reduce to 6.5 acres per 1,000 people.

While providing park acreage at or above the established community standard is important, the City must also ensure that park improvements and park locations respond to demographic needs. Brea is a young city, with families attracted to the community by the excellent school district. Children play sports, and sports fields occupy significant acreage. For many years, Breans have desired a dedicated sports park to meet increasing demands and to reduce wear on long-used fields. The City has planned for a sports complex aimed at organized recreational youth sports such as Little League Baseball, the American Youth Soccer Organization (AYSO), and similar youth sports organizations. By 2005, such a facility should be in place on Birch Street just west of Valencia Avenue, as the City and Brea-Olinda Unified School District work together to create a joint school-park facility.

As the benefits of recreation activities are recognized to individuals, families, and the community-at-large, the City anticipates that strong demand for parks and recreation will continue. New residential developments will be required to dedicate land and/or pay fees to help the City maintain its parkland standard, both in terms of overall acreage and facilities appropriate to meet the needs of residents.

With regard to park location, service area standards recommended by the National Recreation and Parks Association (NRPA) indicate that a park typically should cover a 1/2- to 1/4-mile service area radius, depending on the size of the park. Larger parks, such as Carbon Canyon Regional Park and Chino Hills State Park usually have a much larger service area, meeting park demand beyond the City of Brea.

Based on this standard, there are few areas in Brea that lack park and recreation facilities except within Brea's sphere of influence (Figure CR-1). Park facilities within the Tomlinson Park Specific Plan will relieve recreation demand in the Northwest Neighborhoods not served by Woodcrest Park. The City will carefully examine opportunities for public park space in Brea's sphere of influence whenever new development is proposed for the area. Similarly, any new residential neighborhoods developed east of Valencia Avenue in the City will be required to provide local or community park facilities to meet the demand of new residents and to better serve neighborhoods that do not have ready access to parks.



*Skateboarders at Arovista Skate Park enjoying an afternoon perfecting their skating abilities.*

In the Orange County General Plan, the Olinda Alpha Landfill is designated as a future County Regional Park. Landfill properties cover about 562 acres. Once the landfill has closed, the County's intent is to provide "urban-natural" and wilderness areas, and to provide active and passive recreational opportunities.

## Open Space

The Puente and Chino Hills form a backdrop to Brea that harkens back to its rural beginnings. The hills provide vast areas of open space containing vital biological resources and wildlife habitat areas. Approximately half of the open space areas within Brea's sphere of influence are privately held lands which have been, and to a degree continue to be, exploited for the oil and gas that lie beneath. Other property owners include the State of California (Chino Hills State Park), Orange County (Olinda Alpha Landfill), and the City of Industry.



*View of Puente Hills from Brea Boulevard.*

The canyons that snake through the hillsides are dotted with oil wells and oil production facilities. Many of these facilities have been in operation for over 100 years, and in some areas, past practices have eroded slopes and degraded natural habitat area. As oil field operations have consolidated, however, nature has displayed its tremendous resiliency by reclaiming stream valleys and grass-covered hills. Wildlife use the hills as habitat and movement corridors. As open space throughout Southern California continues to disappear, Breans increasingly value and enjoy the open space areas that make Brea unique. As a community, Brea has grown progressively more interested in issues surrounding these open spaces.

Open space issues were the focus of seven separate planning efforts between 1986 and 2001. These efforts included the 1986 Open Space Element of the General Plan, the Brea Project, the Sphere of Influence Vision Building workshops, creation of a Hillside Management Ordinance, the Open Space Task Force, and the Parks, Recreation, Open Space and Human Services Master Plan. Through these programs, the community identified the preservation of open space as significant for three key reasons:

- **Environmental Sensitivity:** To preserve and protect resources, habitat, scenic and/or aesthetic areas
- **Enhanced Recreation:** For additional passive and active recreation areas
- **Quality of Life:** To preserve rural characteristics while maintaining or improving traffic, noise, and air quality



*Open space in Telegraph Canyon. Protected open spaces are increasingly recognized as vital to improving the quality of life.*

## Goals and Policies

The City's role in providing parks, recreation, and open space relates directly to community well-being, livability, and quality of life. The City is committed to providing, facilitating, and supporting parks, recreation facilities and programs, and open space of the highest quality and in a cost-effective manner to enhance the living environment for all residents. Further, the City believes that pursuits of the individual must be carefully examined in light of the general public interest, and specifically the conservation of natural resources.

**Parks and Recreation**

To meet park facility needs of future Breans, new parks and land will need to be acquired incrementally. New park facilities will be designed to allow for changing community needs. Ensuring adaptability in park design in response to demographic shifts allows for greater options and flexibility. Upgrading and maintaining existing parks is vital for their long-term usability. The City is committed to continued investment in its parks as resources are available. The City is also committed to providing new facilities, especially in underserved areas or new types of facilities such as a sports park.

**Goal CR-1 Provide a variety of parks and recreation facilities that meet the diverse needs and interests of the community.**

Policy CR-1.1 Develop a high-quality network of parks and recreational facilities that meet the needs of families, young adults, seniors, children, and disabled individuals.

Policy CR-1.2 Provide similar or equal levels of parks and recreational facilities to all areas of the community.

Policy CR-1.3 Use the following as standards for park development, recognizing that the function of a particular park also affects classification within the system:

<b>Type of Park</b>	<b>Size and Service Area</b>
Smaller green spaces in urban areas (mini, tot lots, and/or pocket parks)	0.5 to 5 acres, with a ¼ mile service area radius in residential setting
Neighborhood Park	5-10 acres, with a ¼- to ½-mile service area radius
Community or Sports Park	20-50+ acres, with a ½- to three-mile service area radius
Regional Park	50 acres or larger

Policy CR-1.4 Incorporate into large-scale residential developments small neighborhood parks and greens suitable for unstructured play and passive recreation.

Policy CR-1.5 Develop, wherever possible, recreation facilities that have multi-use capabilities and high degree of adaptability to more intensive use or uses as recreation demand changes and/or population density increases.

Policy CR-1.6 Provide similar or equal attention to the development of facilities for individualized activities (casual park use, bicycling, walking, running, skating and riding) as is given to organized recreation and sports.

Policy CR-1.7 Locate and develop a sports park that combines intensive-use lighted sports facilities with shared support facilities such as ample parking, concessions, and restrooms.

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**Goal CR-2 Protect and preserve existing parks and recreation facilities.**

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Policy CR-2.1 Protect existing public parks and open space areas from non-recreational uses.

Policy CR-2.2 Ensure that sports facilities for organized sports do not displace existing casual use facilities and parks.

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**Goal CR-3 Maximize use of open space areas capable of supporting park-type activities.**

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Policy CR-3.1 Maximize use of available facilities through careful scheduling.

Policy CR-3.2 Continue the school/park joint use concept for increased recreational resources and year-round use of these facilities.

Policy CR-3.3 Use Carbon Canyon Regional Park, Craig Regional Park, and Army Corps of Engineer properties to satisfy some of the City's recreational demands, particularly as they pertain to facilities that require large, relatively level land, such as sports park fields.

Policy CR-3.4 Explore the recreational potential of publicly owned lands and utility rights-of-way.

Policy CR-3.5 Coordinate efforts with other public agencies regarding State and federal programs for existing and potential trail systems, recreational facilities, and recreation programs.

Policy CR-3.6 Encourage the development of recreational facilities by the private sector, including small parks and large-scale facilities requiring a high level of supporting services, supplies, and maintenance. Recreational facilities should be available to all members of the public.

Policy CR-3.7 Develop parks and recreation facilities in a manner that ensures that a minimum of damage to the environment occurs, while still providing a high quality recreation experience.

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**Implementation  
Guide** 

See Section VI in the Implementation Guide.

**Open Space**

“Open space is a defining characteristic of Brea. The prominent ridgelines and hillsides to the north of Brea form a dramatic backdrop to the entire community. Moreover, many of these ridgelines and hillsides are connected by expansive open spaces. Although not all pristine, these open spaces are valued by Breans and yet these lands are owned by others. Many believe that open space is one reason why Brea remains so special.”

– Final Report of the Open Space Task Force, June 1994

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**Goal CR-4    Preserve open space aggressively for diverse purposes – as a visual and scenic resource, for habitat conservation, to protect watersheds, and for recreation.**

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Policy CR-4.1 Protect and preserve open space wherever possible.

Policy CR-4.2 Select areas for open space preservation using an evaluation system that incorporates the following selection criteria: connectivity, access/recreations, sensitive areas, natural features, subdivision pattern, and buffer zones.

Policy CR-4.3 Work aggressively with the Orange County, Los Angeles County, State, and other appropriate public agencies, private entities, and landowners to conserve, protect, and enhance open spaces and natural resources, particularly within the sphere of influence.

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**Goal CR-5 Provide a flexible and balanced open space and conservation plan.**

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Policy CR-5.1 Create an open space network that is part of both the natural and urban fabric of Brea and connects to the regional open space system.

Policy CR-5.2 Encourage compatible uses and activities near open space areas such as schools, parks, residential, and agricultural uses.

Policy CR-5.3 Develop and maintain strong relationships with local and regional environmental and conservation organizations.

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**Implementation  
Guide**



See Section VI in the Implementation Guide.

# TRAILS

Accessibility between neighborhoods, employment centers, civic and cultural areas, wilderness areas, schools, and other social and natural places unifies the community. Pedestrian trails and bikeways enhance community mobility and provide opportunities for recreation and exercise. A well-defined, interconnected trail system also reduces dependence on the automobile. The key is designing a system comfortable, suitable, and safe for those who wish to use it.

Trails in hillside areas allow access into open space and parkland areas and provide recreational activities for the avid hiker, mountain biker, naturalist, and equestrian. Urban trails increase the connectivity within the urban fabric of a city. Cities and urban centers are stronger and livelier when access is easily accomplished by good public transportation, bicycling, or on foot. Brea can enjoy both wilderness and urban trails through a trail system designed to increase connectivity.



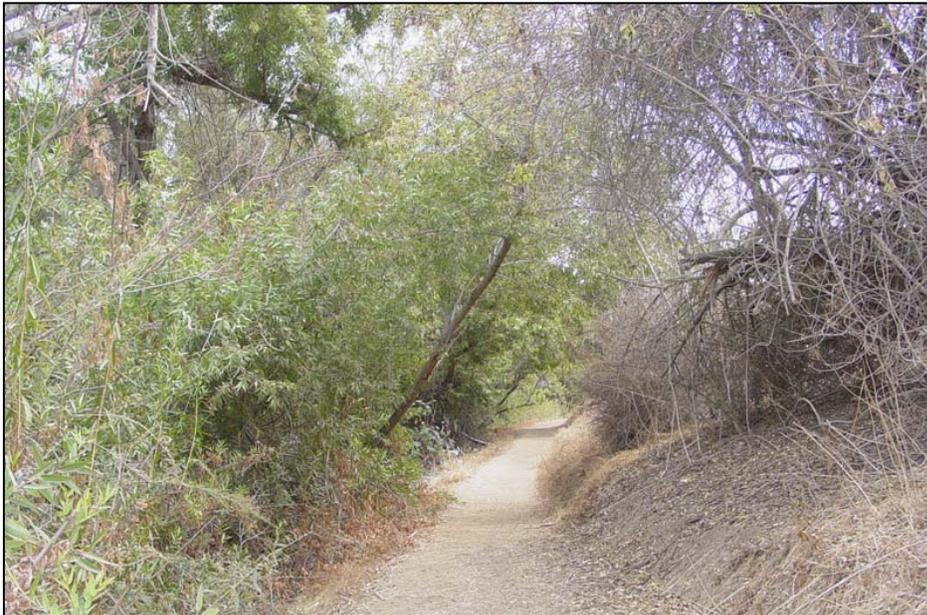
*Equestrian and pedestrian trail along Valencia Avenue will expand Brea's existing trail network and allow connection points into Carbon Canyon Regional Park and Chino Hills State Park.*

## Context

The hillsides surrounding the City and within Chino Hills State Park contain a network of both informal and improved trails. Few similar pedestrian ways have been established within the urban area, other than sidewalks and a few improved multi-use trails, but tremendous opportunities present themselves for linking dynamic activity centers citywide and making it possible to get anywhere in Brea and into natural open space areas without a car. The partially abandoned railroad right-of-way corridor that runs through the City represents just one potential.

### Natural and Wilderness Trails

Natural and wilderness trails are multi-use trails for walking, hiking, biking, and horseback riding. In Chino Hills State Park, trails include fire access roads and dirt trails. Similar trails traverse private properties throughout the hills, which people use though access may be restricted. These trails allow passage into wooded and open space areas, excellent for the enjoyment of natural surroundings.



*Nature trail located in Carbon Canyon Regional Park leads hikers to a 10-acre Redwood grove adjacent to Carbon Canyon Dam.*

### Classification

Trails are identified in three general classification categories:

- *Regional Trails:* Serve as service trails which are included in the Orange County Master Plan of Hiking and Riding Trails. Regional Trails double as major links in Brea's trail system. These are all multi-use trails suitable for hiking, mountain biking, and horse riding.

- *Community Trails:* These are multi-use trails which are either a service road, a rail right-of-way, a flood control channel right-of-way, or adjacent to a street right-of-way. They can also be used by equestrians or hikers. Some of these trails are in an urban setting, while others are in open space areas.
- *Local Trails:* These are public trails that serve a local purpose. The trails are usually short and may connect to the major and regional trails. The trails could be dedicated to a single use such as hiking or riding.



*Mountain biker riding down Telegraph Canyon Trail in Chino Hills State Park, a Regional Trail.*

### **Staging Areas**

Staging areas connect the urban and natural areas. They link an existing public facility or street right-of-way to important parkland or open space such as Chino Hills Park. A linkage normally consists of two components: (1) a staging area where people can gather and park and (2) the trail head. Additional staging areas are needed at various points along Brea's trail system to enhance accessibility. Nine such staging areas have been identified in the Trails Master Plan. These should be built out in conjunction with related trail segments. Trail heads that include amenities for hikers, bicyclists and equestrians to prepare for, start, and return from hikes should be located strategically throughout the City of

Brea's trail system. Staging areas and trail heads should include, as applicable:

- Identification signs, directions signs, and maps
- Marked parking stalls to accommodate horse trailers
- Water for horses
- Water for hikers and riders
- Hitching posts
- Picnic tables
- Shade trees
- Restrooms

### **Chino Hills State Park**

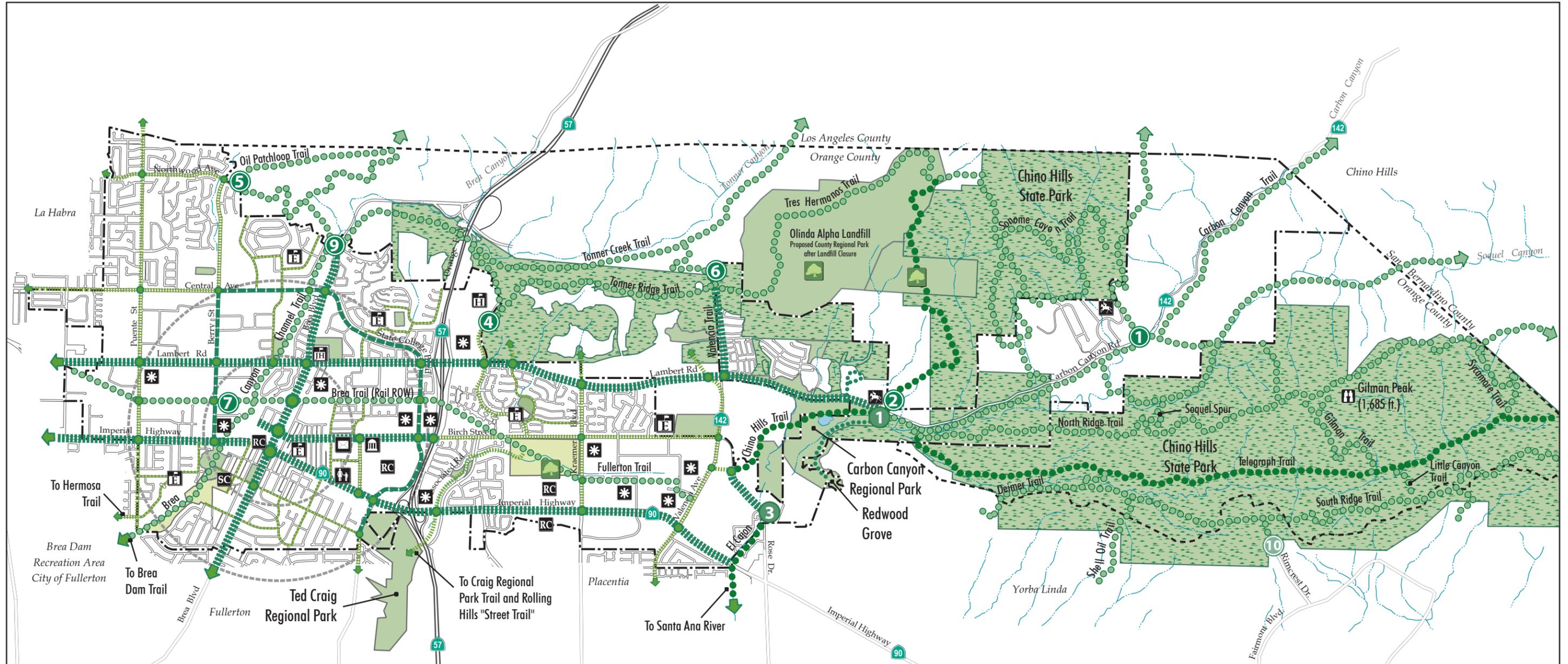
Chino Hills State Park offers 13,000 acres of wilderness area and over 65 miles of hiking, mountain bike riding, and horseback riding trails. The trails allow for passive activities such as bird watching, photography, and nature study, and access many of the park's secluded areas, including riparian areas, open grasslands, and scenic view points. This system of trails should be integrated to Brea's proposed trail system to create a seamless network connecting urban and wilderness areas.

A one-mile-long nature trail in Carbon Canyon Regional Park provides access for hikers, bird watchers, and other amateur naturalists to the park's beautiful Redwood Grove. The park also contains an equestrian trail.

The opportunity to create trails in Carbon and Tonner Canyons and across other hillside properties not within Chino Hills State Park will exist as the properties become publicly owned or are acquired by private conservation organizations interested in opening properties to public access. Additionally, any development proposal can be conditioned to provide public trails consistent with the City's Trails Master Plan.

## **Urban Trails and Sidewalks**

Walking is a common leisure and recreational activity, as well as a means to get around town; the urban environment should be amendable to those who like to walk and particularly to school children. Sidewalks should be continuous and part of a system that provides access to goods, services, schools, and homes. Creating an extended network of sidewalks that are comfortable and safe for pedestrians increases accessibility and creates "eyes on the street" in residential neighborhoods. Figure CR-2 illustrates the network of urban and walkway trails that will link all parts of Brea.



Source: City of Brea, Cotton/Bridges/Associates, RTKL, and Brea Chamber of Commerce; 2002.



Figure CR-2  
Trails Plan



In a built-out urban environment like Brea, creating pedestrian/bike ways separated from the street is difficult. However, the rail right-of-way that extends more than four miles from Valencia Avenue west to the western City accesses several activity centers (Figure CR-2). Throughout California and the nation, communities have acquired such rights-of-way to create multi-purpose trails, and Brea aims to accomplish the same. Issues that will need particular consideration in establishing the trail include user safety, interface with adjacent residential neighborhoods, easy and safe access, maintenance, and connectivity to other trails and walkways.



*Trail opportunities await along the rail right-of-way.*

On the street system, Brea will provide improvements to encourage walking and biking, particularly between residential neighborhoods and schools and parks, as well as to commercial centers. Improvements will take the form of landscaped buffers between the road and sidewalk, and/or dedicated trails where adequate right-of-way exists. Sidewalks should be wide and spacious, with decorative accent paving materials located at major intersections and pedestrian crosswalks along the route. Intersections will be designed to allow easy crossings and to alert motorists to pedestrians. Key roadways to receive such treatment include:

- Lambert Road
- Brea Boulevard south of Lambert Road
- Birch Street east and west of SR-57
- Valencia Avenue
- Elm Street between Arovista Park and Craig Regional Park
- Kraemer Boulevard
- Berry Street
- Puente Street

Special design studies of Birch Street will be conducted to determine an appropriate mixed-use path that links Downtown to the Civic and Cultural Center and Brea Mall, and neighborhoods east of SR-57 to the new sports park and middle school.

Flood control rights-of-way represent additional areas where trails can be established, where it is safe to do so. The Trails Master Plan (Figure CR-2) incorporates channels.



*Trees and other landscaping provide a pleasant atmosphere for the pedestrian.*

### **Bikeways**

Bikeways form an important component of Brea’s recreation and circulation system. The Circulation section of the Community Development Chapter addresses bicycle paths.

## **Goals and Policies**

All types of trails will work together to allow easy transition from urban trails to wilderness trails and back again. New developments will require incorporation into the trail system and removal of potential barriers to the trail network.

<b>Goal CR-6</b>	<b>Provide an extensive trail system that links all areas of Brea.</b>
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Policy CR-6.1 Create linkages to trails within Carbon Canyon and Chino Hills State Park existing and proposed trail system.

Policy CR-6.2 Coordinate trails placement with landowners and conservation biologists knowledgeable of the area.

Policy CR-6.3 Provide a useful, enjoyable, safe, and efficient trail system for equestrians and hikers, with the following objectives and standards:

- Provide multi-purpose trails, where possible, to serve hikers, bicyclers, and horseback riders
- Link trails with adjacent City, County, and State trail systems
- Maintain trail areas in good condition, and free of litter and debris
- Design trails to be flexible and site-specific to minimize the impact on adjacent property and fragile habitats
- Provide a trail system with both short and long hikes/rides and serve the needs of both beginning and advanced hikers/riders
- Utilize citizen volunteers to assist in the development, maintenance and operation of trails and facilities
- Keep citizens aware of the trail system through publication of a trails map which also notes safety and courtesy tips
- Separate trails from automobile traffic when possible in order to provide safe conditions for riders and walkers
- Provide appropriate signs to mark all trails
- Design trails entrances to prevent unwanted trail usage by motorized vehicles
- Locate trails to provide linkages between open space and the City greenway system

Policy CR-6.4 Work to incorporate recreational amenities such as trail systems, bike paths, and jogging paths with existing drainage ways, open-space corridors, and utility rights-of-way so that natural resources are retained as assets in the community's recreational system and natural environment.

Policy CR-6.5 Coordinate efforts with other public agencies regarding State and federal programs for existing and potential trail systems, recreational facilities, and recreation programs.

Policy CR-6.6 Develop the trail system illustrated in Figure CR-2.

Policy CR-6.7 Require new developments to provide access and linkage to the citywide trail system.

**Goal CR-7 Encourage an urban and walkway trail system within the urban areas of the City.**

Policy CR-7.1 Encourage the development of landscaping, walkways, and bike trails that provide direct pedestrian access between work places and residential neighborhoods.

Policy CR-7.2 Provide shielded safety lighting along trails and other public and private walkways separated from a street.

Policy CR-7.3 Acquire abandoned rail rights-of-way to create a continuous multi-purpose trail through Brea. Encourage the creation of small parks, rest rooms, and recreational facilities such as picnic tables and basketball courts along the trail system.

**Implementation  
Guide**



See Section VII in the Implementation Guide.

# WILDLIFE HABITAT

Preserving and protecting wildlife habitat equates to the preservation and protection of wildlife species. The great diversity of vegetation types and habitat located in the hillsides of Brea support a wide variety of animal populations. Natural habitat such as riparian areas provides food, cover, and shelter for birds, mammals, reptiles and insects. Wildlife corridors provide areas of undisturbed open space that allow regional wildlife migration between natural habitats, promoting proliferation of indigenous species.

Why is it important to preserve wildlife and promote biodiversity? Biological systems have interrelated dependencies, and they require options and opportunities to survive. If we protect habitat, we increase the probability that important and critical species will survive and flourish. Wildlife and the habitat they live in enhance our own life experiences. Creating the balance between habitat preservation and meeting people's needs for housing, jobs, and resources represents a key planning challenge.

## Context

The Puente and Chino Hills above Brea are just part of a much larger natural area that extends to the Santa Ana Mountains and Cleveland National Forest to the west. The Cleveland National Forest contains an enormous amount of natural open space and habitat for animals, stretching from San Bernardino County to San Diego County. Incremental transformation of wilderness to urban uses, including areas of Brea, has significantly reduced and will continue to reduce habitat for wildlife species and sever connections to larger habitat resources.

## Wildlife Species

Among the diverse wildlife species within the Brea planning area are sensitive species, some of which have protected status under the federal Endangered Species Act and various California statutes. "Sensitive" means any wildlife species native to California that is vulnerable or declining and is likely to become endangered or threatened in a significant portion of its range within the State without cooperative management or removal of threats.

Some of the larger mammals known to inhabit the hills, and sometimes visit urban areas on the hillside fringes, include mountain lions, deer, and coyotes. Large birds and raptors such as hawks, owls, falcons, eagles, and vultures have been observed

## *Imagine Brea*

foraging and nesting. Smaller birds sighted include wrens, gnatcatchers, quails, babblers, starlings, crows, ravens, woodpeckers, flycatchers, and hummingbirds. Sensitive species confirmed include mountain lions, Cooper's hawk, golden eagle, coastal cactus wren, California gnatcatcher, least Bell's vireo, coastal western whiptail lizard, northern red diamond rattlesnake, black-bellied salamander, Monterey salamander, and the southwestern pond turtle.<sup>1</sup>



*Coyotes can be found wandering Brea hills, especially along the canyons in Chino Hills State Park.*

More than 200 species of birds and mammals, numerous reptiles and amphibians, and thousands of types of insects and other invertebrates live within Chino Hills State Park. Because of its great variety of habitats and microclimates, the park is an ideal location for observing many wildlife species native to Southern California. Coyotes, deer, bobcats and other mammals are often seen in the woodlands, scrub, and grasslands. The local diversity of native plants and animals found here in the Southern California basin is greater than in any other area of comparable size in the United States.

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<sup>1</sup> California State Parks. *Chino Hills State Park General Plan*. February 1999; Brea Highlands Specific Plan EIR. City of Brea. November 2000.

## Wildlife Corridors

Wildlife corridors are links connecting large patches of habitat that have become separated by the process of urbanization or otherwise made unavailable for use by native wildlife. They allow for the free movement of wildlife between disjunct habitats and thereby provide access to mates, food, and water; diminish competition for resources by facilitating the dispersal of dense populations; and serve as escape routes from fire, predators, humans, and urbanization.

Wildlife corridors are considered an important declining resource in California because ongoing urbanization has separated remaining natural open space, interrupting wildlife movement and isolating populations. The primary threats to wildlife connectivity in the Southern California region include urbanization, roads, invasive species, and agriculture.<sup>2</sup> Scientific studies have shown that the isolation of habitat can lead to ecosystem collapse. Small, isolated areas of habitat simply cannot support as many species in a larger area.

The Puente and Chino Hills have become increasingly isolated by the conversion of the surrounding natural landscape to urban uses. The Puente and Chino Hills and Santa Ana Mountains together encompass about 511,000 acres of wildlands. This area supports important biological resources, including several rare and endemic communities. Large blocks of habitat ridgelines along the Puente and Chino Hills allow wildlife to move throughout the hills and into the large open spaces of Chino Hills State Park and the Cleveland National Forest.

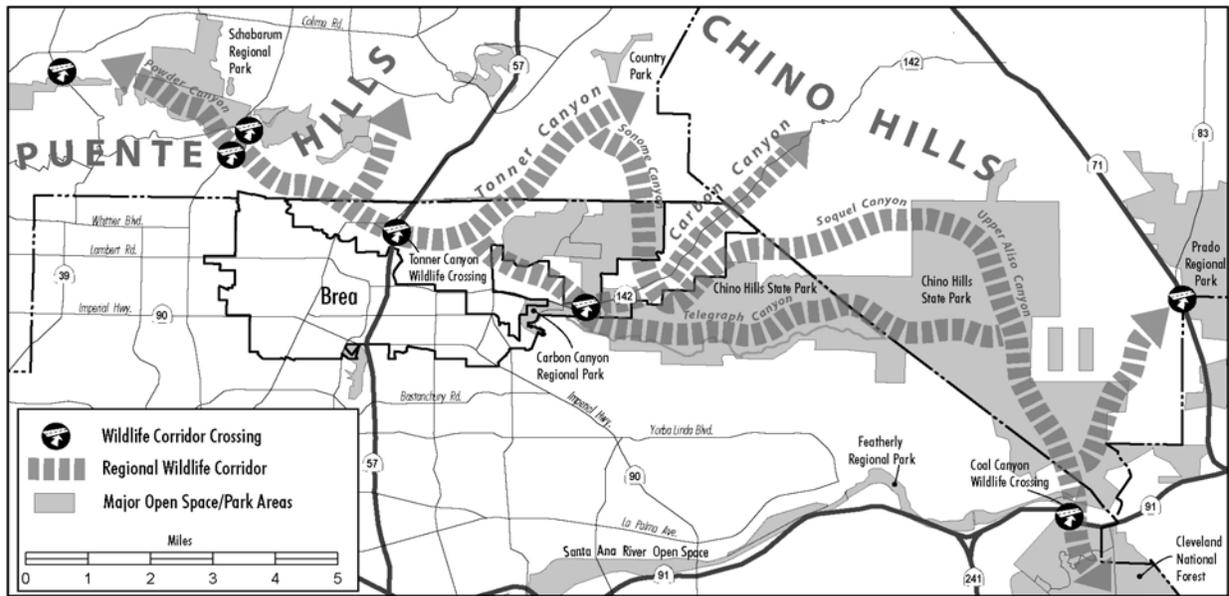
Chino Hills State Park and the Puente and Whittier Hills are linked via the Tonner Canyon and Sonome Canyon wildlife corridors, as illustrated in Figure CR-3. Tonner Canyon is considered a major wildlife movement corridor, providing year-round water and shelter for small and large mammals. The Tonner Canyon wildlife crossing at the intersection of SR-57 and Tonner Canyon Road provides wildlife access beneath the freeway. However, the Sonome Canyon wildlife corridor is bisected by Carbon Canyon Road, where several culverts pass under the road. These culverts are considered inadequate for larger mammals such as deer and mountain lions because of their size. Animals must cross Carbon Canyon Road at grade and avoid automobiles in order to leave Chino Hills State Park through the corridor.<sup>3</sup>

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<sup>2</sup> California Wilderness Coalition. *Missing Linkages: Restoring Connectivity to the California Landscape*. November 2000.

<sup>3</sup> California State Parks. *Chino Hills State Park General Plan*. February 1999.

Figure CR-3 Regional Wildlife Corridors



Source: *Parklands and Wildlife Corridors Map* - Santa Monica Conservancy, Wildlife Corridor Conservation Authority, Puente Hills Landfill Native Habitat Preservation Authority, and Dreamline Cartography; March 1999.

### Wildlife Corridor Conservation Authority (WCCA)

The Wildlife Corridor Conservation Authority (WCCA) is a local joint powers authority represented by city and State agencies. The mission of WCCA is to provide for the proper planning, conservation, environmental protection, and maintenance of the habitat and wildlife corridor between the Puente Hills in the west and the Chino Hills in the east, which connects to the Cleveland National Forest, and the east-west connections between Chino Hills State Park and the Prado Basin.

### Plant Communities and Habitat

The following paragraphs describe major plant communities within the planning area. Each plant community contains unique features and supports a variety of wildlife species.

#### *Chaparral Habitat*

Chaparral consists of evergreen, medium-height to tall shrubs which commonly cover hills and slopes of Southern California. This community is highly adapted to drought and fire conditions. Shrub canopy cover is generally continuous. California sagebrush and California buckwheat occur within the understory of larger shrubs. The chaparral community is ecologically fitted to a cycle of fire destruction. Periodic fires help preserve the mixes of plant species and reduce the danger of a more catastrophic fire.

Chaparral provides food and cover for reptiles, birds, rodents, coyote, and deer.

*Grassland Habitat*

Grasslands consist of low herbaceous vegetation dominated by grasses. They thrive in deep, well developed soils on gentle slopes and flats, mostly at low elevations. Three types of grassland are found in the area: native grasslands, non-native annual grasslands, and ruderal grasslands. The grasslands are green during the rainy season and spring, but with the advent of summer dry into a golden mantle. The grasslands must be managed to prevent fire hazards and the intrusion of weed species, chaparral, and invasive plants such as star thistle. Grasslands provide forage for cattle and other grazing animals, and habitat for small reptile, rodents, deer, coyote, songbirds, and birds of prey.

*Riparian Habitat*

A riparian community is a combination of plant species that thrive along intermittent and perennial waterways. These waterways can be found at the bottom of the many canyons found in the hillsides, including Brea, Carbon, Tonner, Sonome, Soquel, and Telegraph Canyons. Creeks and streams and the associated riparian vegetation provide wildlife habitat, stormwater drainage, visual backdrops, and recreation corridors. Riparian habitats are considered among the most valuable habitats for wildlife because of the presence of water, lush vegetation, and high insect populations. Riparian habitat provides cover and food for numerous animals and nesting birds. Many nesting birds are migratory species that come to the streamside habitats from Central and South America each spring to raise their young.

*Sage Scrub Habitat*

Sage scrub can be defined as low-drought-deciduous and evergreen shrubs that occur on steep to moderate slopes mostly below 3,000 feet in elevation. It is considered a sensitive habitat due to its potential to support threatened and endangered species. Common animal species include deer, rabbit, bobcat, and coyote, as they utilize the scrub habitat as part of a larger home range.



*California Sage brush (Artemisia californica), is a low ashy-gray shrub found throughout Brea's planning area.*

#### *Woodland Habitat*

Woodland is a plant community with a diverse mix of tree species forming a dense overstory of vegetation. Woodlands in Brea are primarily located on the north and east facing slopes of the canyons. Common trees in the woodland community include coastal and interior live oaks, balk oak, valley oak, California bay laurel, buck eye, and madrone. Insect- or seed-eating birds and mammals are common in the woodlands and are preyed upon by raptors and owls that also inhabit these areas. The larger mammals utilize these areas as well, including deer, coyote, and foxes.

Diversity is perhaps the most important feature of the vegetation found within Chino Hills State Park and surrounding hillsides. Within creek and riparian areas, cattail stands provide habitat for a variety of wildlife. Along seasonal and year-round creeks, the willow and sycamore woodlands have understories of wild rose, stinging nettle, and mule fat. Southern California black walnut trees, sycamores, and coast live oaks form woodlands above the creeks, often on north facing slopes. These walnut woodlands are another important and rare plant community preserved in Chino Hills State Park and undisturbed hillside areas. Only a few thousand acres of this California habitat still exist, with about one thousand acres in preserves. Several hundred acres are protected at Chino Hills State Park.



*Mugwort (Artemisia) can be found on hillside along Carbon Canyon Regional Park.*

The Tecate cypress is another special type of plant community found only in a few places in the United States. Several different scrub and chaparral communities along the hills and slopes above the canyon floors include coastal sage scrub, California sagebrush, California buckwheat, and purple sage, as well as a mixed chaparral community dominated by laurel sumac and toyon. Many California wildlife species depend on these scrub and chaparral communities for survival. Because these communities are disappearing as urban development continues, they form an increasingly important part of the biological resources protected in the park.

Most of the grassland in the park is non-native annual European grasses that were introduced here during the early ranching years. However, grassland species native to California, such as purple needle grass and giant rye can be found among the annuals. An active grassland restoration program in the park is restoring native grassland to its more natural and dominant state.

## Goals and Policies

Brea has resolved to protect wildlife habitat by preserving wildlife corridors and the vegetation communities that make up the habitat areas. While many thousand acres receive protection due to their location within Chino Hills State Park, the difficulty lies in maintaining linkages to valuable habitat on private

property. Without financial resources to purchase the properties worthy of permanent open space status, the City must look to creative approaches. Land use policy set forth in the Community Development Chapter provides incentives to land owners to avoid sensitive areas whenever development is proposed. The City fully supports efforts of conservation agencies and organizations to purchase properties for preservation.

## Wildlife Corridors

Preserving wildlife corridors allow animals to move freely between large open space areas from protection of predators, urbanization, and wildland fires, and to further promote their individual species. When future improvements to roads are undertaken, including capacity increases planned for the regional transportation system, the construction or enhancement of suitable wildlife crossings, such as bridges or culverts, will be required to maintain corridor function and biological viability.

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<b>Goal CR-8</b>	<b>Preserve and maintain wildlife and animal movement corridors</b>
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Policy CR-8.1 Preserve key wildlife migration corridors and habitat areas.

Policy CR-8.2 Provide adequate wildlife crossings where roadways have severed habitat areas.

Policy CR-8.3 Cooperate with regional agencies and authorities with similar goals in protecting and enhancing wildlife and animal movement corridors.

Policy CR-8.4 Regular monitoring of medium and large mammals is necessary to gauge the effectiveness of wildlife corridors and to identify or increases in wildlife populations.

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<b>Goal CR-9</b>	<b>Preserve and maintain open space, natural habitat, and vegetation communities that support wildlife species and animals.</b>
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Policy CR-9.1 Support regional and sub-regional efforts to acquire, develop, operate, and maintain an open space system extending from the Puente Hills to the Chino Hills.

Policy CR-9.2 Preserve the integrity of blue line streams and riparian habitat areas.

Policy CR-9.3 Preserve and restore the habitat value of creek corridors through the preservation of native plants and the replacement of invasive, non-native plants with native plants.

Policy CR-9.4 Protect sensitive plant species resources from the impacts of development

Policy CR-9.5 Manage areas of diverse wildlife habitat as a natural resource and prevent major destruction or disruption.

Policy CR-9.6 Use specific management programs using sound ecological principles and professionally accepted methods are necessary to protect and restore sensitive animal populations and their habitats.

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**Implementation  
Guide**



See Section VIII in the Implementation Guide.

## SCENIC RESOURCES

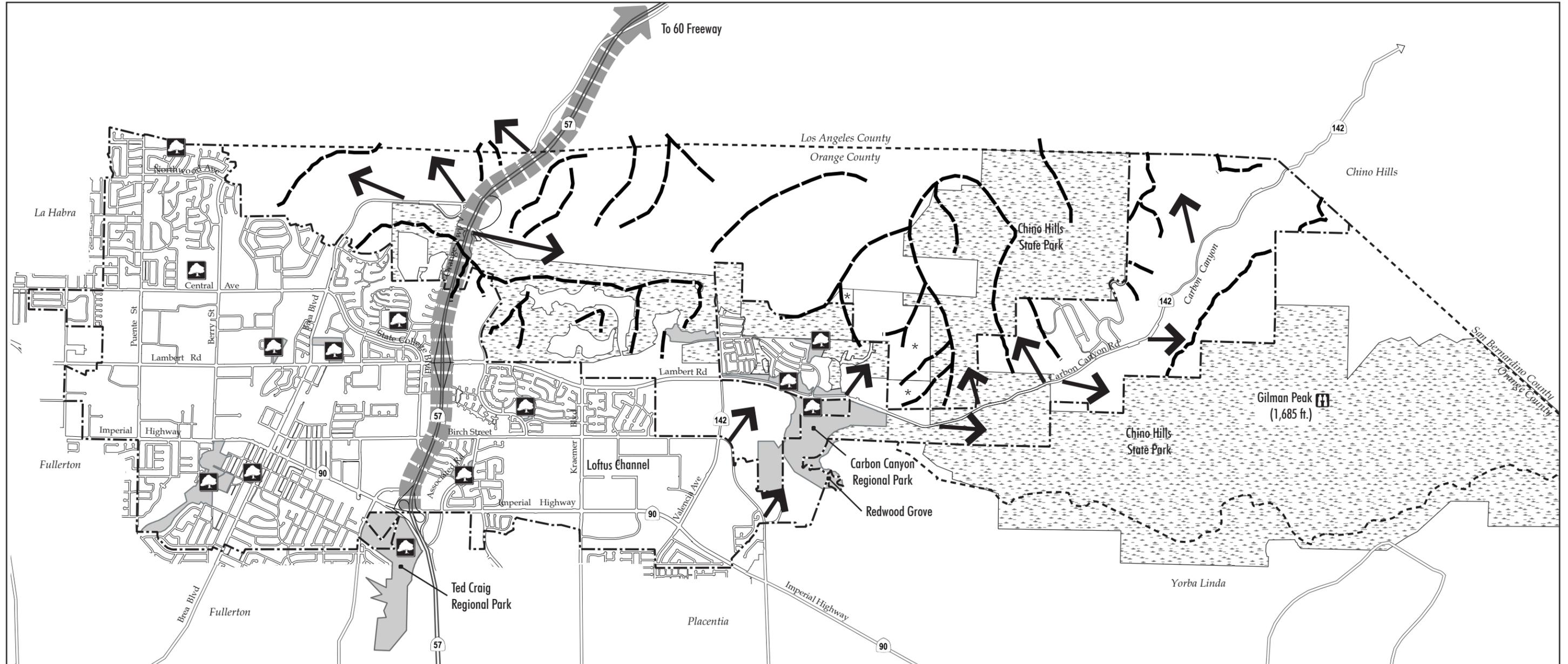
Scenic resources enhance the visual character of the community and provide distinguishing characteristics, an invaluable asset that benefits a community. The scenic qualities of the Puente and Chino Hills include prominent ridgelines, scenic corridors and canyons, view corridors and vista points, roadways through undisturbed habitat, highways, and natural landscaping (Figure CR-4). The drive through Brea Canyon reminds a motorist of the region's ranching and oil producing heritage and provides an easy transition into the urban environment.

### Context

#### Hillsides and Ridgelines

The hillsides above Brea offer several scenic benefits to the community. They are used as landmarks and offer a sense of direction or orientation to people moving about. Hillsides create edges that may define an area or watershed. They beckon people to a sanctuary from the urban environment.

Brea's future should imagine a future that preserves the hillsides to the maximum extent possible. As discussed above, this means balancing the desire with recognition of private property rights. A key concept of this General Plan is to define permitted land use intensities within remaining open hillside areas in a manner consistent with the vision. Land use development intensities and corresponding performance standards in the Community Development Chapter, implemented largely through the Hillside Management Ordinance, aim at maximizing the amount of hillside area left in its natural state. Strategies include limiting densities and grading, prohibiting development on identified significant ridgelines, and encouraging clustered development on flatter portions of development sites.



Source: City of Brea; Cotton/Bridges/Associates, 2002. □

**Legend**

- City Boundary
- - - Sphere of Influence

**Scenic Resources**

- Parks and Recreation Facilities
- Dedicated Open Space
- Prominent Ridgelines
- View Corridors
- Scenic Viewpoint



SR-57 Freeway: Between Imperial Highway and SR-60 freeway, eligible for California State Scenic Highway status

*Note:*

\* Parcels identified with an asterik have the potential to become mitigation bank properties. This does not preclude any other parcel within the planning area from being designated for or used as mitigation bank properties.

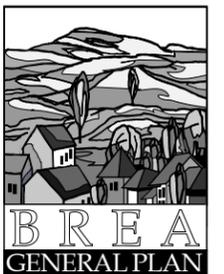
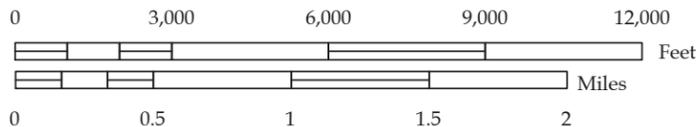
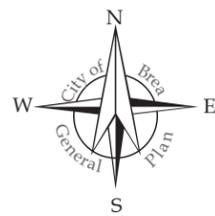


Figure CR-4  
Scenic Resources





*Hillsides in Chino Hills State Park provides breathtaking views of Orange County and surrounding hillsides.*

**Vistas** Vista points can be found throughout Brea both from urban areas toward the hills and from wilderness areas looking back onto Brea. The landscape in Chino Hills State Park has been relatively unaltered by the works of man, especially compared to the surrounding environment. Long distance views of natural terrain and vegetation can be found throughout the park. The Chino Hills State Park Master Plan emphasizes acquiring ridgelines and ridgetops to protect viewsheds within the park.<sup>4</sup> Pristine views of the hills from Telegraph Canyon and from selected panoramic points have been mostly protected from urban encroachments. A viewpoint of particular interest is Gilman Peak (1,685-foot elevation) located in the park. Main trails take hikers and cyclists to points with breathtaking views of Orange County and the surrounding hills.

The ridgelines from Sonome Canyon, Soquel Canyon, and Lions Canyon provide scenic view points of natural rolling hills with southern California black walnut and coast live oak trees found on the north-facing slopes. Seasonal color transformations of vegetation throughout the year change from green during spring to golden brown during the summer.

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<sup>4</sup>Chino Hills State Park General Plan, State Park Recreation Commission, 1999,

## *Imagine Brea*

### Scenic Highways

Both Brea Canyon Road and Carbon Canyon Road traverse through windy canyons and into areas with little or no development. These roads offer views of the natural landscape. Carbon Canyon Road is a two-lane State highway that is not eligible for designation as a State Scenic Highway. Nonetheless, the highway provides motorists with spectacular views of natural landscapes with vegetated valleys, riparian corridors, and steep topographical features.



*Oak trees along  
Carbon Canyon  
Road.*

Photo credit: Hills for Everyone 2002

Brea Canyon Road leads the motorist on an historic drive into the City of Brea. This road follows the contours of Brea Canyon, giving drivers glimpses of natural features and abandoned and active oil wells. Large oil production and drilling towers point into the sky, creating a rustic image of oil industry that was the cornerstone of the community's founding. On the side of the roadway stands a marker identifying the historic expedition and campsite by Captain Gaspar de Portolá with 64 other men during their exploring trek from San Diego to Monterey in 1769<sup>5</sup>.

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<sup>5</sup> Gary S. Breschini, Ph.D., *The Portolá Expedition of 1769*. Monterey County Historical Society

The SR-57 freeway between SR-60 and Imperial Highway is eligible as a California State Scenic Highway. Properties flanking the freeway lie within unincorporated Los Angeles and Orange counties. Designating the freeway as an Official Scenic Highway will require a cooperative effort among many agencies to maintain the scenic qualities by minimizing or avoiding hillside development, and working through the California Department of Transportation to assign the designation.

## Urban Forest

While trees add considerably to the aesthetics of Brea, “urban forests” also promote a good community environment and biological benefits. They contribute to clean air, provide cooling shade, support wildlife, and provide protection from high winds. The urban forest is comprised of the street tree system, trees on park and other public lands, and trees on private properties and in yards through out the City. The urban forest is distinct within the established areas of the City where trees have fully matured. The City is committed to preserving its existing trees, replacing trees that are damaged or dying, and expanding urban forests in newer areas of Brea.



*London Plane (Platanus x acerifolia) trees planted along the meandering sidewalk on Lambert Road provide relief from the sun on afternoon walks.*

## Goals and Policies

Protecting Brea’s scenic resources will be carried out through the following goals and policies. Once a scenic resource, such as an oak tree grove, stream course, or a ridgeline is torn down and developed, it is lost forever. It is important to pursue options to protect and preserve scenic resources.

<b>Goal CR-10</b>	<b>Pursue aggressively the preservation and protection of scenic resources.</b>
Policy CR-10.1	Create and enforce special standards for development occurring within potential scenic highway corridors.
Policy CR-10.2	Identify streets with unique man-made or natural characteristics for special consideration as scenic routes.
Policy CR-10.3	Manage stands of mature trees, particularly native species, as unique and visual resources.
Policy CR-10.4	Preserve major rock outcroppings as unique landmarks and visual resources to the maximum extent possible.
Policy CR-10.5	Preserve stream courses in their natural state, as they represent a recreation resource, provide community identity, and serve as unifying corridors in the planning area.
Policy CR-10.6	Work aggressively with Orange County, Los Angeles County, State, and other appropriate public agencies, private entities and landowners to conserve, protect and enhance natural resources, particularly within the sphere of influence.

**Implementation Guide** 

See Section IX in the Implementation Guide.

# WATER CONSERVATION AND QUALITY

Water resources sustain life in both the urban and natural environments. In the communities where we live, an adequate water supply and good water quality are taken for granted. In the natural environment, water resources promote healthy ecosystems, recharge groundwater basins, and create scenic corridors. Water conservation policies and programs ensure that a healthful, reliable supply of water remains available for future residents and prevents deterioration of natural areas.

## Context

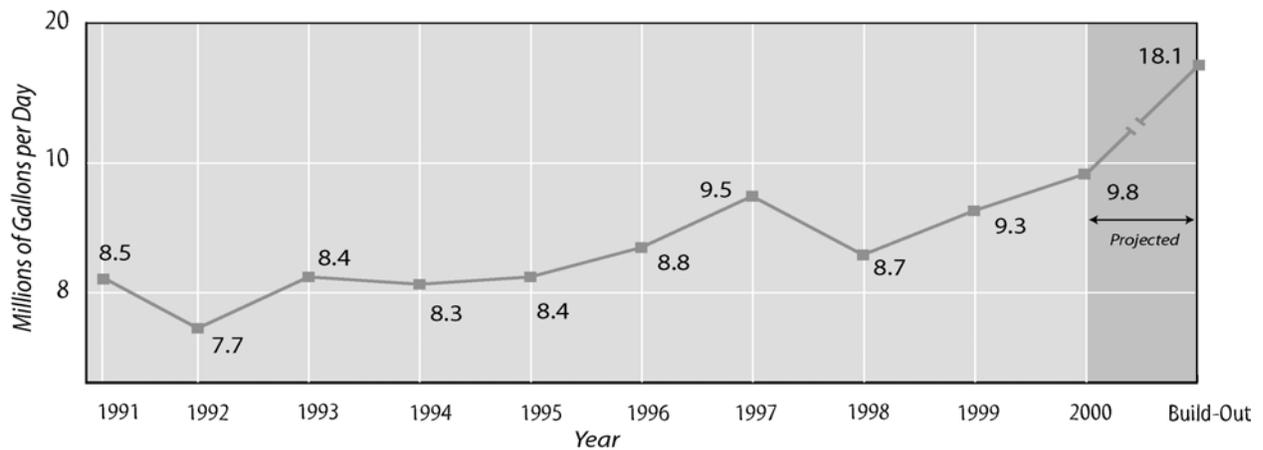
In Brea, two water agencies supply the majority of drinking water: the Metropolitan Water District of Southern California (MWD) and the California Domestic Water Company (CDWC) in Whittier. MWD supplies derive from surface water in the Colorado River and the California Water Project, which draws water from the San Francisco-San Joaquin Bay Delta. CDWC pumps water from wells in the San Gabriel Valley. These two water sources are blended to provide the water directly to households and businesses. Brea purchases an average of 11,000 acre-feet per year, with approximately 66 percent of the supply coming from MWD and 34 percent from CDWC.

The La Habra Groundwater Basin, located directly beneath the City, has poor water quality that would require extensive treatment and blending with higher quality water to meet the State's public health standards.

An abundance of perennial and intermittent streams course through the canyons in the hillsides and Chino Hills State Park. Carbon, Tonner, and Brea Canyons contain three primary trunk streams that drain the Puente Hills and eventually flow into the San Gabriel and Santa Ana Rivers. Carbon and Telegraph Canyon Creeks are the headwaters above Carbon Canyon Dam. Brea Canyon and Tonner Creeks drain into the Brea Dam reservoir located in Fullerton. Three watersheds lie within the planning area: Lower Santa Ana River, Carbon Creek, and San Gabriel River/Coyote Creek.

**Conservation** Water should be regarded as a limited natural resource, and water conservation represents the most cost-effective and environmentally sound way to reduce current and future demand for this resource. Economic and financial incentives can be used to encourage conservation and prudent use of water, although sometimes legislative mandates are required. In response to State law aimed at reducing water use for landscaping, in 1993 the Brea City Council adopted Ordinance No. 932. Simple procedures such as checking for leaks, changing to water-saving appliances, using recycled wastewater, and other measures can encourage efficient use and lead successfully to water conservation. Past water use and projected water demand for Brea is presented in Figure CR-5.

**Figure CR-5. Water Consumption and Projected Demand in Brea**



Source: City of Brea Water Master Plan Update (Draft), July 2002

### Water Quality and Pollutants

The City vigilantly safeguards its water supply, and the water delivered to residents and commercial users meets the standards required by the State and federal regulatory agencies. In fact, in many instances, the City goes beyond what is required to monitor for additional contaminants that have known health risks.

The federal Environmental Protection Agency (EPA) and the California Department of Health Services (DHS) are the agencies responsible for establishing drinking water quality standards. To ensure that tap water is safe to drink, EPA and DHS prescribe regulations that limit the amount of certain contaminants in water provided by public water systems.

For many pollutants, the primary contributor to water pollution is urban runoff. Runoff from streets, parking lots, commercial businesses, and private yards may contain oil, grease, pesticides and herbicides, heavy metals, paints and household chemicals, construction materials, sediment, and eroded soil. Ultimately,

these materials end up in creeks and storm drains that lead directly into the ocean, where they have caused substantial water quality degradation over the past century.

Urban pollutants degrade water quality and impact wildlife and plants dependent on aquatic habitat. The City is a co-permittee with the County of Orange in the National Pollution Discharge Elimination System (NPDES) program, which is designed to reduce pollutants in runoff. According to the NPDES permit, all new development projects and substantial rehabilitation projects will be required to incorporate Best Management Practices (BMPs) as identified in the County Drainage Area Master Plan (DAMP).

The Infrastructure Section of the Community Development Chapter discusses in detail storm drain capacity and urban runoff.

## Goals and Policies

As Brea grows, demand for water will increase. The following goals and policies focus on maximizing water conservation, and on promoting high water quality both in domestic supplies and surface waters that flow into the regional facilities. The City supports safe and practical applications of reclaimed water for landscaping to reduce the demand on water supplies.

<b>Goal CR-11</b>	<b>Conserve and protect water resources through water conservation standards, sustainable development practices, and water quality standards.</b>
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Policy CR-11.1      Develop water conservation plans, standards, and/or guidelines for all new construction projects to address such issues as water-conserving plumbing fixtures, on-site storm water retention, drought-tolerant landscaping, and gray water use.

Policy CR-11.2      Establish standards for the use of reclaimed water for landscaping.

Policy CR-11.3      Base water use fees and charges on a system that rewards conservation and discourages consumption beyond standard levels.

Policy CR-11.4      Promote techniques and methods for water conservation throughout the community.

Policy CR-11.5 Utilize design techniques that conserve natural resources and preserve natural terrain, drainage, and vegetation.

Policy CR-11.6 Cooperate with regional agencies, such as the Regional Water Quality Control Board, in their efforts to improve surface and groundwater water quality for all Southern Californians.

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**Goal CR-12 Protect the beneficial uses of ground and surface waters.**

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Policy CR-12.1 To the maximum extent practicable, adopt and enforce regulations and engage in educational efforts to eliminate pollution from urban runoff.

Policy CR-12.2 Evaluate development projects for compliance with NPDES requirements, aiming toward reducing pollutant loads in stormwater runoff, minimizing impervious surface areas, and minimizing peak flows.

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**Implementation  
Guide**



See Section X in the Implementation Guide.

# AIR QUALITY

Every day, each of us breathes about 3,400 gallons of air. Yet our air is contaminated on a daily basis by our activities: driving cars, burning fossil fuels, and manufacturing chemicals. Although air quality has improved since the 1960s despite substantial economic expansion and population growth, further improvements are needed. Air quality is a regional issue that does not respect jurisdictional boundaries. Every city throughout the region must accept a portion of the responsibility for addressing air quality problems.



*Clear skies  
over Brea.*

## Context

Brea lies within the South Coast Air Basin, a geographic area that extends from the Pacific Ocean north to the San Gabriel Mountains and east to the San Bernardino and San Jacinto Mountains. The Basin is a non-attainment area for federal and State air quality standards for ozone and State standards for particulate matter less than ten microns in diameter ( $PM_{10}$ ). The South Coast Air Quality Management District regulates air quality improvement programs within the Basin and works to improve regional air quality to achieve federal and State standards.

Motor vehicles represent the major source of regional emissions throughout the Basin and within Brea. Land use patterns which poorly disperse housing densities, employment centers, and mass transit facilities lead to excessive automobile usage. Most pollution control strategies have aimed at reducing vehicle usage and using cleaner-burning fuels. Other sources of air pollution include auto repair businesses, dry cleaners, and businesses that regularly use chemicals solvents. Common sources of PM<sub>10</sub> include road dust, construction activity, grading, and wood-burning stoves and fireplaces. Brea has continued to work with the South Coast Air Quality Management District and in accordance with the Air Quality Management Plan to improve the regional transportation system and regional air quality.

Energy conservation is strategy for improving air quality. Air pollutants are generated by the combustion of fossil fuels to produce electricity, and by the combustion of natural gas. Reducing energy usage decreases the amount of pollutants generated. Energy requirement can be diminished through innovative architectural design, building construction, structural orientation, and landscaping.

## Goals and Policies

Cooperation among all agencies in the Basin is necessary to achieve desired improvements to air quality. Brea can participate and contribute its share to those efforts by proper planning and participation in regional air quality management programs.

<b>Goal CR-13</b>	<b>Improve air quality.</b>
Policy CR-13.1	Implement City-wide traffic flow improvements.
Policy CR-13.2	Promote energy conservation and recycling by public and private sectors.
Policy CR-13.3	Integrate air quality planning with land use, economic development, and transportation planning.
Policy CR-13.4	Encourage the expansion and retention of local-serving retail businesses (e.g., restaurants, family medical offices, drug stores) to reduce the number and length of automobile trips to comparable services located in other jurisdictions.

- Policy CR-13.5 Encourage alternative modes of transportation, such as walking, biking, and public transportation to reduce emissions associated with automobile use.
- Policy CR-13.6 Cooperate with the South Coast Air Quality Management District and Southern California Association of Governments in their efforts to implement the regional Air Quality Management Plan.
- Policy CR-13.7 Work with other responsible federal, State, and County agencies to decrease air pollution emissions occurring within the air basin.
- Policy CR-13.8 Cooperate and participate in regional air quality management planning, programs, and enforcement measures.
- 



See Section XI in the Implementation Guide.

## HISTORIC RESOURCES

Identifying, recognizing, preserving, rehabilitating, maintaining, and treasuring the City's historic resources creates an awareness and appreciation for Brea's heritage. Our past tells us who we are, and buildings, structures, and places of the past reinforce for each new generation of Breans the importance of studying and valuing the community's cultural and physical foundations.

### Context

Brea contains a rich history, celebrated through events that took place at original sites and structures that exist today. Brea's history includes a time period when sheep ranching, oil extraction, and citrus production each represented the economic underpinning of the Southern California region. The remaining homes, commercial buildings, parks, and oil production facilities are just some of the historic resources that help define and enhance the community's character. Public awareness of these historic resources has led to the development of programs to aid in their preservation and restoration. To protect Brea's heritage, the community will continue to identify and safeguard resources worthy of such treatment, and ensure that new development enhances rather than alters reminders of early Brea.

### Early Brea History

The properties comprising Brea were once part of the large land holdings of the San Gabriel Mission that was established in 1771 by the Franciscan Padres. The mission land stretched along the coastal areas of California from San Diego to Monterey. Vast herds of Mexican cattle pastured on all the land in and surrounding Brea during the Mission and Rancho periods. In 1863, thousands of acres of Rancho lands, including Brea, were acquired by Abel Stearns, a Los Angeles businessman who later leased much of his land to sheep ranchers.

Beneath the hills in Brea lie what was once one of the world's richest deposits of oil. The Puente Hills and Brea Canyon supported substantial petroleum production during the late 1800s. By 1895, the Puente Hills Oil Company, started by William Lacy and W.R. Roland, had approximately 30 oil wells in the Puente Hills producing 700 barrels of oil a day. Union Oil Company was also lured by the riches of Brea and began purchasing land for oil production and leasing it out to other oil companies. The hillsides were covered with wooden oil derricks and oil production machinery.

The first real village in Brea was called Olinda, originally sited where Carbon Canyon Regional Park lies. Olinda, literally surrounded by hundreds of oil wells, was the workplace for many of the oil field workers. The village contained a church, barbershop, livery stable, boarding house, dance hall, pump house, Santa Fe Freight Depot, a school, and an oil storage tank.

In July 4, 1910, the Pacific Electric Railway connected Brea to La Habra, Whittier, and other Los Angeles communities. The official founding date for the town of Brea is January 19, 1911, with incorporation in 1917 with a population of 732. Many of the buildings of the original town were situated on Pomona Avenue (current day Brea Boulevard). As a booming oil town, the City's population began to grow rapidly. Development of new housing, businesses, and civic buildings began to shape current day Brea.



*The Craig building, completed in 1921, housed many important functions upstairs on the second floor. Brea's post office and drugstore occupied the first floor. The building was located on the east side of Pomona Avenue (Brea Boulevard) just north of Birch Street.*

Following the big oil boom, land in and around the City began transforming from sheep ranches to orange groves. The Bastanchury Ranch Company in La Habra, Fullerton, and Brea was said to have the largest citrus groves in the world, which included over 5,350 acres of orange groves during the 1920s. Union Oil Company also realized the potential of turning over underutilized properties to citrus production.

Post-World War II saw an increase in development. Many citrus groves were plowed under and the land subdivided for greater profits. Brea also saw major development in the manufacturing

and industrial businesses. With Union Oil Company still a major employer in the City, many oil-related companies moved to Brea. In 1972, the SR-57 freeway made agricultural lands and oil lands surrounding Brea accessible to even more growth. Five years later, the Brea Mall opened, providing economic growth for the City while supplying retail opportunities for Breans and the region.

## Preservation Efforts

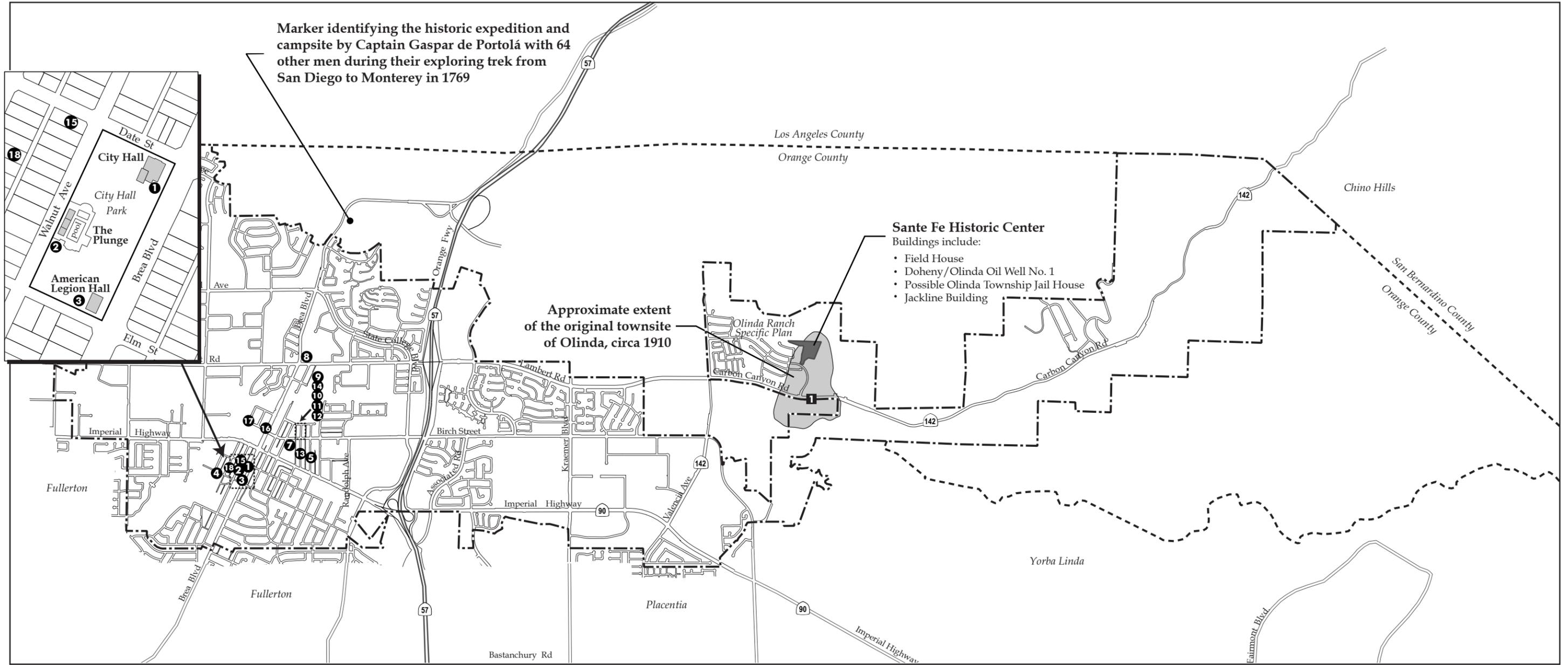
In 1989, the City Council adopted Resolution No. 89-125 establishing the Historical Committee. The Committee was subsequently eliminated in 1995 as it was only established as a temporary committee. The purpose of the Committee was to increase a level of awareness of Brea's historical resources by establishing goals and policies to guide preservation. A key tool adopted toward this end was the 1994 Historic Preservation Ordinance, designed to promote the historic, cultural, educational, economic, and general welfare through a number of specified objectives.

The Historic Preservation Ordinance establishes the Historic Resources Register and describes the procedures for designation and/or removal of individual historic resources. Identifying sites and structures with historical significance protects these resources from potential destruction and creates added awareness of historic resources that residents may not know exist. The Planning Commission is responsible for establishing and maintaining the Register, with input from the Historical Society. Structures and/or sites listed in the Register are eligible for a variety of preservation incentives provided by the City and various other State and local agencies. Figure CR-6 identifies key historic resources, and Table CR-2 identifies location, construction date, and a brief description for each.

Many homes within Brea's southwest residential neighborhoods date to the early twentieth century and display unique architectural qualities. A historic district represents one possible approach to preserving the integrity not just of individual structures but of blocks and neighborhoods.

### **Brea Historical Society**

Founded in 1971, the Brea Historical Society collects and preserves the City's historical materials and resources. This organization meets regularly and provides ongoing display, including historic oil equipment outside of the old City Hall. The collections include a large inventory of historic black-and-white photographs of Brea and residents.



Source: Brea Historic Resources Register, 1994; National Register of Historic Places, 2001; and State of California Register of Historic Resources, 2001; Olinda Heights Specific Plan, 2000. □

**Legend**

- City Boundary
- Sphere of Influence
- ▨ Potential Historic District(s)
- Approximate extent of the original townsite of Olinda, circa 1910

**City of Brea Historic Resources**

- 1 Old City Hall
- 2 Brea Plunge
- 3 Old American Legion Hall
- 4 Pioneer Hall
- 5 Practice House
- 7 Laurel Elementary School
- 8 Brea Junior High School
- 9 Dr. Walter W. Davis House
- 10 Edgar Jamison House
- 11 Roy Mitchel House
- 12 William Casner House
- 13 Charles C. Kinsler House
- 14 Howard Waggener House
- 15 Frank Woodard House

- 16 The "Brea Welcomes You" Sign
- 17 Good Old Brea Clock
- 18 Luther Cox House

**National Register of Historic Places**

- 1 Old City Hall
- 2 Brea Plunge
- 3 Old American Legion Hall

**State of California Register of Historic Resources**

- 1 Original Township of Olinda

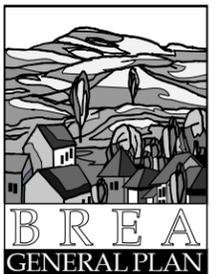
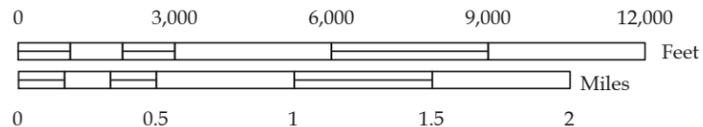
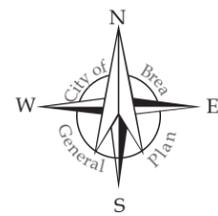


Figure CR-6  
Historic Resources



**Table CR-2  
City of Brea Historic Resources Register**

Historic Resource	Current Address	Construction Date	Description
1. Old City Hall	401 S. Brea Blvd.	1928	The structure was the location of Brea's first publicly owned municipal government facility for over 50 years.
2. Brea Plunge	440 S. Walnut Ave.	1929	Brea Plunge has undergone extensive renovation and remains in use today as the oldest continuously operating civic-owned freshwater swimming pool in Orange County.
3. Old American Legion Hall	495 S. Brea Blvd.	1930	During the Great Depression, the Legion could not make their mortgage payments, and the property was then taken over by the City. It became the Brea Police Station and Jail in the early 1970s. Brea Methodist Church started here, and it was used for an emergency ambulance service in the 1990s.
4. Pioneer Hall	330 W. Elm St.	1923	Pioneer Hall started as a Boy Scout clubhouse and later became a meeting hall for the Woman's Club, Junior Women's Club, and the Lion's Club. It still serves the community today as the site of many community activities.
5. Practice House	259 S. Laurel Ave.	1926	This building is unique in that it was possibly the only self-contained home economics lab in Orange County for the old Brea High School.
6. 1923 Seagrave Fire Truck	400 N. Kraemer Blvd. (Temporary)	1923	Today, the fire truck is believed to be one of the oldest fire trucks remaining in Orange County.
7. Laurel Elementary School	200 S. Flower Ave.	1922	Laurel School has been the focus of elementary education in Brea for over half a century and for many decades served as one of the community's first public schools.
8. Brea Junior High School	400 N. Brea Blvd.	1916	Brea Junior High School is a notable example of public architecture with its simplified Spanish Revival style.
9. Dr. Walter W. Davis House	101 S. Redwood <sup>1</sup>	1920	Dr. Davis constructed this structure as his home and office. He served the needs of the Brea community beginning sometime prior to 1914 and continuing into the late 1920s.
10. Edgar Jamison House	107 S. Redwood <sup>1</sup>	1911	The Craftsman construction symbolizes distinctive characteristics of this architectural style and homes of this period in history.
11. Roy H. Mitchell House	114 S. Redwood <sup>1</sup>	1914	The Craftsman construction symbolizes distinctive characteristics of this architectural style and homes of this period in history.
12. William Casner House	121 S. Redwood Blvd.	1915	The house was constructed for the Casner family circa 1915. William Casner was one of the early settlers who came to Brea in 1910. He was responsible for drilling the first oil well on the Amalgamaed Lease. As a member of the Brea Investment Company, he participated in the construction of many City's commercial structures.
13. Charles C. Kinsler House	258 South Redwood Ave.	1912	The original inhabitants of the house were Charles C. and Rena Kinsler when the house was first built in 1912. The house is a small one-story rectangular plan Craftsman Bungalow with a front-facing gable.

**Table CR-2  
City of Brea Historic Resources Register**

Historic Resource	Current Address	Construction Date	Description
14. Howard Waggener House	126 South Redwood Ave.	1929	The house is a Provincial Revival residence and is identified with an early time period associated with Brea's local history.
15. Frank Woodard House	401 S. Walnut Ave.	1920	The home is identified as being a part of the Union Oil Company subdivision, which was created to provide housing from company employees. The house is a California Bungalow displaying architectural elements that are not present on other similar homes in the surrounding neighborhood.
16. The "Brea Welcomes You Sign"	101 W. Birch St. <sup>1</sup>	1935	The sign was originally hung over Brea Blvd during a dedication ceremony in 1934. Hanging the sign during the Depression was very symbolic because it showed to everyone in the town and to visitors that despite problems, Brea was doing okay.
17. Good Old Brea Clock	Madrona Ave. and Birch St. <sup>1</sup>	1920	The clock was placed on Brea Blvd in 1975 to advertise a clock shop in the old Brea Hotel building. It supposedly was acquired from an old train station in the Inland Empire, but its origins are uncertain. For years it was known as "Charlie's Clock" because Charlie owned the clock shop. When the clock shop closed, the City purchased the clock since it had become a city landmark.
18. Luther Cox House	258 S. Redwood Ave. <sup>1</sup>	1922	The original inhabitants of the house were Charles C. and Rena Kinsler when the house was first built in 1912. The house is a small one-story rectangular plan Craftsman Bungalow with a front-facing gable.

Source: City of Brea Historic Resources Register

Notes: 1. Structure is not located at the original site where it was constructed.

The Brea Historical Society and City Council recognize residents for renovating and restoring historic structures to reflect their time period with the Brea Historic Preservation Award. This award distinguishes residents' hard work and pride of ownership, which allows for greater historic preservation awareness and acknowledgment.

### **National and State Historic Programs**

The National Historic Preservation Act (NHPA) of 1966 is a federal law that establishes the legal and administrative context to encourage preservation of historic resources. In California, the NHPA is administered by the State Historic Preservation office (SHPO). The NHPA establishes criteria for inclusion into the National Register of Historic Places (NRHP). The NRHP is an inventory of the United States' historic resources maintained by the National Park Service. The inventory includes buildings, structures, objects, sites, districts, and archeological resources. The listed properties are not necessarily significant nationally, rather most are significant primarily at the State or local level.

Brea City Hall Park is listed on the National Register of Historic Places. The park, City Hall, Plunge, and American Legion Building were grouped together under one identification number by the NRHP when it was listed on May 5, 1984.

Properties of historical importance in California are designated as significant resources in three State registration programs: State Historical Landmarks, Points of Historical Interest, and the California Register of Historical Resources. State Historical Landmarks are sites, buildings, features, or events that are of statewide significance and have anthropological, cultural, military, political, architectural, economic, scientific or technical, religious, experimental, or other value.

The original town site of Olinda (Figure CR-6) is listed as number 918 on the State Historical Landmarks, as well as the California Register of Historical Resources. Although many of the original buildings located in Olinda were removed because of flooding from Carbon Canyon headwaters, several existing structures are still present today. They are the Doheny/Olinda Oil Well No. 1, the Jackline Building, an oil production field office, and what could have been the Olinda Township Jail House. These structures were all saved from the original oil operations on the site in the 1890s.



*The original town site of Olinda, circa 1910, was made up of small homes and businesses on various oil company properties. Olinda Road (Carbon Canyon Road) can be seen winding east (center) into Carbon Canyon. Today's Carbon Canyon Regional Park takes up most of the flat land on the right side of the picture.*

## Goals and Policies

Brea's historic resources have value worthy of protection and special treatment and focus. The following goals and policies aim to keep Brea's past alive.

<b>Goal CR-14</b>	<b>Preserve historically significant landmarks and/or sites, individual structures, objects, and neighborhoods, and encourage appropriate rehabilitation.</b>
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Policy CR-14.1      Ensure design compatibility of new development within close proximity to designated historic structures and neighborhoods.

Policy CR-14.2      Create historic districts for areas with a concentration of historically and architecturally significant structures.

Policy CR-14.3      Review all development and rehabilitation proposals affecting historic structures, landmarks, and objects in terms of site design and building design.

Policy CR-14.4      Explore funding resources/alternatives for rehabilitation and restoration of historic structures.

Policy CR-14.5      Encourage the recycling and re-use of existing historic structures through incentive programs.

Policy CR-14.6      Discourage the demolition of structures eligible for listing in the National Register of Historic Places and/or the State Historic Landmarks, the County, and/or the local historic register.

Policy CR-14.7      Preserve and enhance the City's history and tradition through inclusion of historic sites and other resources in the City's park lands.

Policy CR-14.8      Accommodate heritage elements in recreation programs and events.

Policy CR-14.9      Create park themes based on local history and culture similar to the historic park at Olinda Ranch.

**Implementation Guide** 

See Section XII in the Implementation Guide.

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**Goal CR-15      Make all Breans aware of the importance of historic preservation.**

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Policy CR-15.1      Encourage resident participation in the maintenance of the quality of historic neighborhoods.

Policy CR-15.2      Promote Brea’s cultural and historic resources.

Policy CR-15.3      Encourage public and private participation in the preservation of Brea’s history.

Policy CR-15.4      Incorporate information on Brea’s history as part of the school curriculum to promote pride and appreciation of the City’s heritage.

Policy CR-15.5      Incorporate historic themes and architecture into development and public improvements along South Brea Boulevard to blend with Old City Hall, link Downtown Brea, and add interest and pedestrian orientation to the streetscape.

Policy CR-15.6      Promote Brea’s heritage through artwork, signs, preservation, and historical structures.

Policy CR-15.7      Promote community awareness and understanding of Brea’s history through the Brea Museum and Heritage Center.

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**Implementation Guide** 

See Section XII in the Implementation Guide.

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Chapter 5

# COMMUNITY SERVICES

THE CITY OF  
BRE A

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GENERAL PLAN



# Chapter 5

## COMMUNITY SERVICES

**HUMAN SERVICES • RECREATION PROGRAMS • EDUCATIONAL SERVICES • LIBRARY SERVICES • CULTURAL ARTS**



### VISION

Imagine Brea, where quality and affordable social, educational, cultural, and recreational programs, activities, and services contribute to each resident's well being and quality of life. Where Brea's schools, parks, community facilities, and arts and cultural institutions are highly regarded and treasured as valuable assets of Brea's community and economic environment. Breans value the many services the City and school district provide, and both the City and the district will continue to provide high quality community program activities and services to all residents, businesses, and visitors.

Community quality of life and spirit are best cultivated when government agencies, non-profit organizations, service clubs, volunteer organizations, the faith community, and private businesses create partnerships which benefit the entire community. These various stakeholders will be important resources that the City will work with to provide comprehensive programs, activities, and services for the entire community.

## HUMAN SERVICES

The City of Brea strives to be a caring, family-oriented community where each person, young and old, is respected and valued. As such, the City will continue to remain part of an active coalition of community organizations, agencies, and non-profit entities to lead a collaborative approach to meeting the human services needs of each segment of the community. While a number of organizations provide services to Brea residents, Brea's Human Services Division will continue to manage and assemble community partners to provide diverse supportive programs and activities that improve the health and well-being of families, children, and adults.



*Young Breans have fun creating arts and crafts projects at the Brea Community Center.*

## Community Context

### Family Resource Center

Established in 1996, the Brea Family Resource Center (FRC) offers a variety of services, programs, and workshops for residents that are free or at low cost. Located in the Brea Community Center, the FRC offers counseling, case management, outreach, emergency rental assistance, tutoring, parent education, classes, employment assistance, and health screenings.



## Service for Youth and Families

One of Brea's strongest values is its commitment to support youth and families within the community in a variety of ways. Brea's adult and senior population are the community's link to our past and present. Our teens and youth represent the future of Brea, and the City has committed focused resources toward developing social awareness, volunteerism, job training, athletic, and cultural arts programs for the City's younger residents. Important activities for youth and families include:

### **Helping Hands Scholarships (up to 17 years)**

Scholarships enable youth from low-income families to participate in City recreational and cultural activities. Children who might otherwise be left out participate in swim lessons, dance lessons, tutoring, youth theatre, art classes, sports, and many other healthy activities.

### **After School Program**

Free supervised activities are offered Monday through Friday at the Community Center for first through twelfth graders. Children have opportunities to interact with all age groups, develop social skills, and practice behaviors that will help them contribute positively at home, at school, and in the community.

### **Parenting Education**

The City, with its non-profit partners, addresses issues of safety, communication, and discipline to help parents raise healthy, well-adjusted, and happy children. A special educational series on the challenges of parenting is presented annually.

### **Family Resource Center Advisory Council**

This community board meets monthly to consider local needs, evaluate services, make recommendations, and encourage participation in the City's Family Resource Center. Members include leaders from the non-profit sector, faith community, businesses, health professionals, community leaders, dedicated volunteers, and parents.

### **Family Counseling Services**

Professional counselors and staff provide affordable counseling for families struggling with adjustment and communication problems, depression, divorce, and other related issues and concerns.

### **Information and Referral Services**

The Family Resource Center provides free, confidential information and referrals to families and individuals (English and Spanish speaking) needing special help.

## *Imagine Brea*

### **Financial Workshops and Counseling**

Financial counseling helps families and individuals learn how to budget, reduce debt, clear up credit records, and establish good credit. Free workshops designed to advise people to be financially independent are offered throughout the year. Free private and confidential financial counseling is also available.

### **Health and Safety**

Health and wellness screenings are offered throughout the year at the Brea Community Center, including vision, hearing, immunizations, breast cancer, dental clinic, and infant car seat inspections. Services are aimed at those Brea residents without medical insurance. In addition, Project Connections offers limited medical care and information targeting infants and toddlers ages 0 to 5 years.

### **Brea Senior Center**

The Brea Senior Center offers programs and services to meet the social, recreational, and service needs of Brea's older adult population. Comprehensive and meaningful services are provided, including a lunch program, food distribution, homemaker services, social services, case management, health and wellness, educational activities, and health counseling.



*The Brea Senior Center is located adjacent to Arovista Park. The Center is staffed by a group of highly competent professionals dedicated to providing quality senior programs for Brea's older adult population.*

### **Citizen Involvement**

Brea's senior service programs have the benefit of many committees, commissions, councils, and boards which provide the staff with insight, leadership, and guidance. These partners assist with project fundraising, marketing of programs and services, and community outreach. Citizen involvement groups include the Senior Advisory Board, which identifies senior issues and concerns throughout the community. The Senior Center Leadership Council, elected by their peers, provide input regarding the operations and programming for the Senior Center. The Senior Site Council is responsible for food distribution programs at the Senior Center.

### **Service Provider Partners**

While the City offers a variety of human service programs at the Brea Community Center and Senior Center, residents also look to the many non-profits and community groups that offer diverse activities and services. Long-established groups that often partner with the City include the Boys & Girls Club of Brea, Brea Boy Scouts, Brea Girl Scouts, CASA (Court Appointed Special Advocates), CAST (Child Abuse Services Team), Orange County Child Abuse Prevention Center, Sheepfold, Easter Seals Senior Day Care, Lions Club, Brea Lions Scout Center Foundation, Brea Ministerial Association, Habitat for Humanity of Orange County, Orangewood Children's Foundation, Orange County Rescue Mission, Kiwanis Club of Brea, Rotary Club of Brea and Soroptimist International Club of Brea. The refurbished historic City Hall at City Hall Park provides a venue for many programs offered by these groups.



*Brea Boy Scouts and Girl Scouts provide opportunity for boys and girls to learn responsibility and leadership.*

## Long-term Community Needs

In 2001, the General Plan outreach effort, input from the Parks, Recreation and Human Services Commission, and a focused Community Forum helped City leaders and service providers identify the long-range service needs of Breans. Key issues requiring attention are:

- Affordable housing, particularly rental units
- Affordable and quality childcare for children under five
- Young teens (12-14 years) and older teens (15-18 years) have different needs and prefer different activities. While the Teen Zone provides a variety of activities, it is primarily geared towards younger teens
- Provide additional activities for older teens in Brea
- Establish stronger working relationships between schools, the Community Center, and parents
- Affordable transportation is needed from Brea schools to the Community Center, especially from Brea Junior High to the Brea Community Center
- The transportation, housing, and service needs of seniors continues to remain an important issue. Frail seniors, seniors that require transportation assistance and the issue of helping seniors maintain independence are of primary concern
- Appropriate after-school care
- Providing services for parents, families and children that do not use English as their primary language is a growing need
- Additional safe areas for children to walk and play

## Goals and Policies

Human service programs assist in improving health and self-esteem, and provide alternatives to self-destructive behavior, incarceration, the prevention of homelessness. In addition, human services programs enhance community spirit, and provide opportunities for learning and living a balanced and productive

life. The City and its private sector and non-profit partners will continue to provide the tools and resources that focus on individual, family, and community improvement that respond to service needs identified by City residents.

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**Goal CS-1: Meet the varied human service needs of Brea’s diverse population.**

---

Policy CS-1.1 Continue to implement the goals, actions, and recommendations identified in the Human Services Master Plan.

Policy CS-1.2 Provide, facilitate (solely or in partnership), and/or support human service programs and actions that are responsive to the needs of all ages, genders, cultural, and socioeconomic groups.

Policy CS-1.3 Improve the provision of human service programs and facilities with the greatest needs.

Policy CS-1.4 Focus efforts on program areas with identified deficiencies:

- Senior programs
- Teen programs
- Cultural integration programs

Policy CS-1.5 Identify innovative funding and development opportunities to support and sustain a responsive human services network.

Policy CS-1.6 Encourage citizen input and utilize demographic data, partnerships, volunteers, and existing resources to meet human service needs.

Policy CS-1.7 Integrate different groups. For example, provide programs that facilitate intergenerational activities between seniors and youth.

## RECREATION PROGRAMS

Brea's parks, recreational areas, trails, and playgrounds offer residents many opportunities to involve themselves and their families and friends in active and passive activities. The City, non-profit sports user groups, and private organizations sponsor recreation programs in Brea for residents of all ages. Through these programs, Breans can remain active and healthy, and interact with people with similar interests. (Goals, polices, and programs relating to City parks and open space resources are discussed in the Community Resources chapter.)



*The Brea Community Center offers a wide variety of recreation and leisure opportunities all under one roof, including: Fitness Complex, Family Resource Center, Employment Services, The Zone, KidWatch baby-sitting services, and facility rentals.*

## Community Context

The Brea Community Services Department coordinates an array of recreational activities, classes, and programs. These include all types of leisure and self-improvement pursuits for all age groups, with a focus on meeting youth, adult, and senior citizen needs. Programs include youth and teen after-school programs, adult and teen sports leagues, and fitness classes for all ages. The Department publishes a recreation schedule each quarter that provides residents with a guide to programs and activities at the Brea Community Center as well as other sites throughout the community. Program offerings are evaluated and adjusted as needed in response to identified needs and interests of Brea residents.

## Tots and Preschoolers

Specialized classes for preschoolers and tots include music, gymnastics, dance, and crafts. Brea's Tiny Tots program is an excellent way for parents to help prepare their children for kindergarten. Other programs offered by the Brea Community Center for tots and pre-schoolers include Parents' Night Out, Kid Care Solutions, and Baby Sitting.

## Teen Programs

Adolescents often have specialized needs because of their rapid emotional and physical changes. As well as providing a number of tutoring, sports, and after-school activities for teens, the Brea Community Center includes "The Zone," a teen activity center that provides a safe, fun place for teens to socialize. Sports tournaments and social events such as dances, pool bashes, sneak previews, and excursions allow teens to participate in organized group activities.

The Student Advisory Board and Teen Action Committee plan various programs and activities geared for Brea's youth and teens, as well as discuss important issues facing the community.



*A variety of instructional sporting activities are available to children at all ages.*

## **Adult Sports and Fitness**

Sports leagues and fitness and recreation classes offered at the Center include basketball, volleyball, yoga, fencing, Body Pump, Cycle, Cardiokick, and Senior Fitness. Adults as well as older teens can use the Center's fitness/exercise equipment to help maintain their health and well being. The City also offers a variety of programs, services, and activities outside the Center such as aquatics, summer day camp, baseball, golf and tennis.

In allocating City resources annually, the City Council has recognized the benefits of supporting recreation programs, activities, and services that offer youth and teens healthy outlets for their energy and interests, and for involving adults in activities that promote good health. Such programs will continue to be of importance, and established practices of periodically assessing program demands will allow Brea to respond to residents' needs.

## **Adult Special Interest Programs**

In addition to traditional health and wellness, fitness, and recreational programs, services, and activities, the Community Services Department offers a variety of special interest classes which meet the wide interests and needs of Brea's residents. Craft classes such as candle making, calligraphy, and holiday-oriented craft making are always popular and well attended. In addition to craft classes, the Community Services Department offers such special interest classes as speed reading, photography, event planning, creative cuisine, and dog training. Finally, the Special Interest Program includes special excursions such as trips to the Getty Museum and excursions to Laughlin, Las Vegas, and the Rose Parade.

## **Special Events**

To build community and help retain the "small town" atmosphere so important to Brea, the Community Services Department conducts numerous special events for the entire community. Family Night, the 4<sup>th</sup> of July Country Fair, the Annual Tree Lighting Ceremony, Snow Play Days, the Nutcracker and Spring Craft Boutiques, and Adult Dances are samples of the special events provided by the Recreation Services Division.

## **Volunteer Program**

Brea's Community Volunteer Program promotes collaboration, partnership, and volunteerism. The volunteer program works with individuals, service groups, and Brea businesses to continue our tradition of supporting the community at large by working together. It is this positive spirit that makes Brea a special place to live, work, and play. Brea's Volunteer Program offers youths, ages 13 to 17 an opportunity to give their time and energy to the community while learning work and leadership skills during their summer break.



*The Volunteering Program offers teenagers a chance to learn and develop skills during their summer break.*

### **Parks, Recreation, and Human Services Commission**

This five-member advisory body appointed by the City Council oversees the coordination of all recreation, leisure, and human services programs, activities, and services of the City. The primary purpose of the Commission is to gather information, weigh public opinion, examine issues in depth, and make recommendations to the City Council pertinent to the Parks, Recreation and Human Services Programs. The duties of the Parks, Recreation, and Human Services Commission include:

- Identify and recommend solutions to fulfill recreational and human services needs and interests of the community
- Recommend general policies and guidelines with respect to Community Services programs and facilities
- Review and recommend the acquisitions, development, beatification, and maintenance of recreation facility, parks, and trails in the City
- Advise in the preparation of the long-range Parks, Recreation and Human Services capital improvement program budget

## Goals and Policies

The quality, amount, and availability of recreational services, programs, and activities add to the health and well-being of a community. As the population of Brea grows and diversifies, Brea must ensure that a variety of recreational programs and facilities are made available to meet the changing need and interests of residents.

**Goal CS-2: Provide a diverse range of recreation services, programs, and activities that are responsive to the present and future needs of all Brea residents.**

Policy CS-2.1 Continue to implement the goals, actions, and recommendations identified in the Human Services and Master Plan.

Policy CS-2.2 Provide recreation services, programs, activities, and opportunities that are responsive to the needs and interests of the community.

Policy CS-2.3 Provide specialized recreation services, programs, and activities that meet the unique and diverse needs of Brea's youth and teens.

Policy CS-2.4 Encourage the development of health and wellness, fitness and/or other recreation programs and activities within private and public work environments.

Policy CS-2.5 Partner with other public agencies, non-profit, and public and private organizations to develop health and wellness, and recreation, programs, services and actions to meet the needs of the community.

Policy CS-2.6 Continue the role of the Parks, Recreation and Human Services Commission as the advisory body for recreation and human services programs, services and activities within Brea.

## EDUCATION SERVICES

Education is the foundation of a community. Education is at the heart of economic prosperity and security; it channels creativity and success and enriches lives. A community that values and promotes education and learning benefits its residents and the business sector. Public and private schools, extension centers, and continued educational resources are most successful when they have the support of students, families, City officials, and the business community. These partnerships are essential to providing quality education that all deserve.



*Brea Students at a school rally.  
The Brea Olinda Unified School  
District provides outstanding  
educational programs and services  
for all attending students.*

## Community Context

The City and the Brea Olinda Unified School District long ago recognized the benefits of partnering to ensure that Brea's youth receive outstanding educational services. The District's boundaries largely correspond to the corporate City limits; thus, Breans feel as though the City and School District represent one government entity, even though the District, under California law, is a separate, independent agency. The City and School District cooperate on many fronts, from sharing athletic fields and meeting facilities, to organizing educational programs.

The Brea Olinda Unified School District provides public school educational services to children in grades kindergarten through grade twelve. Test scores for Brea students consistently rank among the highest in Orange County. A new state-of-the-art high school campus opened in 1990, and Brea Olinda High School is

one of the few in California to be part of the Knowledge Network computer link-up.

Brea's quality public schools attract young families. New home construction and new families with children who are purchasing existing residences have resulted in increased school enrollment and strains on all District schools. The District has identified a need for a new elementary school on the east side of Brea and new junior high and high school classrooms and support facilities. City staff and policy makers continue to cooperate with District officials to help address the District's long-term needs, including developing a new joint-use sports park and school on Birch Street east of the 57 freeway.

In addition to fine public schools, Brea is home to many private schools such as St. Angela's School, Christ Lutheran School, and Brea Olinda Friends Church School. Breans have identified a need for additional post-secondary public and private educational facilities in the community to build on the strong educational ethic and create broad opportunities for all.



*Brea Olinda High School is located within the hills just east of the 57 freeway and north of Lambert Road.*

## Goals and Policies

Every school in Brea is committed to providing a comprehensive education for every student, fostering high academic achievement, positive self-worth, and responsible citizenship. Brea's schools are a source of considerable community pride, as the City is well known for its quality schools and teachers, extensive parent involvement, and exceptional students. As the community grows

and changes, providing quality education to every student will continue to remain a high priority for the Brea Olinda Unified School District. In response, the City will continue to integrate school needs in the planning and development of the community.

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**Goal CS-3.1: Continue to partner with parents, families, and the community to educate all students, encourage students to achieve their fullest potential, and to prepare students to be responsible, contributing citizens.**

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Policy CS-3.1: Continue to build and support strong relationships and partnerships between schools and the community.

Policy CS-3.2: Promote and support the quality public K-12 education system by working closely with the Brea Olinda Unified School District in determining and meeting community needs for public education and related activities.

Policy CS-3.3: Work with the Brea Olinda Unified School District to investigate potential locations and funding sources for new schools.

Policy CS-3.4: Strive to provide equal access to educational and informational resources.

Policy CS-3.5: Actively work with private, non-profit, and public community services organizations to coordinate educational and community services including child care, English translation, after-school programs, and recreational activities.

Policy CS-3.6: Support efforts for the establishment of a community college, satellite university campus, vocational school, and/or other post-secondary educational facility in the community.

## LIBRARY SERVICES

Libraries provide communities with diverse resources and services. Libraries preserve culture and history and transmit them from one generation to the next. They also provide social settings for community activities, support of formal education, and provide opportunities for individuals that can last a lifetime.

### Community Context

The Brea Branch Library is part of the Orange County Public Library community library network. The system includes 28 branches that provide many services to residents throughout the County. The Brea Branch Library, located at the Brea Civic and Cultural Center, shares a common lobby with the Brea Chamber of Commerce. While Orange County operates the library, its location within the Civic and Cultural Center illustrates the importance the City places on ensuring convenient access to a key community resource.



*The Brea Branch Library, located at the Brea Civic & Cultural Center, has several special collections of books and other materials, including: Business Reference, Careers and Jobs, Help Wanted U.S.A. microfiche, Spanish language books, and large print books.*

## Goals and Policies

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**Goal CS-4: Provide library resources that meet the educational, cultural, civic, business, and life-long learning needs of all residents. Retain a local library is that community-oriented, provides knowledgeable, service-oriented staff, and offers access to information, books, and other materials in a variety of formats that use contemporary technology.**

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Policy CS-4.1 Encourage the County to develop programs and services for adults, children, and new readers that meet future needs.

Policy CS-4.2 Work with library staff to assess, select, organize, and maintain collections of materials and information sources of value to and desired by the community.

Policy CS-4.3 Work with library staff to maintain technological services that meet the needs of residents, as well as reader advisory, reference and referral services responsive to user needs.

Policy CS-4.4 Explore funding opportunities for the City to expand the existing County branch library and/or operate a local, independent library.

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**Implementation  
Guide** 

See Section XVI of the Implementation Guide for action programs.

## CULTURAL ARTS

The cultural arts humanize us. Through visual art, music, dance, and other performance art we as humans express our emotions, our convictions, and reach out to share these feelings with others. Simply put, the arts bring us together as a community.

Brea has long recognized the importance and value that cultural arts bring to the community. Through its various cultural art programs, facilities, and activities, as well as being home to one of the nation's largest outdoor public art sculpture collections, Brea has earned the reputation as one of Orange County's foremost arts and cultural centers. This ongoing support of the arts has been achieved through City programs, commissions, volunteers, and private and public sector support. Brea's diversity of cultural arts programs, services, and activities has become a source for community education and enrichment, and have added to the special qualities that distinguish Brea from other communities.



***Quintessence***, by Joan Carl, is a part of the Brea Arts in Public Places Program. This piece is located on the west side of Brea Boulevard north of Central Avenue.

Without intervention, preservation, and promotion, art and cultural resources can easily be lost through demolition or disregard, or can be distorted through economically driven or unintended land use decisions and policies. The goals and policies set forth below reemphasize and underscore the City's commitment to the arts in Brea.

## Community Context

### Curtis Theatre

Constructed in 1981, the 199-seat Curtis Theatre is located on the plaza level of the Brea Civic & Cultural Center. The Curtis Theatre is an active and versatile facility designed for use as a professional quality theatre hosting full-stage productions, touring companies, musical concerts, guest artists, special events, and meetings with multi-media presentations. The facility, technical equipment, and sound, stage and lighting staff offer capabilities for all types of performances. Major theatre events include the family-oriented Kids Culture Club Series that present live musical, theatrical and guest artist performances. In addition, the Youth Theatre Program offers children the opportunity to participate on Brea's stage as part of a real theatre production. Finally, dramas, musicals, and comedies are presented throughout the year by local, national, and international theatre professionals and musicians.



*The intimacy of the Curtis Theatre brings the action up close and personal. With only 199 seats, the farthest seat is still less than 35 feet from the main stage.*

## *Imagine Brea*

### **Brea Gallery**

Also located at the Brea Civic & Cultural Center is the Brea Gallery, a City-owned art exhibition space that hosts a variety of exhibitions throughout the year. In addition to the exhibition, the Gallery offers numerous art-related programs, including art competitions, workshops, lectures, demonstrations, and a Children's Art Space specially dedicated to children's art displays. The Gallery Gift Shop sells an assortment of fun and exciting gifts, as well as displaying and selling the works of local artists.

### **Art in Public Places Program**

Brea's recognition that public art is an important visual and community resource was formalized in 1975 when the City Council passed a resolution to implement the *Art in Public Places Program*. The primary goal of this innovative program is to provide permanent outdoor art work throughout the City accessible to the general public. Art works installed and funded by private developers create unique public spaces and help distinguish development projects.



***The Family***, by artist Gregory Hawthorn, is located on the northeast corner of Brea Boulevard and Ash Street. This sculpture of a mother, father, and child holding hands shows support, protection, and love.

Brea's nationally recognized Art in Public Places program includes more than 135 outdoor sculptures and is one of the largest public art collections in the country. The collection encompasses a wide range of mediums, styles, and themes by regional, national, and international artists.

**Art Shelters**

The Art Shelter Program was developed to provide local and regional artists with a venue to exhibit their work. The artwork is displayed within the City's various bus stop shelter locations. This unique program is funded by bus shelter advertising revenues.

**Brea Fest**

This annual event usually held in late August, Brea Fest is a community-wide celebration of the arts. This event includes live music, dance productions, theatre performances craft demonstrations, and samples from over 30 local restaurants. The festival includes "Kidsville," live music, and story telling for children. The event raises funds to support Brea's various cultural arts programs.

**Concerts in the Park**

For over twenty years, the City and its partners have hosted free weekly summer concerts at City Hall Park.

**Arty Party**

Students in Brea's public schools participate in educational field trips to the Brea Civic & Cultural Center where they are treated to a performance in the Curtis Theatre, a backstage tour, a theatrical make-up demonstration, a Brea Gallery exhibit tour, and free art books. This educational program is funded by community partnerships.

**Cultural Arts Commission**

A five-member City Council-appointed body, the Cultural Arts Commission guides and promotes cultural arts activities in the community. These activities include:

- To promote cultural arts activities within community
- To advise in the coordination of cultural arts activities in the City
- To provide insight into the establishment and maintenance of cultural art programs
- To ensure the efficient operation of all cultural arts facilities within the City

The Cultural Arts Commission, since its inception in 1979, has successfully guided many diverse cultural arts programs despite any long-range, overarching plan to address Brea's commitment to and philosophy regarding the cultural arts. A need has been identified

**Brea Olinda High  
School Performing  
Arts Center**

for a cultural arts master plan to provide a comprehensive, coordinated vision for all cultural arts.

The 400+ seat theater located on the campus of Brea Olinda High School represents a community resource for both school-related and non-school performances and meetings.

## Goals and Policies

Brea's many arts and cultural resources, heritage buildings and structures, outdoor sculpture collections, performing and visual arts centers, festivals, and arts and education programs, as well as individual and corporate assets, comprise an important component of community life. Looking toward the future, these cultural and arts programs can be strengthened and expanded to meet changing community values and vision.

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**Goal CS-5: Promote and support the cultural arts in varied aspects of community life.**

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- Policy CS-5.1 Continue to support and implement the Art in Public Places program.
- Policy CS-5.2 Acknowledge and support Brea's heritage through its cultural arts programs.
- Policy CS-5.3 Encourage strong arts education in Brea.
- Policy CS-5.4 Honor outstanding achievements in the arts.
- Policy CS-5.5 Explore opportunities for creating live/work housing for artists.
- Policy CS-5.6 Encourage the incorporation of art into architectural design.
- Policy CS-5.7 Pursue partnerships with the Brea Olinda Unified School District, local public and private colleges and universities, and private organizations to support cultural arts programs and education.
- Policy CS-5.8 Promote the involvement of local service clubs and similar organizations in arts events citywide.

Policy CS-5.9 Provide cultural arts programs that meet the needs and interests of all Brea residents.

Policy CS-5.10 Raise community awareness and understanding of the Arts in Public Places program.

Policy CS-5.11 Continue the role of the Cultural Arts Commission as the advisory body for the arts in Brea.

Policy CS-5.12 Promote citizen participation and conduct outreach programs to ensure City cultural arts programs meet changing needs in the community.

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**Goal CS-6 Ensure that adequate physical facilities are available to support cultural arts programs.**

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Policy CS-6.1 Maintain the Curtis Theatre, the Brea Gallery, and the Community Center as key cultural arts venues.

Policy CS-6.2 Maintain joint-use agreements with the Brea Olinda Unified School District for the sharing of performance and exhibit spaces.

Policy CS-6.3 Explore opportunities for providing new facilities to accommodate current or emerging cultural arts.

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**Goal CS-7 Draw from broad and creative funding sources to achieve the City's cultural arts vision and goals.**

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Policy CS-7.1 Pursue public and private grants to fund arts programs, activities, and facilities.

Policy CS-7.2 Explore private/public funding collaborations to expand art festivals, live performances, and public art programs.

Policy CS-7.3 Promote and support volunteer opportunities for public involvement in City-sponsored arts events.

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**Goal CS-8 Create national recognition of Brea as an arts community.**

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Policy CS-8.1 Provide funding to market and promote Brea's cultural arts programs at the local level and beyond.

Policy CS-8.2 Involve Brea in County, State, and national arts-related events.

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**Implementation  
Guide**



See Section XVII of the Implementation Guide for action programs.

Chapter 6  
**PUBLIC SAFETY**

THE CITY OF  
BRE A  
GENERAL PLAN



# Chapter 6

## PUBLIC SAFETY

EMERGENCY SERVICES • HAZARDS MANAGEMENT • WILDLAND FIRES •  
FLOOD CONCERNS • GEOLOGIC AND SEISMIC CONSIDERATIONS • NOISE



### VISION

Imagine Brea, where public safety is a high priority and residents feel protected from hazards. Imagine Brea, where proactive planning addresses how to mitigate hazards, such as crime, flood hazards, earthquake effects, urban and wildland fires, and noise exposure. Imagine rapid and effective emergency response. Imagine quiet residential neighborhoods designed to minimize traffic and other noise sources. Imagine urban and wildland fires quickly extinguished by a fire department that has superior personnel, equipment, and facilities to handle any fire.

One of the City's highest priorities is to ensure a safe and secure community. This Public Safety Chapter sets forth goals and policies to protect and safeguard Brea residents from wildland and urban fires, crime, hazardous materials incidents, flooding, earthquakes, and exposure to excessive noise levels. Brea will strive to be fully prepared for natural disasters and to minimize public exposure to hazards. Brea will maintain and enhance its emergency services to allow police, fire, and paramedics to respond swiftly to emergency situations. The City will maintain public safety facilities to improve public safety and protection.

## EMERGENCY SERVICES AND SAFETY

One of the most fundamental qualities valued in Brea is the safety and well-being of its neighborhoods and businesses. Brea is highly regarded for its provision of quality, effective, community-oriented police and fire services, personnel, and volunteers. Brea will continue to sustain and improve its commitment to safety through a comprehensive approach to police and fire services, including public outreach and education, community awareness, and partnerships with public agencies and private businesses. Foresight and planning regarding land use decisions represent proactive approaches to enhancing safety in the community. Keeping neighborhoods buffered from both urban and wildland fire hazards reduces incidents requiring response and minimizes damage to property when fires do occur. Incorporating public safety considerations into the design of new buildings — allowing the community’s “eyes on the street” to be the first defense against crime — creates safer environments. The Public Safety goals and policies reflect Brea’s emphasis on preventing public safety problems rather than reacting to them.



*Along with the regular duties of traffic and law enforcement, the Brea Police Department also has activities such as Neighborhood Watch, D.A.R.E. (Drug Abuse Resistance Program), Here's Looking at You Kid (underage drinking program), Personal Safety Programs, Home Security Inspections, Vacation House Checks and station tours.*

## Context

The Brea Police and Fire departments provide invaluable life-saving services and are held in high regard for the high quality and commitment of their personnel.

### Police Services

The mission of the Brea Police Department is to preserve and enhance the quality of life in the community by balancing the need for traditional response to calls for services with effective problem solving. As such, the Brea Police Department is a highly valued positive force in the community, providing effective safety and emergency response services, but also engaging in community programs and educational activities. Volunteerism and community involvement continue to be the cornerstone of the Brea Police Department. The Police Department includes a number of innovative, community-oriented services, including the Bicycle Safety Program, the Explorers Program, the Citizen's Academy, the Community Action Patrol, and several traffic safety programs.

The Police Department provides police services to more than 94,000 residents and a daytime population of more than 150,000 in the cities of Brea and Yorba Linda. From the central station at the Brea Civic & Cultural Center, an administrative office in Yorba Linda, and a substation in Downtown, the Department offers quick response to all community law enforcement needs.

#### **Police Explorers Program**

Initiated in 1969, the Brea Police Explorers Program, an extension of the Boy Scouts of America, provides a unique opportunity for young people to receive training in fields such as K-9, Narcotics, Hostage Negotiation, Weaponless Defense, and Physical Fitness. This training can prepare participants for a career in law enforcement and teach them valuable life skills. Explorers perform a variety of community services including traffic control, crowd control, fingerprinting, and assisting in emergency situations.

#### **Citizen's Academy**

The Citizen's Academy consists of evening workshops designed to give participants a greater understanding of police work. Taught by members of the Police Department, this informal academy covers topics such as evidence gathering, patrol deployment, special response teams (SWAT), firearms and special weapons, K-9 units, court procedures, drug and alcohol enforcement, and dispatch. Many graduates of this unique program have embarked on a career in law enforcement or continue in a volunteer capacity for the City.

**Community Action Patrol (C.A.P.)**

This program enables Police personnel to form and maintain lasting and effective partnerships with Brea and Yorba Linda residents and businesses. C.A.P. is a group of residents and business owners from Brea and Yorba Linda who volunteer time each month to the Police Department. C.A.P. participants assist with traffic and crowd control at civic functions; proactively patrol our community schools, businesses, and residential areas in specially marked and equipped cars; file records; answer telephones in the detective bureau; and on occasion, even take part in surveillance operations to help deter crime.

**Bicycle Patrol Program**

Since 1992, the Police Bicycle Patrol has allowed Police personnel to have higher visibility, accessibility, and mobility in Downtown Brea, residential neighborhoods, and other business areas. The program provides officers with an ideal means to be actively involved in neighborhoods, recognizing neighborhood problems visually and through constant interactive with citizens or business owners. Bicycle patrols are especially useful during large, public events like the Fourth of July Celebration and Fiesta Day in Yorba Linda, and Concerts in the Park and Family Film Festival in Brea.



*Brea Bicycle Patrol officer on watch in Downtown Brea. The Bike Team consists of thirty members of the Department, including one full-time bike officer assigned to the downtown area. The downtown bike officer position is funded through a State grant.*

## Fire Services

The primary mission of the Brea Fire Services Department is the delivery of life safety services. The Department provides 24-hour emergency response to a wide spectrum of community situations, including fires, explosions, hazardous material incidents, medical emergencies, accidents, and general public assistance requests. The Fire Prevention and Emergency Preparedness Programs provide fire inspections, hazardous process permitting, fire code enforcement, public education, and business emergency planning. The Department also leads community outreach and volunteer programs, including the Fire Explorer Program, Apprentice Firefighters, and C.A.R.E.S. program.



*The Brea Fire Department has an internal equipment committee that identifies equipment needs, researches new equipment and tools, and makes recommendations for purchase. This applies to everything from small equipment to the purchase of new fire engines.*

Each of the Department's stations is assigned to a Fire Management Zone, a geographically based area of responsibility that represents the station's primary assigned emergency response district. As new development occurs within the northern and eastern areas of Brea, the Department will need to continually assess the best approaches to serving new residents, including consideration of a new station or stations to maintain minimum response times.

### **Fire Explorer Program**

Affiliated with the Boy Scouts of America, this program allows young men and women ages 14 to 19 to experience the career of a firefighter. Fire Explorers work under the supervision of career firefighters to learn emergency response services and prevention programs.

**Community Assistance and Response to Emergency Situations Program (CARES)**

CARES is a citizen-based program that teaches residents how to respond during all types of emergencies. Classes include Emergency Preparedness and Fire Extinguishment, Disaster Medical Aid, Light Search and Rescue, Disaster Psychology, and CPR. Each course is taught by Brea Fire Department personnel with a focus on application to the home and workplace.

**Fire Flow Analysis**

To combat fire, the Fire Department must have sufficient water pressure and storage throughout the City to quickly extinguish urban and wildland fires wherever they arise. Insufficient water pressure can jeopardize fire-fighting capabilities and allow fire to spread uncontrollably, causing excessive damage to structures. The fire-flow analysis assesses the ability of the water system to meet fire flows throughout the planning area while also providing maximum day demands. The water system must be capable of providing at least 20 pounds per square inch residual pressure at one location in the entire water system.

The City has conducted many computer simulations of fire-flow analysis and has recorded deficiencies only in Olinda Village. For most areas in Olinda Village, the pressures are adequate for all demand periods; however, higher elevation areas do not receive adequate pressure during higher demand periods and fire flows. The City has placed special requirements on landowners at the higher elevations to mitigate pressure problems. Prior to approving new development in remote areas in Carbon Canyon and Olinda Village, the City will study the requirement of any new booster pumps, storage tanks, sprinklers, and additional water facilities for new development. A new reservoir in the Carbon Canyon will help improve the low pressure areas.

The Wildland Fires Section within this chapter discusses the protection and services against fires within the Sphere of Influence and hillside areas. The Infrastructure Section in the Community Development Chapter discusses water distribution system and water storage capacity improvements.

## Emergency Preparedness Program

Brea is well prepared to respond to all types of emergencies. The City’s Emergency Preparedness Program addresses appropriate responses to major earthquakes, hazardous materials accidents, wildfire, and dam failure. The program describes how emergency response will be coordinated and how evacuation of residents will proceed. Program objectives include injury reduction and avoiding loss of life and property damage through effective management of emergency forces.

### Goals and Policies

Brea aims to build on its proven safety record by fostering safer neighborhoods through demand-responsive police and fire services, active enforcement, community involvement, and continued proactive public safety planning.

<b>Goal PFS-1</b>	<b>Ensure that City public safety services are of the highest quality.</b>
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- Policy PS-1.1 Work with the Police Department to determine and meet community needs for law enforcement services.
- Policy PS-1.2 Provide up-to-date technology to the Brea Police and Fire Department.
- Policy PS-1.3 Continue to maintain and develop a community-based police strategy compatible with the needs and size of the community.
- Policy PS-1.4 Work with the Fire Department to determine and meet community needs for fire protection and related emergency services. Ensure that sufficient stations, personnel, and equipment are provided to meet growth needs in the City.
- Policy PS-1.5 Maintain a maximum 4 to 6 minute emergency response time for fire safety services. Maintain a 3 to 5 minute response time from emergency police response services. Require that all new development be able to meet established standards for such response.
- Policy PS-1.6 Impose special conditions as needed on development projects to ensure that adequate fire protection measures are in place and maintained.

Policy PS-1.7 Incorporate the tenets of Community Oriented Policing into the design of crime prevention and enforcement programs.

Policy PS-1.8 Use technology to improve crime prevention efforts.

Policy PS-1.9 Maintain and update, as appropriate, the City's emergency preparedness programs, plans, and procedures to ensure the health and safety of the community in the event of a major disaster.

Policy PS-1.10 Initiate annual public information programs that explain the City's Emergency Preparedness Program and other emergency preparedness programs.

Policy PS-1.11 Support volunteer programs, after school activities such as DARE, police activities within high schools, and Neighborhood Watch programs.

Policy PS-1.12 Ensure that local authorities have the necessary tools and education to cite and correct problems before they becomes safety issues.

**Implementation  
Guide** 

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See Section XVIII in the Implementation Guide.

**Crime Prevention  
through  
Environmental  
Design (CPTED)**

Secure streets and public places reduce the potential for criminal activity. One method of addressing crime prevention is defensible space planning. Defensible space planning uses design techniques, building orientation, and features of the built environment to deter criminal activity and positively influence human behavior and the perception of safety. Defensible space planning is a key element in a local comprehensive crime prevention and safety plan. CPTED techniques look to incorporate four key considerations into site planning and architectural design: territoriality, natural surveillance, activity support, and access control.<sup>1</sup>

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<sup>1</sup>Basics of Crime Prevention Through Environmental Design. [www.ncpc.org](http://www.ncpc.org)

### **Territoriality**

People tend to protect territory that they feel is their own and to respect the territory of others. Low decorative fences, artistic pavement treatments, well designed signs, good property maintenance, quality landscaping, and public art express pride in ownership and identify personal space.

### **Natural Surveillance**

Placing physical features, activity centers, and people in ways that maximize the ability of passers-by to see what's going on discourages crime. Barriers such as bushes, sheds, or shadows make it difficult to observe activity. Landscaping and lighting that promote natural surveillance from inside a home or building and from the outside by neighbors are effective means of passive crime prevention.



*This supermarket uses landscaping, artistic treatments, walkways and lighting to reinforce territoriality and provide natural surveillance.*

### **Activity Support**

Encouraging legitimate activity in public spaces assists in discouraging crime. Improvements like a basketball court in a public park and community activities such as a clean-up day, block party, or civic or cultural event bring people out, get them involved, and help discourage vagrancy and potential illegal acts.

### **Access Control**

Entrances and exits, pedestrian paths, and driveways can be located to direct people to safe, lighted, visible areas. Strategies include adding front porches or a front office to an apartment building, locating drive-up or walk-up business services in view of the street, and planting low-level landscaping along walkways.

---

**Goal PS-2 Improve community safety and reduce opportunities for criminal activity through appropriate physical design.**

---

Policy PS-2.1 Develop defensible space guidelines to be used in the review of development proposals.

Policy PS-2.2 Maximize natural surveillance through physical design features, including well-lighted driveways, walkways, and exteriors; visible entryways from surrounding structures and businesses; well-defined walkways and gates; and landscaping that does not obscure visibility.

Policy PS-2.3 Ensure that community areas and amenities such as transit stops, sidewalks, plazas, and parks are appropriately lighted, free of hidden spaces, and patrolled.

Policy PS-2.4 Practice active surveillance measures in certain areas such as parking structures.

**Implementation Guide** 

See Section XIX in the Implementation Guide.

**Pedestrian Safety**

Hand-in-hand with the goal of creating a walkable city is the goal of creating safe streets and travelways. People need to feel safe in their environment if they are to be encouraged to use trails, pedestrian paths, and sidewalks to move about the community.

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**Goal PS-3 Provide safe pedestrian environments citywide.**

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Policy PS-3.1 Ensure that pedestrian safety is enhanced and maintained through the inclusion of well-designed streets, sidewalks, crosswalks, traffic control devices, and school routes throughout Brea.

Policy PS-3.2 Require all developments to provide adequate safety lighting in pedestrian areas and parking lots.

Policy PS-3.3 Provide shielded safety lighting along trails and other public and private walkways separated from a street.

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**Implementation  
Guide**



See Section XVIII in the Implementation Guide.

## HAZARDS MANAGEMENT

Our industrial processes have created a myriad of products and materials that make life easier. Unfortunately, many of these products and associated by-products present hazards to human health. The current regulatory environment provides a high level of protection from the hazardous materials manufactured within, transported to, and stored in Brea's industrial businesses. By recognizing these hazards and ensuring that an educated public can work with City officials to minimize risks associated with hazardous materials in the urban environment, Brea can create safe conditions citywide.

### Context

Many industrial businesses and oil production and processing companies are permitted to store, transport, and handle hazardous materials in Brea. Businesses that handle and generate small quantities of hazardous materials include dry cleaners, auto repair shops, medical facilities, and photo processing centers. Larger businesses, primarily in the industrial areas, can generate large quantities of hazardous products.

#### Industrial Businesses

Hazardous materials are handled and stored on a number of properties in Brea, primarily in the western industrial districts. More than 130 small quantity generators produce between 100 and 1,000 kilograms of hazardous waste annually. Large quantity generators, of which several call Brea home, produce over 1,000 kilograms per year. Per federal law, all such businesses register with the Environmental Protection Agency (EPA).



*Used oil and other hazardous waste can be dumped at the Household Hazardous Waste Collection Center in the city of Anaheim.*

Although the use of hazardous materials is carefully regulated today, past activities have led to the contamination of several sites in the City. Contamination has resulted from leaking underground storage tanks, disposal of hazardous materials, and various industrial practices. These lingering conditions and the potential for accidents and spills from currently regulated businesses challenge the City to reduce risks and prepare for emergencies.

The Superfund Act is a federal law designed to protect the environment from risks created from previous chemical disposal practices. Eleven “archive” sites in Brea have been identified as Superfund sites under the Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA). Most are located in the western industrial section of City, although the Olinda Alpha landfill is included on the list. Archive status indicates that to the best of the EPA’s knowledge, no immediate or long-term risks to human health or to the environment are associated with these sites.

The EPA has identified more than 20 facilities in Brea on the Toxic Release Inventory (TRI). These are sites that are known to release toxic chemicals into the air. The EPA closely monitors the emissions from these facilities to ensure that their annual limits allowed under federal regulations are not exceeded.

In the 1990s, the State of California’s Department of Toxic Substances Control aggressively pursued a program to identify and force remediation of Leaking Underground Storage Tank (LUST) sites throughout the State. Some of these fuel leaks resulted in substantial soil and groundwater contamination. Over 50 sites in Brea identified as sources of industrial solvent or metals contamination have likewise been subjected to target clean-up efforts. Thus, the problems of past practices largely have been addressed. Focus instead has been turned toward avoiding future contamination.

The Environmental Health Division of Orange County implements the Hazardous Waste Inspection Program throughout County. The program ensures that all hazardous wastes generated by Orange County businesses are properly handled, recycled, treated, stored, and disposed. The Orange County Certified Unified Program Agency is the local administrative agency that coordinates the following six programs regulating hazardous materials and hazardous wastes:

- Hazardous Waste
- Underground Storage Tanks (UST)
- Aboveground Storage Tanks (AST)
- Hazardous Materials Disclosure (HMD)
- Business Plan
- California Accidental Release Program (CalARP).

## Oil Fields

Historically, Brea was founded as an oil community. In Spanish, *brea* means tar, of which plenty was discovered in the hills above the town. Oil fields in Brea today contain producing wells and associated petroleum and natural gas facilities managed by a number of energy companies. Production levels fluctuate depending upon world energy prices. Many wells have been closed and abandoned after years of operation. Property owners continue to explore land use options for the hilly terrain. Land use policy limits development based on slopes and other property constraints. Should an owner pursue development, remediation will be necessary to clean the sites and render them suitable for housing. Remediation can include cleanup operations to remove soils contaminated with hazardous materials common to oil fields.



*Sucker rod pumping unit extracting oil from a well in Tonner Canyon. Driven by a motor which turns a flywheel with a crank arm, the sucker rod pump can easily lift oil 10,000 feet or more.*

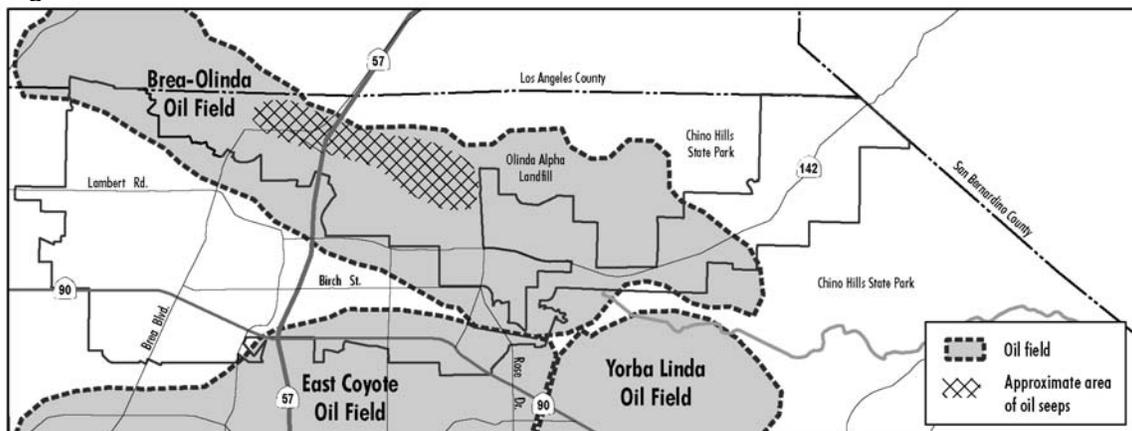
Brea may continue to have oil wells and associated uses in its midst as long as production remains economically viable. The major area of potential of environmental concern is oil spills. Spills can occur adjacent to wells, at a storage tank, due to broken pipes, and during operation and maintenance. A secondary concern is explosions from gas plants and pressurized facilities.

### Oil Seeps

The Brea-Olinda oil field (Figure PS-1) is one of the oldest producing regions in California. Natural oil seeps inspired early pioneers to drill for oil, leading to the discovery of the oil fields during the 1880s. Oil seeps are natural springs where liquid hydrocarbons (mixtures of crude oil, tar, natural gas, and water) leak out of the ground. Whereas freshwater springs are fed by underground pools of water, oil seeps are fed by natural underground deposits of oil.

Unlike human-caused pollutants, most oil and gas seeps are produced by natural geologic processes that take place over millions of years. Though natural, they can nevertheless pollute air and watercourses. The emergence of oil seeps often occurs following a major earthquake. Oil seeps or concentrations of methane at explosive or near explosive limits can pose a constraint to development.

**Figure PS-1: Oil Fields**



Source: California Department of Conservation, Division of Oil, Gas, and Geothermal Resources, 2002; U.S. Geological Survey, <http://seeps.wr.usgs.gov>, 2002.

## Goals and Policies

Understanding the potential threat of hazards and creating safeguards to prevent accidents will ensure a level of safety for Brea residents and the community at large. The following goals and policies express the importance of working with and supporting other agencies that monitor and regulate hazardous waste and oil operations.

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**Goal PS-4    Protect the community from the hazards associated with the transportation, use, and storage of hazardous materials in the urban environment.**

---

Policy PS-4.1    Ensure that hazardous materials used in businesses and industry are handled properly.

Policy PS-4.2    Reduce the risks associated with ground transportation hazards.

Policy PS-4.3    Work with responsible Federal, State, and County agencies to identify and regulate the disposal of toxic materials.

Policy PS-4.4    Provide education and information to City residents regarding the proper use and disposal of household hazardous materials.

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**Goal PS-5:    Minimize the public's exposure to potential hazards associated with existing and abandoned oil facilities.**

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Policy PS-5.1    Work closely with responsible State and Federal agencies to ensure that active oil field operations comply with all current regulations and, once oil field operations cease, that appropriate closure and clean-up activities occur.

Policy PS-5.2    Continue to support the regulations of the California Division of Oil, Gas, and Geothermal Resources regarding abandoned oil facilities.

Policy PS-5.3    Require comprehensive investigation, disclosures, and remediation of any former oil field property proposed for an alternative use.

**Implementation  
Guide** 

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See Section XIX in the Implementation Guide.

# WILDLAND FIRES

Wildland fires pose a major threat to isolated development located within hillside areas and also to residential development along the fringes of the hillsides. During the summer season, dry vegetation, little seasonal rain, and Santa Ana wind conditions can combine to increase the likelihood of fires. Understanding the risks associated with development in and near fire-prone areas can help advance planning to reduce the risks associated with major wildland fires.

## Context

A significant portion of Brea's sphere of influence area remains undeveloped and consists of rugged topography with highly flammable native vegetation. The woodlands, grasslands, and chaparral areas, while providing important open space, natural resources, and scenic qualities, also create a high fire hazard when development is located in or adjacent to these areas. Wildland fires have been documented to have occurred within Carbon Canyon and the hillsides within Chino Hills State Park.



*Fires usually last only a few hours or days, but their effects can last much longer. An intense wildfire may destroy all the vegetation. Fire also destroys most of the roots that hold the soil in place, allowing running water to wash the soil away. As a result, even normal rainfall can cause exceptional erosion, flooding, and sediment flows from a burned area.*

Wildland fire is a natural process. In the past, the presumption has been that all fire is bad and should be extinguished promptly. This has caused fire-starved vegetation to grow more densely, which ultimately weakens plants in a struggle for living space and increases destruction by pests and disease. Finding the balance between accommodating nature's course and protecting people and property from fire's destructive forces presents a continuing challenge to government agencies.

## High Fire Hazard Areas

The City has established its wildland boundary, known as the High Fire Hazard Area (Figure PS-2), and has developed wildland interface construction and development standards for all new structures built within this boundary. Examples of such construction requirements include:

- Automatic fire sprinkler system
- Class A roof covering
- Exterior wall surface made of one-hour fire rated materials
- Spark arrestor on chimneys
- Site landscape fuel modification

The Brea Fire Department has also developed standards to accommodate all types of fire apparatuses utilized by the Brea Fire Department, circulation and street design, and water system requirements for all new development proposed in the High Fire Hazard Areas.

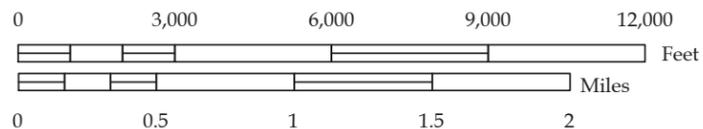
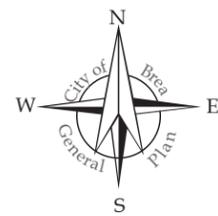
A computer model was also used to determine different zones of fire susceptibility. Different variables such as vegetation, weather, topography, and other related factors were imputed into the model to determine which areas in the Brea Planning Area were vulnerable to wildland fires. Figure PS-2 shows the results of the model. Most of the very high fire areas are located in Tonner and Brea Canyons.

### **Site Landscape Fuel Modification Zones**

A fuel modification zone is the area of land directly adjacent to a building where combustible vegetation has been removed or modified or both and partially or totally replaced with drought-tolerant, fire-resistant plants to provide an acceptable level of risk from wildland fires. Fuel modification reduces radiant and convective heat, thereby providing fire suppression forces a safer area in which to take action. The Brea Fire Department has set a minimum distance of 180 feet beyond the construction boundary of any property on which a structure can be built as its fuel modification zone within the designated High Fire Hazard Areas of the City.



Source: □ Earth Consultants International, Inc., 2001; City of Brea Fire Department. □



**Legend**

- City Boundary
- Sphere of Influence

**High Fire Hazard Areas (Designated by the City of Brea Fire Department)**

- ▨ Very High Fire Hazard Area (City of Brea)
- ▩ Very High Fire Hazard Area (Sphere of Influence)

**Fire Susceptibility Mapping**

- Very High Fire Area
- High Fire Area
- Moderate Fire Area
- Low to Very Low Fire Areas

**Fire Stations**

- ① Fire Stations
- ② New Fire Station #2 (Underconstruction)
- ② Temporary Facility (Station #2)



Figure PS-2  
High Fire Hazard Areas



The best proactive measure homeowners can pursue to reduce the risks wildland fires is to maintain a fuel modification and vegetation clearance zone around structures consistent with Fire Department regulations, and to plant landscaping that can slow a fire's advance.

### **Fire-Fighting Resources**

The level of hazard to life and property is affected not only by the fire itself but by road access for evacuation, the number of available firefighters, vegetation clearance around property, availability of water and water pressure, and the effectiveness of building codes and inspection of developments in fire hazard areas.

Station 4 is located at 170 North Olinda Place to enhance response to fires in areas accessible directly via Carbon Canyon Road. During fire season, the station's staffing levels are often enhanced to ensure adequate response. However, should development levels increase within Carbon Canyon or other hillside areas within or adjacent to high fire hazard zones, increased response capabilities will become necessary.

The Emergency Services and Safety Section within this chapter discusses fire-flow analysis and adequate water pressure to combat urban and wildland fires within Brea's Planning Area.

## **Goals and Policies**

New development will create increased fire hazards due to higher levels of interactions between open grassland and residential development. The City will continue to reduce the potential for dangerous fires by coordinating with the Orange County Fire Agency to implement fire hazard education, fire protection, and fuel modification programs. In addition, the City will work closely with the local water districts and the County to ensure that water pressure is adequate for fire-fighting purposes. Development proposals within high fire areas will be required to provide appropriate and adequate safeguards and response capabilities to prevent the loss of structures and to ensure established development does not experience reduced service. New development may be required to fund new fire facilities or equipment toward this end.

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**Goal PS-6    Protect the community from wildland fires.**

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- Policy PS-6.1    Consider implementing a vegetation management plan that considers non-traditional methods of controlling vegetation, such as prescribed burning, in undeveloped areas.
- Policy PS-6.2    Encourage residents to plant and maintain fire-retardant slope cover to reduce the risk of brush fire in areas adjacent to the canyons, and develop and implement stringent site design and maintenance standards for areas with high fire potential. To the extent possible, native, non-invasive plant material are encouraged.
- Policy PS-6.3    Assure provision of adequate fire equipment access and fire suppression resources to all developed and open space areas.
- Policy PS-6.4    Require new development to ensure that the City's five-minute fire response time be maintained.
- 

**Implementation  
Guide**



See Section XX of the Implementation Guide for action programs.

# FLOOD CONCERNS

Portions of Brea are susceptible to limited but credible flood events from either a major storm or dam failure resulting from a significant earthquake. Recognizing and preparing for such occurrences allow Breans to avoid associated dangers.

## Context

Brea has a history of major flooding in the past, but comprehensive storm drain improvements and the construction of Carbon Canyon Dam in 1961 have reduced the threat of floods, but not entirely. An unusually large storm and flash flooding can create flooding hazards within the City.

### Storm Flooding

The unpredictable seasonal range in rainfall that is typical of coastal southern California, coupled with geographic and geologic conditions, makes Brea vulnerable to flooding during the winter storm season. The Orange County Flood Control District has constructed and maintains several improved flood control channels within Brea, most notably Brea Creek Channel west of and parallel to Brea Boulevard (see Figure PS-3) and the Loftus Channel east of SR-57, below Birch Street. The Brea Creek Channel captures and conveys significant runoff from the hillsides. Any new development that would affect the volume or speed of runoff within creeks tributary to the Brea Creek Channel would be required to ensure that channel capacity and function be maintained to protect adjacent and downstream properties.

The City participates in the National Flood Insurance Program. Flood Insurance Rate Maps prepared by the Federal Emergency Management Agency, or FEMA, show potential flood zones for the 100-year and 500-year flood zones.<sup>2</sup> Figure PS-3 indicates the location of these flood zones, including channelized areas wherein the channel is capable of containing 100-year flood volumes. The area upstream from Carbon Canyon Dam shown in Figure PS-3 is open space specifically designed and intended to accommodate flood waters.

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<sup>2</sup> These are floods that, respectively, have a 1 percent and 0.2 percent chance of occurring every year.

## Dam and Reservoir Failure

The two dams located within the Brea planning area, Carbon Canyon Dam and Orange County Reservoir, provide valuable flood control and water storage functions. Carbon Canyon Dam retains water over a 221-acre area and has a capacity of 7,033 acre-feet.<sup>3</sup> Carbon Canyon Dam is vital for the flood protection of portions of the coastal plains in Orange County, including the cities of Fullerton, Placentia, and Anaheim. The U.S. Army Corps of Engineers is responsible for the dam's safety and conducts inspections on a regular basis. In the unlikely event of dam failure due possibly to an earthquake, areas below the dam would be flooded to the extent illustrated in Figure PS-3.

Orange County Reservoir, used for water storage, is owned by the cities of Brea (40%), La Habra (20%), and Fullerton (40%), and is operated by the Metropolitan Water District. The reservoir covers 7 acres and has a capacity of 217 acre-feet. Like Carbon Canyon Dam, this dam receives periodic inspections. The flood inundation path (Figure PS-3), should the dam fail, would affect largely residential properties.

Five reservoir tanks in the hills above Brea contain a total of 67.3 million gallons of domestic-use water. All but the Valencia reservoir have been built consistent with up-to-date seismic codes.

## Goals and Policies

Brea will strive to protect residents and the community at large from flood hazards.

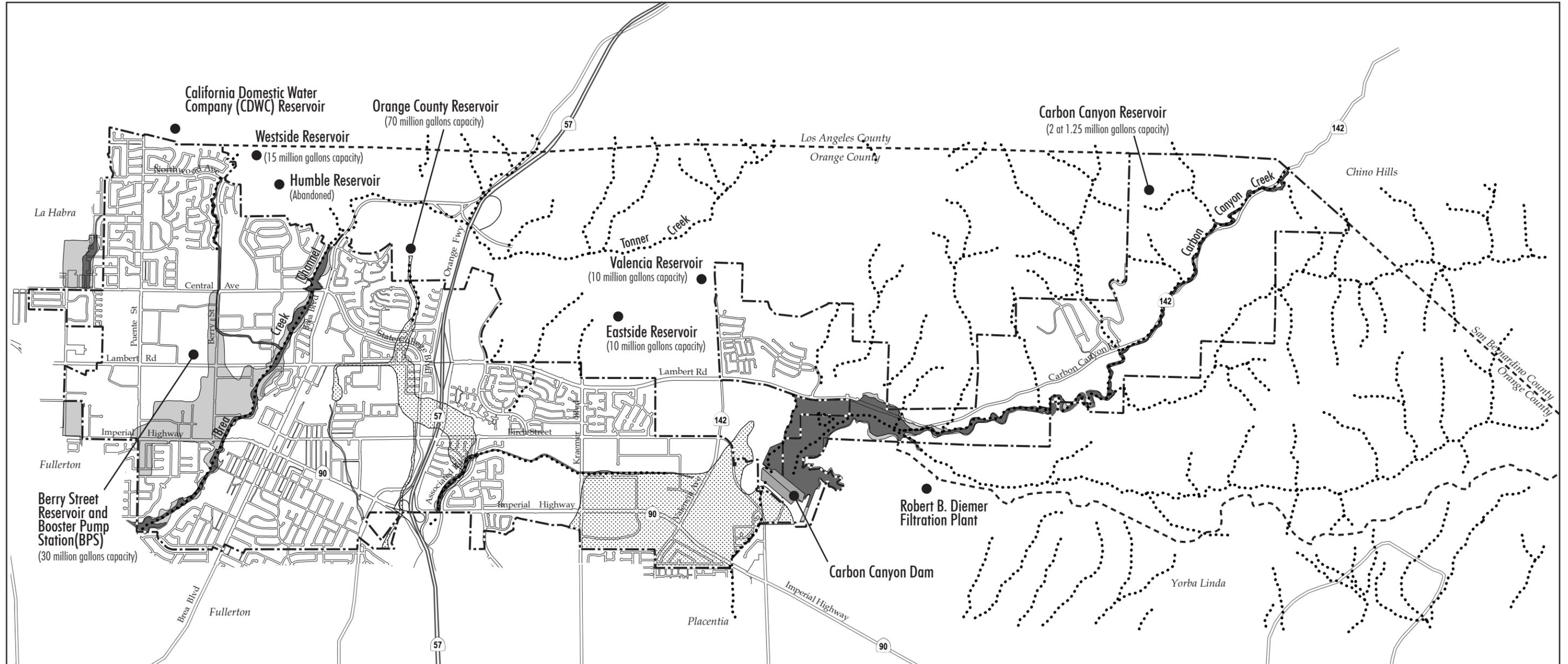
<b>Goal PS-7: Reduce the risk to the community from flooding hazards.</b>
---

Policy PS-7.1: Cooperate and work with the Orange County Flood Control District to ensure District flood control facilities are well maintained and capable of accommodating, at a minimum, 100-year storm flows.

Policy PS-7.2: Require that new developments minimize stormwater and urban runoff into drainage facilities by incorporating design features such as detention basins, on-site water features, or other strategies.

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<sup>3</sup> One acre-foot equals 325,851 gallons.



Source: □ Federal Emergency Management Agency-National Flood Insurance Program, May 1996;  
 □ Earth Consultants Internation, January 2002; California Governor's Office of Emergency Services (OES); Los Angeles County Department of Water and Power. □

**Legend**

- City Boundary
- - - Sphere of Influence
- ..... Creeks, Streams, and Drainage Channels

**Floodplains**

- 100-Year Floodplain
- 500-Year Floodplain

**Dam Failure Inundation and Reservoir Location**

- Reservoir
- ▨ Dam/Reservoir Failure Inundation Pathway

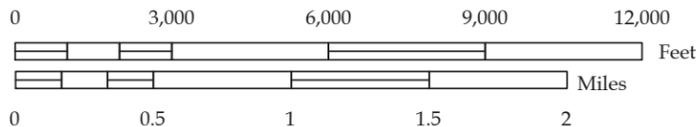
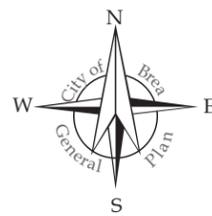


Figure PS-3  
 Flooding Hazards



Policy PS-7.3: Maintain an active storm drain inspection program.

Policy PS-7.4: Protect critical facilities located within areas subject to flooding.

Policy PS-7.5: Evaluate and monitor water storage facilities to determine which facilities are not self-contained and might pose an inundation hazard to downstream properties.

Policy PS-7.6: Employ strategies and design features that will reduce the amount of impervious surfaces (i.e. paved area) for new development projects.

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**Implementation  
Guide**



See Section XXI of the Implementation Guide for action programs.

## GEOLOGIC AND SEISMIC CONSIDERATIONS

The complex topography and geology of the Puente and Chino Hills create wonderful visual features that form a strong backdrop to Brea. These features are evidence of conditions that pose potential constraints to development. Brea lies within one of the most potentially seismically active areas of Southern California, adjacent to the Whittier fault and atop the Elysian Park Thrust fault. Both of these fault zones have the potential to generate moderate to large earthquakes that could cause substantial property damage and possibly loss of life. Most properties within the sphere of influence area and Carbon Canyon are characterized by steep rugged hillside terrain subject to landslides and soil erosion. Areas adjacent to stream beds and drainage channels tend to exhibit liquefaction conditions and ground instability.



*Ground shaking associated with earthquakes can cause slope failures.*

### Context

#### Seismic Hazards

Two major faults traverse the planning area, the Whittier fault and Elysian Park Thrust fault. The Whittier fault cuts across the hills and through the eastern half of the City in a northwesterly direction (Figure PS-4). Several traces are considered active. The Elysian Park Thrust fault, a buried fault approximately 6 to 10 miles below the ground surface, is considered to be more threatening, with a greater potential to cause a large magnitude earthquake. In hillside areas an earthquake may also trigger landslides. In flat valley areas, the deep, alluvial soils can increase the amplitude and duration of groundshaking.

### Earthquake Magnitude and Intensity

Magnitude and intensity measure different characteristics of earthquakes. Magnitude measures the energy released at the source or epicenter of the earthquake with the use of a seismograph. Intensity measures the strength of shaking produced by the earthquake at a certain location and is determined from effects on people, human structures, and the natural environment. Table PS-1 gives intensities that are typically observed at locations near the epicenter of earthquakes of different magnitudes.

Preparing for earthquakes requires preparing for the worst-case scenario earthquake. Engineers construct dams and bridges to withstand the maximum credible earthquake (MCE) to prevent structure failure. The MCE is defined as the largest possible earthquake that could reasonably occur along the recognized faults or within a particular seismic source, and by definition has a very low probability of occurrence. City officials can prepare against the worst case scenario earthquake by understanding the potential effects of a MCE. Table PS-2 lists magnitude and intensity of the MCE for faults that could potentially impact Brea.

**Table PS-2**  
**Magnitude and Intensity of Maximum Credible Earthquake for**  
**Faults Potentially Impacting Brea**

<b>Fault Name</b>	<b>Distance to Brea (miles)</b>	<b>Magnitude of MCE</b>	<b>Intensity from MCE</b>
Elysian Park Thrust	0	6.7	X
Whittier	Less than a mile	7.3	X
San Andreas	33	7.8	VII

Source: Earth Consultants International, 2002.

Computer models can estimate the effects of earthquakes with the potential magnitudes described in the above table. An earthquake on either the Elysian Park Thrust or Whittier fault could damage residential buildings, particularly those constructed with wood or reinforced masonry, as well as mobile homes.

Computer analyses conducted for Brea estimated more than 12,000 buildings could be damaged during an earthquake on the Elysian Park Thrust fault, and more than 15,000 buildings could be at least moderately damaged during an earthquake on the Whittier fault. Soft-story buildings do not typically perform well in earthquakes. These buildings are typically characterized by a first story lacking adequate strength or toughness. The soft-story is prevalent in the older historic neighborhood along South Brea Boulevard.

**Table PS-1  
Magnitude and Intensity Scales of Earthquakes**

<b>Magnitude</b>	<b>Descriptor</b>	<b>Intensity</b>	<b>Description</b>
1.0 - 3.0	Very Minor	<b>I</b>	<b>I.</b> Not felt except by a very few under especially favorable conditions.
3.0 - 3.9	Minor	<b>II - III</b>	<b>II.</b> Felt only by a few persons at rest, especially on upper floors of buildings.  <b>III.</b> Felt quite noticeably by persons indoors, especially on upper floors of buildings. Many people do not recognize it as an earthquake. Standing motor cars may rock slightly. Vibrations similar to the passing of a truck. Duration estimated.
4.0 - 4.9	Light	<b>IV - V</b>	<b>IV.</b> Felt indoors by many, outdoors by few during the day. At night, some awakened. Dishes, windows, doors disturbed; walls make cracking sound. Sensation like heavy truck striking building. Standing motor cars rocked noticeably.  <b>V.</b> Felt by nearly everyone; many awakened. Some dishes, windows broken. Unstable objects overturned. Pendulum clocks may stop.
5.0 - 5.9	Moderate	<b>VI - VII</b>	<b>VI.</b> Felt by all, many frightened. Some heavy furniture moved; a few instances of fallen plaster. Damage slight.  <b>VII.</b> Damage negligible in buildings of good design and construction; slight to moderate in well-built ordinary structures; considerable damage in poorly built or badly designed structures; some chimneys broken.
6.0 - 6.9	Strong	<b>VIII - IX</b>	<b>VIII.</b> Damage slight in specially designed structures; considerable damage in ordinary substantial buildings with partial collapse. Damage great in poorly built structures. Fall of chimneys, factory stacks, columns, monuments, walls. Heavy furniture overturned.  <b>IX.</b> Damage considerable in specially designed structures; well-designed frame structures thrown out of plumb. Damage great in substantial buildings, with partial collapse. Buildings shifted off foundations.
7.0 - 7.9	Major	<b>X - XII</b>	<b>X.</b> Some well-built wooden structures destroyed; most masonry and frame structures destroyed with foundations. Rails bent.  <b>XI.</b> Few, if any (masonry) structures remain standing. Bridges destroyed. Rails bent greatly.  <b>XII.</b> Damage total. Lines of sight and level are distorted. Objects thrown into the air.
8.0 and higher	Great		

Source: United States Geological Survey (USGS) National Earthquake Information Center, (<http://neic.usgs.gov>), April 2001.

An MCE earthquake on the Whittier fault could potentially create an economic loss of \$1.04 billion (equivalent to 27 percent of the total replacement value of the City's buildings). An Elysian Park Thrust fault MCE earthquake could potentially create an economic loss of \$518 million (equivalent to 13 percent of the total replacement value of the City's buildings). Even an MCE earthquake on the San Andreas fault, located approximately 30 miles away, could be potentially devastating, destroying approximately 8,000 buildings and potentially causing an economic loss over \$300 million.

### **Alquist-Priolo Earthquake Fault Zoning Act**

The State of California, per the requirements of the Alquist-Priolo Earthquake Fault Zoning Act, requires the delineation of earthquake fault zones along faults that are sufficiently active and well-defined. The Act requires cities and counties to withhold development permits for sites within an earthquake fault zone until geologic investigations demonstrate that the sites are not threatened by surface displacements from future faulting. In Brea, the Whittier fault meets this definition, and the fault zone boundaries shown in Figure PS-4 reflect State-delineated boundaries.

### **Liquefaction, Landslides, and Soil Erosion**

Geologic hazards associated with ground shaking include liquefaction and ground failure. Liquefaction is a phenomenon in which the stiffness of a soil is reduced when ground shaking causes water-saturated soil to become fluid and lose its strength. Liquefaction and related phenomena have been responsible for tremendous amounts of damage, creating problems with bridges, buildings, buried pipes, and tanks. Liquefaction hazards are significant along stream channels due to the porous nature and high water content of the soil. These areas include Tonner Canyon Creek, Brea Canyon, and areas around the Carbon Canyon Dam (Figure PS-4).

Steep topography, fractured and unconsolidated bedrock conditions, expansive soils, and high erosion potential make many hillside areas highly unstable. Landslides are typical on moderate to steep slopes. Historically, Brea's hillsides have experienced many failures, as illustrated in Figure PS-4, and the potential for future landslides is high. Landsliding may result from heavy rain, erosion, removal of vegetation, and seismic activity. While failed slope materials can be removed and slopes stabilized to allow development, great care must be taken to identify all potential geologic hazards wherever development is proposed.

## Goals and Policies

In Brea, hillside development is tricky, given the constraints posed by local geology. Land use policies set forth in the Community Development chapter identify the City’s approach to allowing development in areas with challenging terrain and geologic conditions, policies that balance the need to protect life and property with the goal of providing new housing opportunities. In all cases, development can only occur where living conditions can be made safe.

The combination of sound planning practices and continued education of residents will minimize risks to the community from seismic and geologic hazards, and will protect the health, safety, and welfare of Brea residents.

<b>Goal PS-1</b>	<b>Reduce the risk to the community from seismic activity and geologic conditions, including ground shaking, fault rupture, liquefaction, and landslides.</b>
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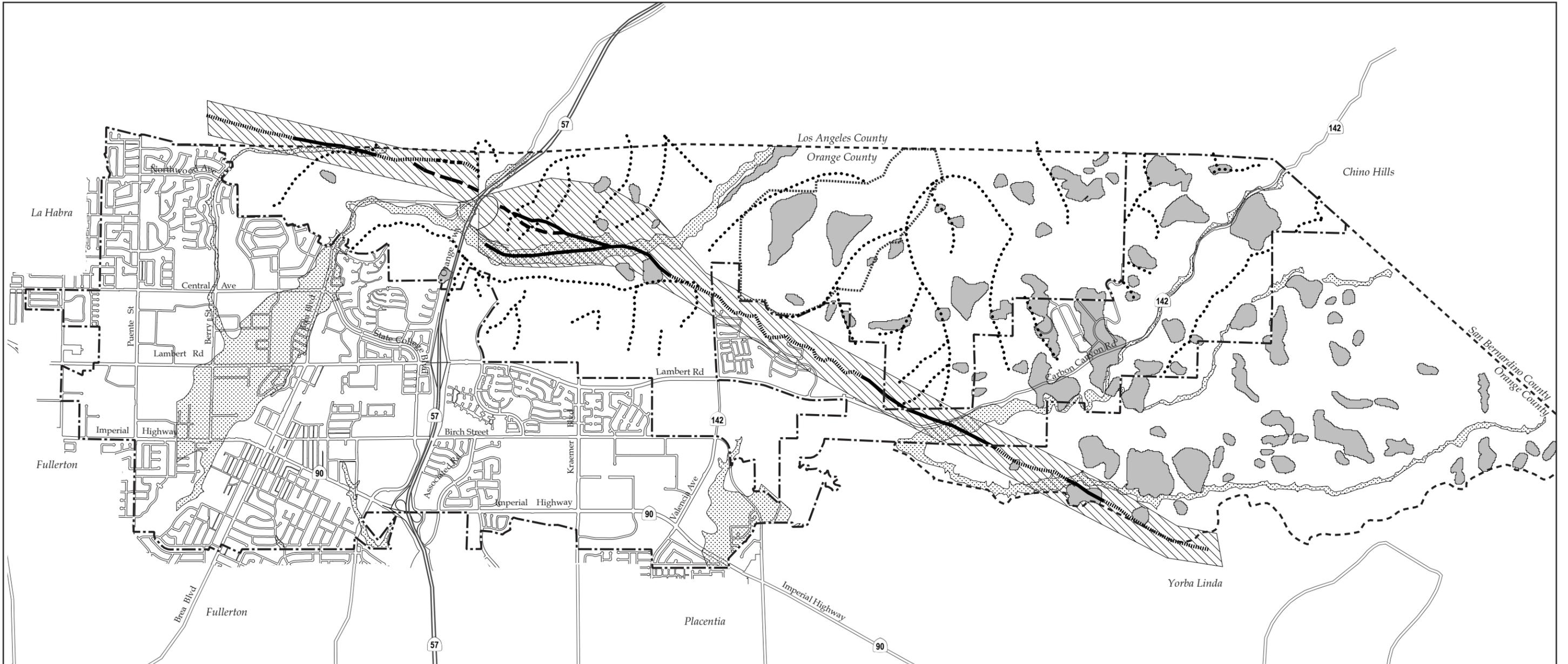
Policy PS-8.1 Minimize the potential damage to structures and loss of life that may result from an earthquake.

Policy PS-8.2 Require seismic safety standards for construction of all new buildings.

Policy PS-8.3 Continue to require geological and geotechnical investigations of all new developments in areas of potential seismic or geologic hazards as part of the environmental and development review process.

Policy PS-8.4 Require that careful, site-specific evaluations based on detailed surface and subsurface geotechnical studies be conducted in areas where landslides are suspected or known to occur.

Policy PS-8.5 Participate in Federal, State, and local earthquake preparedness and emergency response programs.



Source: □ Earth Consultants Internation, January 2002; California Division of Mines and Geology, 1980, 1991, and 1998. □

**Legend**

- City Boundary
- - - Sphere of Influence

**Seismic Hazards**

- Accurate Fault Location
- - - Approximate Fault Location
- · · · · Concealed Fault
- ||||| Inferred
- ▨ Alquist-Priolo Earthquake Fault Zone
- ▤ Liquefaction
- Mapped Landslides
- · · · · Major Ridgelines

Areas where historic occurrence of liquefaction, or local geological, geotechnical and groundwater conditions indicate a potential for permanent ground displacements such that mitigation as defined in Public Resource Code Section 2693c would be required

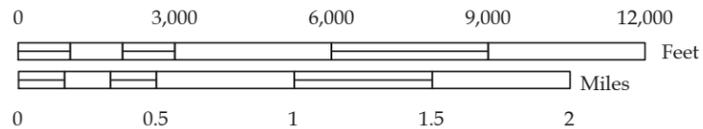
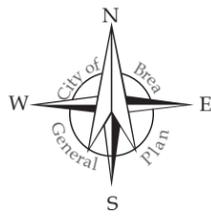


Figure PS-4  
Geologic and Seismic Hazards



Policy PS-8.6 Continue programs, such as the Community Assistance and Response to Emergency Situations (C.A.R.E.S), that further educate and train individuals and neighborhoods how to respond to emergency situations.

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**Implementation  
Guide**



See Section XXII of the Implementation Guide for action programs.

## NOISE

Studies have identified noise as an environmental pollutant that can have substantial and permanent impacts on human health and general well-being. Not only is noise a health hazard, excessive noise can be a source of annoyance, tension, and discomfort that disrupts everyday activities. Brea aims to substantially reduce noise and its impacts within the urban environment, with a focus on protecting residential neighborhoods, schools, and similar sensitive uses.

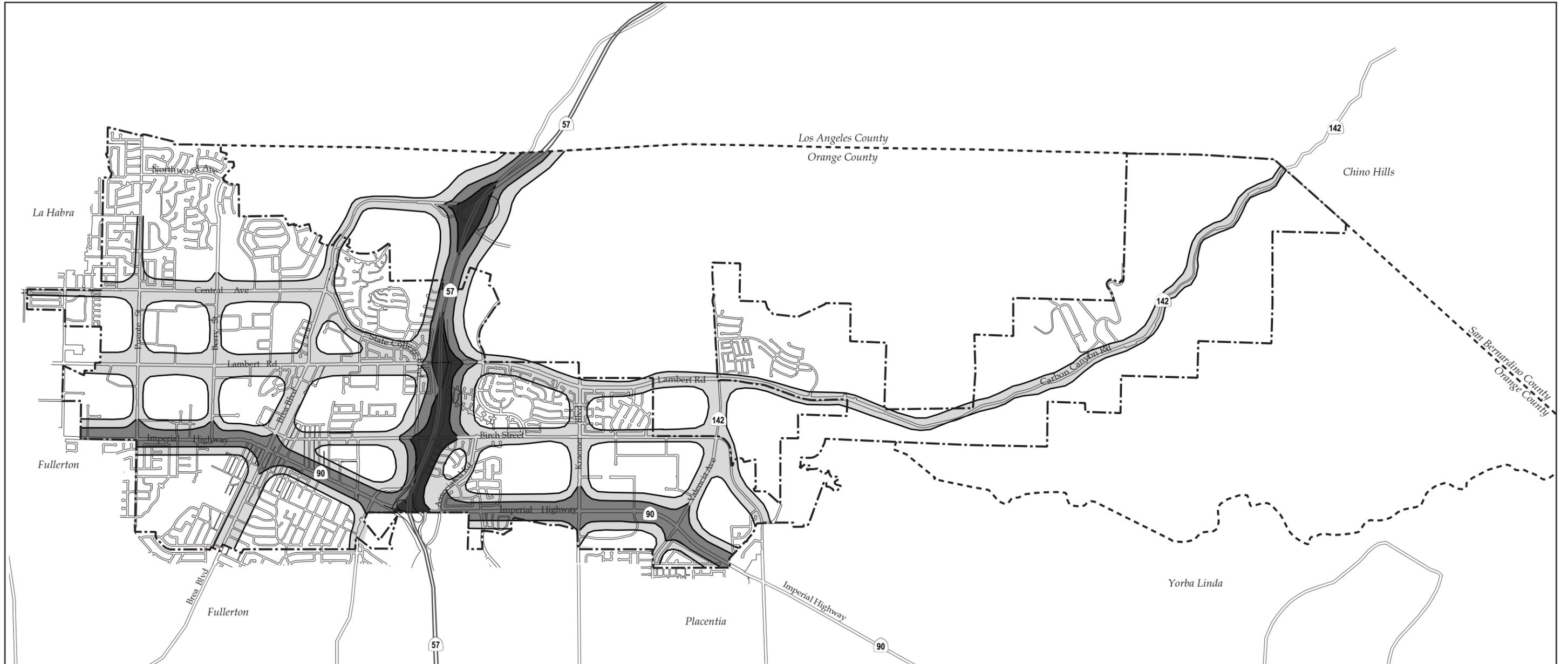
### Context

In Brea, street and freeway traffic represent the primary noise sources. Industrial and commercial activity present concerns where adjacent to residential neighborhoods. In addition, mechanical equipment, playgrounds, leaf blowers, and construction equipment are examples of random noise sources that can contribute to neighborhood noise.

Figure PS-5 displays a composite picture of average noise levels in Brea in 2001. As Figure PS-5 illustrates, the loudest noise levels occur along State Highway 57, Imperial Highway, Brea Boulevard, and Valencia Avenue, where truck traffic associated with the Olinda Alpha landfill lingers past homes and places of business. Lambert Road also experiences loud noise levels due to access to State Highway 57.

### Noise and Land Use Compatibility Guidelines

Noise generally is defined as unwanted or intrusive sound. Because noise consists of pitch, loudness, and duration, describing noise with a single unit of measure presents a challenge. The A-weighted decibel scale, or dB(A), has been developed to describe the loudness of a sound or sound environment based on the sensitivity of the human ear. A sound level meter that measures A-weighted decibels has an electrical circuit that allows the meter to have the same sensitivity to sound at different frequencies as the average human ear. Table PS-3 indicates criteria the State has established to reduce adverse noise effects on human health.



Source: Weiland Associates, 2001. □

**Legend**

- City Boundary
- Sphere of Influence

**Community Noise Equivalent Level (CNEL) Contours**

- Less than 60 CNEL
- 60 to 65 CNEL
- 65 to 70 CNEL
- Greater than 70 CNEL

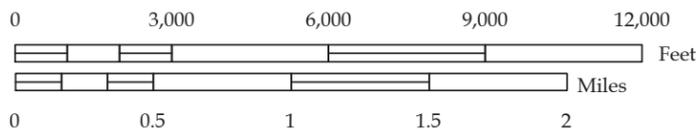
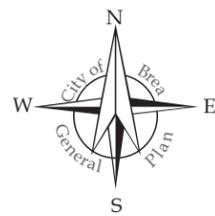


Figure PS-5  
Noise Contours (2001)



**Table PS-3**  
**State Criteria for Minimizing Adverse Noise Effects on Humans**

<b>Objective</b>	<b>dB(A) Range</b>
Prevent Hearing Loss	75-80
Prevent Physiological Effects (Other than hearing loss)	65-75
Prevent Speech Interference	50-60
Address People's Subjective Preference for Noise Control	45-50
Prevent Sleep Interruption	35-45

Source: California General Plan Guidelines, 2000.

The dB(A) descriptor only reports noise from a single source or combination of sources at a point in time. To allow a more comprehensive description of a noise environment, federal and State agencies have established noise and land use compatibility guidelines that use averaging approaches to noise measurement. Two measurement scales commonly used in California are the Community Noise Equivalent Level (CNEL) and the day-night level (Ldn). To account for increased human sensitivity at night, the CNEL level includes a 5-decibel penalty on noise during the 7:00 A.M. to 10:00 P.M. time period and a 10-decibel penalty on noise during the 10:00 P.M. to 7:00 A.M. time period. The Ldn level includes only the 10 decibel weighting for late-night noise. These values are nearly identical for all but unusual noise sources.

The City will use land use compatibility standards when planning and making development decisions in order to ensure that noise producers do not adversely affect sensitive receptors.

The City's primary goal with regard to community noise is to minimize the exposure of residents to unhealthy or excessive noise levels to the extent possible. To this end, the noise/land use compatibility guidelines in Figure PS-6, based on cumulative noise criteria for outdoor noise, are used to review development proposals and to identify and mitigation measures necessary to avoid or minimize impacts, including traffic noise impacts, a new use may have on established uses.

**Figure PS-6  
Noise/Land Use Compatibility**

Land Use Category	Community Noise Equivalent Level (CNEL) or Day-Night Level (Ldn), dB						
	55	60	65	70	75	80	85
Residential- Low-Density Single-Family, Duplex, Mobile Homes	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Residential- Multiple Family	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Transient Lodging - Motels, Hotels	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Schools, Libraries, Churches, Hospitals, Nursing Homes	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Auditoriums, Concert Halls, Amphitheaters	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Sports Arenas, Outdoor Spectator Sports	White	White	Diagonal	Diagonal	Diagonal	Diagonal	Diagonal
Playgrounds, Neighborhood Parks	White	White	White	White	Diagonal	Diagonal	Diagonal
Golf Courses, Riding Stables, Water Recreation, Cemeteries	White	White	White	White	Diagonal	Diagonal	Diagonal
Office Buildings, Business, Commercial and Professional	White	White	White	Diagonal	Diagonal	Diagonal	Diagonal
Industrial, Manufacturing, Utilities, Agriculture	White	White	White	White	Diagonal	Diagonal	Diagonal

**Nature of the noise environment where the CNEL or Ldn level is:**

**Below 55 dB**  
Relatively quiet suburban or urban areas, no arterial streets within 1 block, no freeways within 1/4 mile.

**55-65 dB**  
Most somewhat noisy urban areas, near but not directly adjacent to high volumes of traffic.

**65-75 dB**  
Very noisy urban areas near arterials, freeways or airports.

**75+ dB**  
Extremely noisy urban areas adjacent to freeways or under airport traffic patterns. Hearing damage with constant exposure outdoors.

-  **Normally Acceptable**
-  **Conditionally Acceptable**
-  **Normally Unacceptable**
-  **Clearly Unacceptable**

Specified land use is satisfactory, based on the assumption that any buildings are of normal conventional construction, without any special noise insulation requirements

New construction or development should be undertaken only after a detailed analysis of noise reduction requirements is made and needed noise insulation features included in design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning, will normally suffice.

New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of noise reduction requirements must be made and needed noise insulation features included in design.

New construction or development should generally not be undertaken.

The Community Noise Equivalent Level (CNEL) and Day-Night Noise Level (Ldn) are measures of the 24-hour noise environment. They represent the constant A-weighted noise level that would be measured if all the sound energy received over the day were averaged. In order to account for the greater sensitivity of people to noise at night, the CNEL weighting includes a 5-decibel penalty on noise between 7:00 p.m. and 10:00 p.m. and a 10-decibel penalty on noise between 10:00 p.m. and 7:00 a.m. of the next day. The Ldn includes only the 10-decibel weighting for late-night noise events. For practical purposes, the two measures are equivalent for typical urban noise environments.

## Noise Contours and Noise Impact Areas

The use of noise contours based on the major noise sources can describe the noise environment for the community. Noise contours outline areas of equal noise exposure. Future noise contours have been estimated using information about existing and projected land use development and transportation activity.

The projected noise contours and noise impact areas for Brea are displayed in Figure PS-7. These contours will serve as a guide for land use and development decisions. Contours of 60 dB(A) or greater define noise impact areas. An acoustical analysis must be prepared when noise sensitive land uses are proposed within noise impact areas. The analysis must show that the project is designed to attenuate noise to meet the City's noise standards in order to receive approval. If the project design does not meet the noise standards, mitigation can be recommended in the analysis. If the analysis demonstrates that the noise standards can be met by implementing the mitigation measures, the project can be approved conditioned upon implementation of the mitigation measures.

## Transportation Related Noise

Transportation activity is the primary source of noise in Brea. The three major sources of transportation related noise are:

- Traffic on the Orange Freeway (SR-57)
- Commercial truck traffic associated with the Olinda Alpha landfill on Valencia Avenue and Imperial Highway
- Automobile traffic on Brea Boulevard, Brea Canyon Road, Central Avenue, and Lambert Road

Residential neighborhoods bordering the Orange Freeway are subject to loud noise levels. Properties directly adjacent to the freeway can experience decibels as high as 70 to 75 dB(A). Sound attenuation walls, landscaped buffers, and dirt mounds all help to reduce the sound intensity of the freeway. Figure PS-7 illustrates the projected noise contours along the freeway.

Commercial trucks associated with the Olinda Alpha landfill contribute to excessive noise levels via the Orange Freeway, Imperial Highway, and Valencia Avenue to connect with the landfill. Commercial trucks are not permitted to access Valencia Avenue via Lambert Road, minimizing some of the noise impacts to adjacent residential neighborhoods. Olinda Alpha is slated to close in the year 2013; however, the County of Orange is investigating extending the life of the landfill much further.

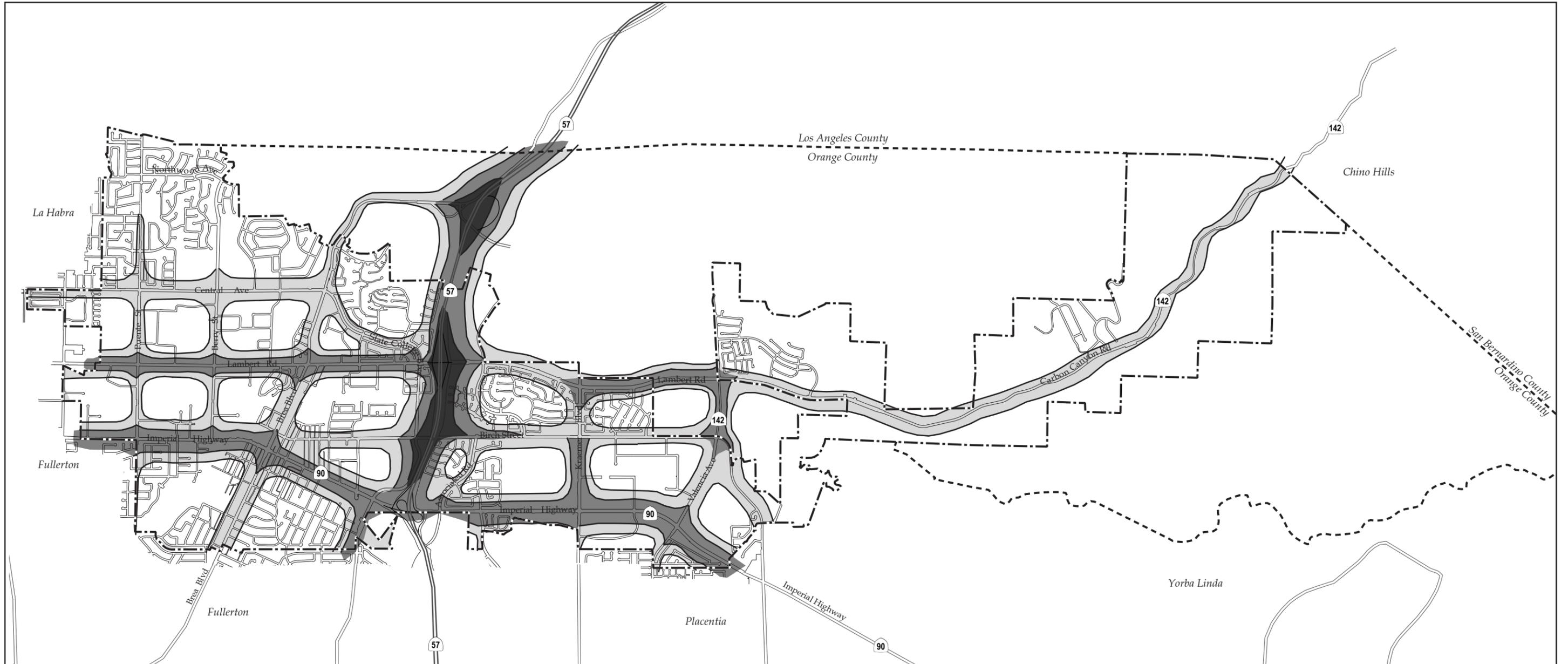
New residential development has been recently constructed at the northeast corner of Valencia Avenue and Carbon Canyon Road just south of the Olinda Alpha landfill entrance. Dense planting of deciduous and evergreen trees as well as a sound attenuation wall were set within the landscaped area to minimize the impacts to residential properties directly abutting Valencia Avenue.

Proposed residential land uses have also been designated for both sides of Valencia Avenue south of Lambert Road/Carbon Canyon Road. Residential, school, and park uses are proposed for the Hartley Research Center. Careful planning and mitigation within these areas will protect these sensitive uses from excessive noise levels.

Noise levels along Brea Boulevard, Brea Canyon Road, Central Avenue, and Lambert Road are above average and can impact sensitive uses adjacent to these streets.

Traffic noise depends primarily on the speed of traffic and the percentage of truck traffic. The primary source of noise from automobiles is high-frequency tire noise, which increases with speed. In addition, trucks and older automobiles produce engine and exhaust noise.

The most efficient and effective means of controlling noise from transportation systems is to reduce noise at the source. However, the City has little direct control over noise produced by transportation sources because State noise regulations preempt local regulations. Because the City cannot control noise at the source, City noise programs focus on reducing the impact of transportation noise at reception sites. During the planning stages of the development process, potential impacts from transportation noise will be identified and mitigation measures will be required as needed to meet City noise standards.



Source: Weiland Associates, 2001. □

**Legend**

- City Boundary
- ... Sphere of Influence

**Community Noise Equivalent Level (CNEL) Contours**

- Less than 60 CNEL
- 60 to 65 CNEL
- 65 to 70 CNEL
- Greater than 70 CNEL

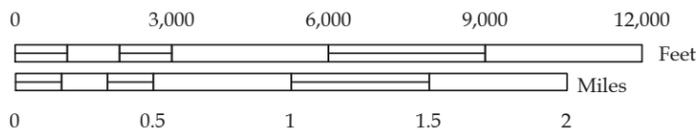
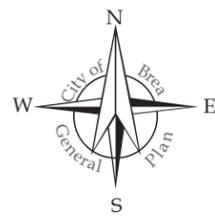


Figure PS-6  
Future Noise Contours





*Sound attenuation walls and limits on speed help to shield residential neighborhoods from automobile noise on Imperial Highway.*

## Non-Transportation Related Noise

When reviewing a proposed industrial, commercial, or public project, noise generation and potential impacts to surrounding development are considered in accordance with CEQA. Specific attention should be given to non-residential proposals adjacent to residential neighborhoods. An acoustical analysis is required for projects that will generate noise potentially affecting sensitive receptors. Where significant impacts are identified, mitigation measures are required. Common mitigation measures that could be applied when reviewing projects include acoustically treated and quiet design: 1) furnaces, 2) fans, 3) motors, 4) compressors, and 5) valves and pumps. The City may also require limited delivery hours and hours of operation to minimize impacts on adjacent residential users or other sensitive receptors.

## Goals and Policies

Certain areas of Brea are subject to high levels of noise. This in turn can reduce the quality of life within these neighborhoods. Consideration of noise sources in the planning process and identification of who that noise impacts is an effective method of minimizing the impacts of noise on residents. Areas already impacted by noise can explore different noise attenuation and rehabilitative improvements.

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**Goal PS-9     Minimize the impact of point source noise and ambient noise levels throughout the community.**

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- Policy PS-9.1     Evaluate the need to require acoustical studies for development proposals that address both direct and indirect, particularly traffic, noise impacts, and require such studies, with appropriate mitigation included, as warranted.
- Policy PS-9.2     Ensure that the noise standards set forth in the Municipal Code reflect standards most appropriate for Brea.
- Policy PS-9.3     Ensure that acceptable noise levels are maintained near schools, hospitals, convalescent homes, and other noise sensitive areas in accordance with the City's Municipal Code and noise standards contained in the General Plan.
- Policy PS-9.4     Employ creative methods of reducing noise pollution in the City.
- Policy PS-9.5     Avoid placing high-noise activity centers near residential areas.

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**Goal PS-2     Minimize the impacts of transportation-related noise.**

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- Policy PS-2.1     Reduce transportation noise by imposing traffic restrictions where necessary.
- Policy PS-2.2     Work with the counties of Orange and Los Angeles to include noise mitigation measures in the design of new roadway projects.

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**Goal PS-3     Minimize noise impacts from sources other than transportation.**

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- Policy PS-3.1     Require the inclusion of noise mitigation measures, techniques, and design features in the planning, design, and construction of future development and redevelopment projects.

Policy PS-3.2 Require that mixed-use structures be designed to prevent transfer of noise and vibration from commercial/retail to residential use.

Policy PS-3.3 Minimize stationary noise sources and noise emanating from construction activities and special events.

Policy PS-3.4 Require that new non-residential development plan delivery areas away from existing residential areas.

Policy PS-3.5 Continue active enforcement to limit commercial and industrial delivery hours adjoining residential areas.

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**Implementation  
Guide**



See Section XXIII of the Implementation Guide for action programs.

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# GLOSSARY

THE CITY OF  
BRE A

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GENERAL PLAN



# GLOSSARY



**Access** - A way of approaching or entering a property, including ingress (the right to enter) and egress (the right to leave).

**Acres, Net** - The portion of a site that can actually be built upon. The following generally are not included in the net acreage of a site: public or private road right-of-way, public open space, and floodways.

**ADT** - Average daily trips made by vehicles in a 24-hour period.

**Air Basin** - One of 14 self-contained regions in California minimally influenced by air quality in contiguous regions.

**Air Pollutant Emissions** - Discharges into the atmosphere, usually specified in terms of weight per unit of time for a given pollutant from a given source.

**Air Pollution** - The presence of contaminants in the air in concentrations that exceed naturally occurring quantities and are undesirable or harmful.

**Air Quality Standards** - The prescribed level of pollutants in the outside air that cannot be exceeded legally during a specified time in a specified geographical area.

**Alquist-Priolo Earthquake Fault Zoning Act** – California state law that mitigates the hazard of surface faulting to structures for human occupancy.

**Ambient Noise Level** - The combination of noise from all sources near and far. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location.

**Annexation** - The incorporation of a land area into an existing city with a resulting change in the boundaries of that city.

**Aquifer** - An underground bed or layer of earth, gravel, or porous stone that contains water.

**Arterial** - A major street carrying the traffic of local and collector streets to and from freeways and other major streets, with controlled intersections and generally providing direct access to nonresidential properties.

**Assisted Housing** - Generally multi-family rental housing, but sometimes single-family ownership units, whose construction, financing, sales prices, or rents have been subsidized by federal, state, or local housing programs, including, but not limited to, Federal Section 8 (new construction, substantial rehabilitation, and loan management set-asides), Federal Sections 213, 236, and 202, Federal Section 221(d)(3) (below-market interest rate program), Federal Section 101 (rent supplement assistance), CDBG, FmHA Section 515, multi-family mortgage revenue bond programs, local redevelopment and in lieu fee programs, and units developed pursuant to local inclusionary housing and density bonus programs.

**A-Weighted Decibel or dB(A)** - A numerical method of rating human judgment of loudness. The A-weighted scale reduces the effects of low and high frequencies in order to simulate human hearing.

**Base Flood Elevation** - The highest elevation, expressed in feet above sea level, of the level of flood waters expected to occur during a 100-year flood (i.e., a flood that has one percent likelihood of occurring in any given year).

**Bicycle Lane (Class II facility)** - A corridor expressly reserved for bicycles, existing on a street or roadway in addition to any lanes for use by motorized vehicles.

**Bicycle Path (Class I facility)** - A paved route not on a street or roadway and expressly reserved for bicycles traversing an otherwise unpaved area. Bicycle paths may parallel roads but typically are separated from them by landscaping.

**Bicycle Route (Class III facility)** - A facility shared with motorists and identified only by signs, a bicycle route has no pavement markings or lane stripes.

**Bikeways** - A term that encompasses bicycle lanes, bicycle paths, and bicycle routes.

**Buffer** - A strip of land designated to protect one type of land use from another incompatible use. Where a commercial district abuts a residential district, for example, additional use, yard, or height restrictions may be imposed to protect residential properties. The term may also be used to describe any zone that separates two unlike zones, such as a multi-family housing zone between single-family housing and commercial uses.

**Building** - Any structure having a roof supported by columns or walls and intended for the shelter, housing or enclosure of any individual, animal, process, equipment, goods, or materials of any kind or nature.

**California Environmental Quality Act (CEQA)** - A State law (California Public Resources Code Section 21000 et. seq.) requiring State and local agencies to regulate activities with consideration for environmental protection. If a proposed activity has the potential for a significant adverse environmental impact, an Environmental Impact Report (EIR) must be prepared and certified as to its adequacy before taking action on the proposed project. General Plans usually require the preparation of a "Program EIR."

**California Housing Finance Agency (CHFA)** - A State agency, established by the Housing and Home Finance Act of 1975, authorized to sell revenue bonds and generate funds for the development, rehabilitation, and conservation of low- and moderate-income housing.

**Caltrans** - California Department of Transportation.

**Capital Improvement Program (CIP)** - A proposed timetable or schedule of all future capital improvements (government acquisition of real property, major construction project, or acquisition of long lasting, expensive equipment) to be carried out during a specific period and listed in order of priority, together with cost estimates and the anticipated means of financing each project. Capital improvement programs are usually projected five or six years in advance and should be updated annually.

**Census** - The official decennial enumeration of the population conducted by the federal government.

**City** - City, with a capital "C," generally refers to the government or administration of a city. City, with a lower case "c" may mean any city.

**City Council** - The governing board of the City. The five-member elected council is responsible to the electorate for keeping pace with changing community needs, for establishing the quality of municipal services through the open conduct of public affairs, and for encouraging constructive citizen participation.

**Collector** - A street for traffic moving between arterial and local streets, generally providing direct access to properties.

**Community Care Facility** - Any facility, place, or building which is maintained and operated to provide non-medical residential care, day treatment, adult day care, or foster family agency services for children, adults, or children and adults, including, but not limited to, the physically handicapped, mentally impaired, incompetent persons, and abused or neglected children, and includes residential facilities, adult day care facilities, day treatment facilities, foster family homes, small family homes, social rehabilitation facilities, community treatment facilities, and social day care facilities.

**Community Development Block Grant (CDBG)** - A grant program administered by the U.S. Department of Housing and Urban Development (HUD) on a formula basis for entitlement communities, and by the State Department of Housing and Community Development (HCD) for non-entitled jurisdictions. This grant allots money to cities and counties for housing rehabilitation and community development, including public facilities and economic development.

**Community Noise Equivalent Level (CNEL)** - The average equivalent sound level during a 24-hour day, obtained after addition of five decibels to sound levels in the evening from 7 P.M. to 10 P.M. and after addition of 10 decibels to sound levels in the night after 10 P.M. and before 7 A.M. See also "A-Weighted Decibel."

**Compatibility** - The characteristics of different uses or activities that permit them to be located near each other in harmony and without conflict. The designation of permitted and conditionally permitted uses in zoning districts are intended to achieve compatibility within the district. Some elements affecting compatibility include: intensity of occupancy as measured by dwelling units per acre; pedestrian or vehicular traffic generated; volume of goods handled; and such environmental effects as noise, vibration, glare, air pollution, or the presence of hazardous materials. On the other hand, many aspects of compatibility are based on personal preference and are much harder to measure quantitatively, at least for regulatory purposes.

**Condominium** - A building, or group of buildings, in which units are owned individually, and the structure, common areas and facilities are owned by all the owners on a proportional, undivided basis.

**Congestion Management Plan (CMP)** - A mechanism employing growth management techniques, including traffic level of service requirements, development mitigation programs, transportation systems management, and capital improvement programming, for the purpose of controlling and/or reducing the cumulative regional traffic impacts of development. AB 1791, effective August 1, 1990, requires all cities, and counties that include urbanized area, to adopt and annually update a Congestion Management Plan.

**Congregate Care Housing** - generally defined as age-segregated housing built specifically for the elderly which provides services to its residents, the minimum of which is usually an on-site meal program, but which may also include housekeeping, social activities, counseling, and transportation. There is generally a minimum health requirement for acceptance into a congregate facility as most do not offer supportive health care services, thus differing from a nursing home. Residents usually have their own bedrooms and share common areas such as living rooms, dining rooms, and kitchens; bathrooms may or may not be shared.

**Conservation** - The management of natural resources to prevent waste, destruction, or neglect.

**Council of Governments (COG)** - A regional planning and review authority whose membership includes representation from all communities in the designated region. The Southern California Association of Governments (SCAG) is an example of a COG in Southern California.

**Coverage** - The proportion of the area of the footprint of a building to the area of the lot on which it stands.

**Critical Facility** - Facilities housing or serving many people which are necessary in the event of an earthquake or flood, such as hospitals, fire, police, and emergency service facilities, utility "lifeline" facilities, such as water, electricity, and gas supply, sewage disposal, and communications and transportation facilities.

**Day-Night Average Level (Ldn)** - The average equivalent sound level during a 24-hour day, obtained after addition of 10 decibels to sound levels in the night after 10 P.M. and before 7 A.M. See also "Community Noise Equivalent Level."

**Decibel** (dB) - A unit for describing the amplitude of sound, as it is heard by the human ear. See also "A-Weighted Decibel," "Community Noise Equivalent Level," and "Day-Night Average Level."

**Dedication** - The turning over by an owner or developer of private land for public use, and the acceptance of land for such use by the governmental agency having jurisdiction over the public function for which it will be used. Dedications for roads, parks, school sites, or other public uses often are made conditions for approval of a development by a city.

**Density** - The number of families, individuals, dwelling units or housing structures per unit of land; usually density is expressed "per acre." Thus, the density of a development of 100 units occupying 20 acres is 5 units per acre.

**Density Bonus** - The allocation of development rights that allow a parcel to accommodate additional square footage or additional residential units beyond the maximum for which the parcel is zoned, usually in exchange for the provision or preservation of an amenity at the same site or at another location.

**Density Transfer** - A way of retaining open space by concentrating densities, usually in compact areas adjacent to existing urbanization and utilities, while leaving unchanged historic, environmentally sensitive, or hazardous areas.

**Developer** - An individual who or business which prepares raw land for the construction of buildings or causes to be built physical building space for use primarily by others, and in which the preparation of the land or the creation of the building space is in itself a business and is not incidental to another business or activity.

**Development** - The division of a parcel of land into two or more parcels; the construction, reconstruction, conversion, structural alteration, relocation or enlargement of any structure; any mining, excavation, landfill or land disturbance, and any use or extension of the use of land.

**Development Impact Fees** - A fee or charge imposed on developers to pay for the costs to the city of providing services to a new development.

**Development Plan** - A plan, to scale, showing uses and structures proposed for a parcel or multiple parcels of land. It includes lot lines, streets, building sites, public open space, buildings, major landscape features and locations of proposed utility services.

**Development Rights** - The right to develop land by a landowner that maintains fee-simple ownership over the land or by a party other than the owner who has obtained the rights to develop. Such rights usually are expressed in terms of density allowed under existing zoning. For example, one development right may equal one unit of housing or may equal a specific number of square feet of gross floor area in one or more specified zone districts.

**Dwelling** - A structure or portion of a structure used exclusively for human habitation.

**Dwelling, Multi-family** - A building containing two or more dwelling units for the use of individual families maintaining households; an apartment or condominium building is an example of this dwelling unit type.

**Dwelling, Single-family Attached** - A one-family dwelling attached to one or more other one-family dwellings by a common vertical wall; duplexes and townhomes are examples of this dwelling unit type.

**Dwelling, Single-family Detached** - A dwelling which is designed for and occupied by not more than one family and surrounded by open space or yards and which is not attached to any other dwelling by any means.

**Dwelling Unit** - One or more rooms, designed, occupied or intended for occupancy as separate living quarters, with cooking, sleeping and sanitary facilities provided within the unit for the exclusive use of a single family maintaining a household.

**Easement** - A grant of one or more of the property rights by the property owner to and/or for use by the public, a corporation, or another person or entity.

**Economic Base** - The production, distribution, and consumption of goods and services within a planning area.

**Element** - A division of the General Plan referring to a topic area for which goals, policies, and programs are defined (e.g., land use, housing, circulation).

**Eminent Domain** - The authority of a government to take, or to authorize the taking of, with compensation, private property for public use.

**Endangered Species** - A species of animal or plant is endangered when its prospects for survival and reproduction are in immediate jeopardy from one or more causes.

**Environment** - The sum of all external conditions and influences affecting the life, development, and survival of an organism.

**Exaction** - A contribution or payment required as an authorized precondition for receiving a development permit; usually refers to mandatory dedication (or fee in lieu of dedication) requirements found in many subdivision regulations.

**Fault** - A fracture in the earth's crust forming a boundary between rock masses that have shifted.

**Fault, Active** - A fault that has moved within the last 11,000 years and that is likely to move again within the next 100 years.

**Fault, Inactive** - A fault which shows no evidence of movement in the last 11,000 years and no potential for movement in the relatively near future.

**Fault, Potentially Active** - A fault that last moved within the Quaternary Period (the last 2,000,000 to 11,000 years) before the Holocene Epoch (11,000 years to the present); or a fault that, because it is judged to be capable of ground rupture or shaking, poses an unacceptable risk for a proposed structure.

**FEMA** - Federal Emergency Management Agency.

**FHWA** - Federal Highway Administration.

**Fire Flow** - A rate of water flow that should be maintained to halt and reverse the spread of a fire.

**Flood Insurance Rate Map (FIRM)** - For each community, the official map on which the Federal Insurance Administration has delineated areas of special flood hazard and the risk premium zones applicable to that community.

**Flood, Regulatory Base** - Flood having a one percent chance of being equaled or exceeded in any given year (100-year flood).

**Floodplain** - A lowland or relatively flat area adjoining the banks of a river or stream which is subject to a one percent or greater chance of flooding in any given year (i.e., 100-year flood).

**Floodway** - The channel of a watercourse or river, and portions of the flood plain adjoining the channel, which are reasonably required to carry and discharge the base flood of the channel.

**Floor Area Ratio (FAR)** - The gross floor area of all buildings on a lot divided by the lot area; usually expressed as a numerical value (e.g., a building having 5,000 square feet of gross floor area located on a lot of 10,000 square feet in area has a floor area ratio of 0.5:1).

**General Plan** - A legal document that takes the form of a map and accompanying text adopted by the local legislative body. The plan is a compendium of policies regarding the long-term development of a jurisdiction. The state requires the preparation of seven elements or divisions as part of the plan: land use, housing, circulation, conservation, open space, noise, and safety. Additional elements pertaining to the unique needs of an agency are permitted.

**Goal** - The ultimate purpose of an effort stated in a way that is general in nature and immeasurable; a broad statement of intended direction and purpose (e.g., "Achieve a balance of land use types within the city").

**Grade** - The degree of rise or descent of a sloping surface.

**Greenbelt** - An open area which may be cultivated or maintained in a natural state surrounding development or used as a buffer between land uses or to mark the edge of an urban or developed area.

**Ground Failure** - Mudslide, landslide, liquefaction (see this Glossary), or the compaction of soils due to ground shaking from an earthquake.

**Ground Shaking** - Ground movement resulting from the transmission of seismic waves during an earthquake.

**Groundwater** - The supply of fresh water under the ground surface in an aquifer or soil that forms a natural reservoir.

**Group Quarters** - A dwelling that houses unrelated individuals.

**Growth Management** - Techniques used by government to control the rate, amount, and type of development.

**Habitat** - The physical location or type of environment in which an organism or biological population lives or occurs.

**HCD** - State Department of Housing and Community Development.

**HDC** – Non-profit Housing Development Corporation.

**HOME** - Home Investment Partnership Act.

**Hazardous Materials** - An injurious substance, including pesticides, herbicides, toxic metals and chemicals, liquefied natural gas, explosives, volatile chemicals and nuclear fuels.

**Household** - According to the U.S. Census, a household is all persons living in a dwelling unit whether or not they are related. Both a single person living in an apartment and a family living in a house are considered households.

**Household Income** - The total income of all the people living in a household. Households are usually described as very low income, low income, moderate income, and upper income for that household size, based on their position relative to the regional median income.

**Housing Affordability** - Based on state and federal standards, housing is affordable when the housing costs are no more than 30 percent of household income.

**Housing Unit** - A room or group of rooms used by one or more individuals living separately from others in the structure, with direct access to the outside or to a public hall and containing separate toilet and kitchen facilities.

**HUD** - U.S. Department of Housing and Urban Development.

**Implementation Measure** - An action, procedure, program, or technique that carries out general plan policy.

**Income Categories** - Four categories for classifying households according to income based on the median income for each County. The categories are as follows: Very Low (0-50% of County median); Low (50-80% of County median); Moderate (80-120% of County median); and Upper (over 120% of County median).

**Infrastructure** - The physical systems and services which support development and population, such as roadways, railroads, water, sewer, natural gas, electrical generation and transmission, telephone, cable television, storm drainage, and others.

**Intensity** - A measure of the amount or level of development often expressed as the ratio of building floor area to lot area (floor area ratio) for commercial, business, and industrial development, or units per acre of land for residential development (also called "density").

**Intersection** – A location where two or more roads cross at grade.

**Issue** - A problem, constraint, or opportunity requiring community action.

**Jobs/Housing Balance; Jobs/Housing Ratio** - The jobs/housing ratio divides the number of jobs in an area by the number of employed residents. A ratio of 1.0 indicates a balance. A ratio greater than 1.0 indicates a net in-commute of employed persons; less than 1.0 indicates a net out-commute of employed persons.

**Land Use** - A description of how land is occupied or used.

**Land Use Plan** - A plan showing the existing and proposed location, extent and intensity of development of land to be used in the future for varying types of residential, commercial, industrial, agricultural, recreational and other public and private purposes or combination of purposes.

**Landslide** - A general term for a falling or sliding mass of soil or rocks.

**Liquefaction** - A process by which water-saturated granular soils transform from a solid to a liquid state due to groundshaking. This phenomenon usually results from shaking from energy waves released in an earthquake.

**Local Agency Formation Commission (LAFCO)** - A five or seven-member commission within each county that reviews and evaluates all proposals for formation of special districts, incorporation of cities, annexation to special districts or cities, consolidation of districts, and merger of districts with cities. Each county's LAFCO is empowered to approve, disapprove, or conditionally approve such proposals.

**Local Street** - A street providing direct access to properties and designed to discourage through-traffic.

**Lot** - The basic unit of land development. A designated parcel or area of land established by plat, subdivision, or as otherwise permitted by law, to be used, developed or built upon as a unit.

**LOS** - Level of Service, a descriptor of traffic operating conditions based on an intersection's volume-to-capacity ratio.

**Maximum Credible Earthquake (MCE)** - The largest possible earthquake that could reasonably occur along recognized faults or within a particular seismic source.

**Median Income** - The annual income for each household size which is defined annually by the federal Department of Housing and Urban Development. Half of the households in the region have incomes above the median and half are below.

**Mitigate** - To ameliorate, alleviate, or avoid to the extent reasonably feasible.

**MGD** – Million gallons per day.

**MWD** – Metropolitan Water District of Southern California.

**MWDOC** – Municipal Water District of Orange County.

**National Flood Insurance Program (NFIP)** - The National Flood Insurance Program, managed by FEMA, makes Federally-backed flood insurance available in communities that agree to adopt and enforce floodplain management ordinances to reduce future flood damage.

**Noise** - Any undesired audible sound.

**Noise Exposure Contours** - Lines drawn about a noise source indicating constant energy levels of noise exposure. CNEL and Ldn are the metrics utilized to describe community noise exposure.

**Non-Attainment** - The condition of not achieving a desired or required level of performance. Frequently used in reference to air quality.

**Non-Conforming Use** - A use that was valid when brought into existence, but by subsequent regulation becomes no longer conforming. "Non-conforming use" is a generic term and includes (1) non-conforming structures (by virtue of size, type of construction, location on land, or proximity to other structures), (2) non-conforming use of a conforming building, (3) non-conforming use of a non-conforming building, and (4) non-conforming use of

land. Thus, any use lawfully existing on any piece of property that is inconsistent with a new or amended General Plan, and that in turn is a violation of a zoning ordinance amendment subsequently adopted in conformance with the General Plan, will be a non-conforming use.

**Oil Seep** - Natural springs where liquid hydrocarbons (mixtures of crude oil, tar, natural gas, and water) leak out of the ground.

**Open Space** - Any parcel or area of land or water essentially unimproved and set aside, designated, dedicated, or reserved for public or private use or enjoyment.

**OC** – Orange County.

**OCFCD** – Orange County Flood Control District.

**Ordinance** - A law or regulation set forth and adopted by a governmental authority, usually a city or county.

**Overcrowding** - As defined by the State Department of Housing and Community Development, a household with greater than one person per room, excluding bathrooms, kitchens, hallways, and porches.

**Parcel** - A lot or tract of land.

**Planning Area** - The Planning Area is the land area addressed by the General Plan. Typically, the Planning Area boundary coincides with the Sphere of Influence which encompasses land both within the City limits and potentially annexable land.

**Planning Commission** - A group of people appointed by the city council that administer planning and land use regulations for the city and provide recommendations on a wide array of land use and land use policy issues.

**Policy** - Statements guiding action and implying clear commitment found within each element of the general plan (e.g., "Provide incentives to assist in the development of affordable housing").

**Pollution** - The presence of matter or energy whose nature, location, or quantity produces undesired environmental effects.

**Program** - A coordinated set of specific measures and actions (e.g., zoning, subdivision procedures, and capital expenditures) the local government intends to use in carrying out the policies of the general plan.

**Recreation, Active** - A type of recreation or activity which requires the use of organized play areas, including, but not limited to: softball, baseball, football and soccer fields, tennis and basketball courts and various forms of children's play equipment.

**Recreation, Passive** - Type of recreation or activity which does not require the use of organized play areas.

**Redevelopment** - Redevelopment, under the California Community Redevelopment Law, is a process with the authority, scope, and financing mechanisms necessary to provide stimulus to reverse current negative business trends, remedy blight, provide job development incentives, and create a new image for a community. It provides for the planning, development, redesign, clearance, reconstruction, or rehabilitation, or any combination of these, and the provision of public and private improvements as may be appropriate or necessary in the interest of the general welfare. In a more general sense, redevelopment is a process in which existing development and use of land is replaced with new development and/or use.

**Regional** - Pertaining to activities or economies at a scale greater than that of a single jurisdiction, and affecting a broad homogeneous area.

**Regional Housing Needs Assessment (RHNA)** - The Regional Housing Needs Assessment (RHNA) is based on State of California projections of population growth and housing unit demand and assigns a share of the region's future housing need to each jurisdiction within the SCAG (Southern California Association of Governments) region. These housing need numbers serve as the basis for the update of the Housing Element in each California city and county.

**Rehabilitation** - The upgrading of a building previously in a dilapidated or substandard condition, for human habitation or use.

**Restoration** - The replication or reconstruction of a building's original architectural features, usually describing the technique of preserving historic buildings.

**Rezoning** - An amendment to the map and/or text of a zoning ordinance to effect a change in the nature, density, or intensity of uses allowed in a zoning district and/or on a designated parcel or land area.

**Right-of-Way** - A strip of land acquired by reservation, dedication, prescription or condemnation and intended to be occupied by a road, crosswalk, railroad, electric transmission lines, oil or gas pipeline, water line, sanitary or storm sewer, or other similar uses.

**Risk** - The danger or degree of hazard or potential loss.

**Seiche** - An earthquake-generated wave in an enclosed body of water such as a lake, reservoir, or bay.

**Seismic** - Caused by or subject to earthquakes or earth vibrations.

**Sensitive Species** - Includes those plant and animal species considered threatened or endangered by the U.S. Fish and Wildlife Service and/or the California Department of Fish and Game according to Section 3 of the Federal Endangered Species Act. Endangered - any species in danger of extinction throughout all, or a significant portion of, its range. Threatened - a species likely to become an endangered species within the foreseeable future throughout all, or a portion of, its range. These species are periodically listed in the Federal Register and are, therefore, referred to as "federally listed" species.

**Sewer** - Any pipe or conduit used to collect and carry away sewage from the generating source to a treatment plant.

**Significant Effect** - A beneficial or detrimental impact on the environment. May include, but is not limited to, significant changes in an area's air, water, and land resources.

**Site** - A parcel of land used or intended for one use or a group of uses and having frontage on a public or an approved private street. A lot.

**Site Plan** - The development plan for one or more lots on which is shown the existing and proposed conditions of the lot including: topography, vegetation, drainage, floodplains, marshes and waterways; open spaces, walkways, means of ingress and egress, utility services, landscaping, structures and signs, lighting, and screening devices; any other information that reasonably may be required in order that an informed decision can be made by the approving authority.

**Slope** - Land gradient described as the vertical rise divided by the horizontal run, and expressed in percent.

**Solid Waste** - Unwanted or discarded material, including garbage with insufficient liquid content to be free flowing, generally disposed of in landfills or incinerated.

**Southern California Association of Governments (SCAG)** - The Southern California Association of Governments is a regional planning agency which encompasses six counties: Imperial, Riverside, San Bernardino, Orange, Los Angeles, and Ventura. SCAG is responsible for preparation of the Regional Housing Needs Assessment (RHNA).

**SCWC** – Southern California Water Company.

**Special District** - A district created by act, petition or vote of the residents for a specific purpose with the power to levy taxes.

**Special Needs Groups** - Those segments of the population which have a more difficult time finding decent affordable housing due to special circumstances. Under State planning law, these special needs groups consist of the elderly, handicapped, large families, female-headed households, farmworkers and the homeless.

**Specific Plan** - Under Article 8 of the Government Code (Section 65450 et seq), a legal tool for detailed design and implementation of a defined portion of the area covered by a General Plan. A specific plan may include all detailed regulations, conditions, programs, and/or proposed legislation which may be necessary or convenient for the systematic implementation of any General Plan element(s).

**Sphere of Influence** - The probable ultimate physical boundaries and service area of a local agency (city or district) as determined by the Local Agency Formation Commission (LAFCO) of the County.

**Standards** - (1) A rule or measure establishing a level of quality or quantity that must be complied with or satisfied. The State Government Code (Section 65302) requires that General Plans describe the objectives, principles, "standards," and proposals of the General Plan. Examples of standards might include the number of acres of park land per 1,000 population that the community will attempt to acquire and improve. (2) Requirements in a zoning ordinance that govern building and development as distinguished from use restrictions; for example, site-design regulations such as lot area, height limit, frontage, landscaping, and floor area ratio.

**Stationary Source** - A non-mobile emitter of pollution.

**Structure** - Anything constructed or erected which requires location on the ground (excluding swimming pools, fences, and walls used as fences).

**Subdivision** - The division of a lot, tract or parcel of land that is the subject of an application for subdivision.

**Subdivision Map Act** - Division 2 (Sections 66410 et seq) of the California Government Code, this act vests in local legislative bodies the regulation and control of the design and improvement of subdivisions, including the requirement for tentative and final maps. (See "Subdivision.")

**Subsidence** - The sudden sinking or gradual downward settling and compaction of soil and other surface material with little or no horizontal motion. Subsidence may be caused by a variety of human and natural activities, including earthquakes.

**Subsidize** - To assist by payment of a sum of money or by the granting of terms or favors that reduce the need for monetary expenditures. Housing subsidies may take the forms of mortgage interest deductions or tax credits from federal and/or state income taxes, sale or lease at less than market value of land to be used for the construction of housing, payments to supplement a minimum affordable rent, and the like.

**Topography** - Configuration of a surface, including its relief and the position of natural and man-made features.

**Transportation Demand Management (TDM)** - A strategy for reducing demand on the road system by reducing the number of vehicles using the roadways and/or increasing the number of persons per vehicle. TDM attempts to reduce the number of persons who drive alone on the roadway during the commute period and to increase the number in carpools, vanpools, buses and trains, walking, and biking. TDM can be an element of TSM (see below).

**Transportation Systems Management (TSM)** - Individual actions or comprehensive plans to reduce the number of vehicular trips generated by or attracted to new or existing development. TSM measures attempt to reduce the number of vehicle trips by increasing bicycle or pedestrian trips or by expanding the use of bus, transit, carpool, vanpool, or other high occupancy vehicles.

**Trip** - A one-way journey that proceeds from an origin to a destination via a single mode of transportation; the smallest unit of movement considered in transportation studies. Each trip has one “production end,” (or origin – often from home, but not always), and one “attraction end,” (destination).

**Uniform Building Code (UBC)** - A standard building code which sets forth minimum standards for construction.

**Water Course** - Any natural or artificial stream, river, creek, ditch, channel, canal, conduit, culvert, drain, waterway, gully, ravine or wash in which water flows in a definite channel, bed and banks, and includes any area adjacent thereto subject to inundation by reason of overflow or flood water.

**Wetland** - An area that is inundated or saturated by surface water or groundwater at a frequency and duration sufficient to support a prevalence of vegetation typically adapted for life in saturated soil conditions, commonly known as hydrophytic vegetation.

**Zoning** - A police power measure, enacted primarily by units of local government, in which the community is divided into districts or zones within which permitted and special uses are established as are regulations governing lot size, building bulk, placement, and other development standards. Requirements vary from district to district, but they must be uniform within the same district. The zoning ordinance consists of a map and text.

**Zoning District** - A geographical area of a city zoned with uniform regulations and requirements.

**Zoning Map** - The officially adopted zoning map of the city specifying the uses permitted within certain geographic areas of the city.

Appendix A

# IMPLEMENTATION GUIDE

THE CITY OF  
BRE A

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GENERAL PLAN



# Appendix A

# IMPLEMENTATION

# GUIDE



**T**he General Plan Implementation Guide provides a guide to implement adopted General Plan policies and plans for City elected officials, staff and the public. The purpose of the Implementation Guide is to ensure the overall direction provided in the General Plan for City growth and development is translated from general terms to specific actions.

Each implementation measure is a program, procedure, or technique that requires additional City action. This action may either occur on a City-wide basis, or in individual subareas. Some of the implementation measures are processes or procedures the City currently administers on a day-to-day basis (such as development project review), while others identify new programs or projects that will become day-to-day planning activities in Brea. The City Council, by relating the Implementation Guide to the General Plan, recognizes the importance of long-range planning considerations and budgeting to such day-to-day activities. Implementation of the specific programs will be subject to funding constraints.

The Implementation Guide is organized into five subsections that correspond to the General Plan Chapters. Each of the subsections is comprised of programs that directly relate to the policies and plans of the corresponding General Plan element.

## Use of the General Plan Implementation Guide

The Implementation Guide is intended for use as the basis for preparing the Annual Report to the City Council on the status of the City's progress in implementing the General Plan, as described in Section 65400 of the California Government Code. Because many of the individual actions and programs described in the Implementation Guide act as mitigation for significant environmental impacts resulting from planned development identified in the General Plan, the annual report can also provide a means of monitoring the application of the mitigation measures as

required by AB 3180. This Implementation Guide should be updated annually with the budget process and whenever the City's General Plan is amended or updated to ensure continued consistency and usefulness.

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
<b>SECTION I: LAND USE</b>									
<b>Citywide</b>									
CD-1.1 CD-1.2 CD-1.4 CD-1.5 CD-1.9 CD-1.11 CD-4.4 CD-6.6	Revise the zoning ordinance and map to reflect the adopted Land Use Policy Map, and implement the updated ordinance and map over time.  Use redevelopment set-aside funds and other funding mechanisms to assist with the development of work force housing.	Development Services  Economic Development	General fund  Redevelopment	By the end of 2004; Ongoing					
CD-1.3 CD-1.6	Structure zoning ordinance use regulations to allow a broad range of commercial and industrial businesses.  Create partnerships with the Chamber of Commerce and other local organizations to attract new businesses to the community.	Economic Development	General fund	Ongoing					
CD-1.7 CD-1.8	As part of a development impact fee program, collect funds for the acquisition and improvement of trails shown on Figure CR-2 of the Community Resources Chapter.  Require developers to provide the open space linkages and trails shown on Figure CR-2 of the Community Resources Chapter.	Development Services	Impact fees	Establish program by end of 2005; Ongoing					
CD-1.10	Revise zoning regulations for residential zones to ensure maximum preservation of open space resources.  Cooperate with conservation agencies in their efforts to acquire open space. Cooperation may include partnering City funds with private sources for strategic open space acquisitions.	Development Services	General fund  Mitigation payments	Revise ordinance by end of 2004  Ongoing					
CD-1.13	Conduct a study to determine whether existing residential zoning regulations adequately address the suitability of additions to existing residential units, the bulk of residential development, particularly as it pertains to established neighborhoods. If the study points toward a need to revise standards, revise standards accordingly.	Development Services	General fund	By the end of 2004					
CD-1.12	Continue the use of the City's housing rehabilitation loan program and NEIGHBORHOOD ENHANCEMENT PROGRAM.	Economic Development	Redevelopment Funds; Community Development Block Grants (CDBG) funds	Ongoing					
CD-1.14	Authorize funds annually and identify necessary staff resources to allow continued implementation of the Neighborhood Preservation Ordinance.	Development Services	General Fund  Redevelopment Funds	Annually					
<b>Focus Areas: Northwest Neighborhoods</b>									
CD-2.1	Develop and adopt design guidelines for residential development.	Development Services	General fund	By the end of 2005					
CD-2.2	For all new residential development proposals, require that street and/or pedestrian connections are provided between existing development that abuts the new development and that new development.	Development Services	Application fees	Ongoing					
CD-2.3 CD-4.3	Study the recommendations of the <i>Street Design and Traffic Calming Recommendations for Neighborhoods 4 and 6 in the City of Brea, CA</i> study, and prioritize recommended improvements.  Identify where such traffic calming devices could be effective in reducing speeds on other residential streets.	Development Services  Economic Development	General fund; CIP funds  Redevelopment funds	Complete analysis by 2006; implement improvements as funds available					
CD-2.4	Continue to implement City ordinances regulating street trees and landscaping.	Development Services  Maintenance	General fund	Ongoing					

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CD-2.5	Utilize grant and loan programs to assist small businesses with façade enhancements.	Economic Development	Redevelopment funds; Community Development Block Grants (CDBG) funds	Ongoing					
CD-3.1 CD-3.2 CD-3.6	Enhance and emphasize Puente and Berry Streets as primary pedestrian routes to Central Avenue by enhanced landscaping and improved pedestrian safety.  Investigate the feasibility of establishing daily, regularly scheduled transit service throughout the Northwest neighborhood and other areas of the City with concentrations of older residents and others that rely heavily on public transit.	Development Services  Development Services, OCTA	Capital Improvement Program (CIP) funds  OCTA Federal grants	Ongoing;  Investigate by end of 2005, implement as needed					
CD-3.2	Investigate the feasibility of establishing daily, regularly scheduled transit service throughout the Northwest neighborhood.  With redevelop of commercial sites within the area, consider establishing a senior center as part of any mixed-use development.	Development Services, OCTA  Economic Development Department	OCTA Federal grants  Redevelopment funds	Investigate by end of 2005, implement as needed;  As development is proposed					
CD-3.4	Work with Chamber of Commerce to help encourage local retail businesses to market and serve residents in the Northwest area.	Economic Development, Chamber of Commerce	General Fund	Ongoing					
CD-3.5 CD-4.2	Extend the Downtown directional signage program to Central Avenue.  Establish streetscape/landscape design plans for Central Avenue and Puente and Berry Streets consistent with the Public Realm Urban Design Palette in the Community Development Chapter.	Development Services	Redevelopment funds  Capital Improvement Program (CIP) funds	By 2004;  By 2006					
<b>Focus Areas: Downtown Brea</b>									
CD-4.1	Use the City's Business and Attraction Program to help diversify the mix of Downtown businesses by attracting new businesses and expanding existing businesses.	Economic Development	General fund	Ongoing	CD-21.1 CD-21.3 CD-21.6 CD-22.1 CD-22.2 CD-22.4 CD-22.5 CD-23.1				
CD-4.4	Revise zoning regulations to be consistent with the Land Use Plan of the General Plan, which plans for mixed-use development along the Brea Boulevard and Birch Street corridors.	Development Services	General fund	By the end of 2004					
CD-4.5 CD-4.6	Ensure Birch Street Streetscape Improvements include signage and landscape links between Downtown and Civic Center/Brea Mall area, as well as public plaza areas.	Development Services	Capital Improvement Program (CIP) funds	By 2004					
CD-4.7	Work with Cultural Arts Commission and Recreation Services to promote Brea's heritage through artwork, signs, preservation, recreation programs, events, and historical structures throughout the City and in City park lands.  Use Federal, state, non-profit, and private programs and resources to promote Brea's historic resources.	Cultural Arts Commission, Recreation Services	General fund	Ongoing	CD-5.6		CR-14.7 CR-14.8 CR-14.9 CR-15.3 CR-15.5 CR-15.6	CS-5.1 CS-5.2 CS-5.4 CS-5.6	
CD-4.8	Conduct a study to determine whether a trolley system that serves Downtown Brea and surrounding areas is feasible.								

**Community Development**  
Breia General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
<b>Focus Areas: Historic Brea</b>									
CD-5.1 CD-5.2	Establish streetscape/landscape design plans for South Brea Boulevard and Historic Brea that complement the existing historic environment and resources. Ensure plans are consistent with the Public Realm Urban Design Palette in the Community Development Chapter.	Economic Development	Capital Improvement Program (CIP) funds	By 2006					
CD-5.3	Revise the zoning ordinance and map to reflect the adopted Land Use Policy Map, and implement the updated ordinance and map over time.  Continue to implement the Neighborhood Enhancement Plan allowing for various rehabilitation programs.	Community Services, Economic Development							
CD-5.4 CD-5.6 CD-6.1 CD-6.5	Update and amend the Brea Towne Plaza Specific Plan to include updated standards on design guidelines for commercial development for South Brea Boulevard that respect and complement the historic character of surrounding neighborhoods.								
CD-6.2	Encourage ownership housing types such as condominiums and townhouses within in the Mixed Use III area in South Brea Boulevard.								
CD-5.5 CD-5.7	Investigate the potential of creating historic districts, conservation districts, and/or preservation overlay zones within the City. Amend the Historic Preservation Ordinance in developing criteria for district designation and include appropriate regulations to safeguard historic resources within the delineated district.	Development Services	General fund,	Investigate by end of 2005			CR-14.2		
<b>Focus Areas: Carbon Canyon and Olinda Village</b>									
CD-7.1 CD-7.3 CD-7.4 CD-8.2 CD-8.3 CD-8.7	Update and continue to implement the Brea Hillside Management Ordinance.  Implement the Hillside Development Policy within the Land Use Section of the General Plan.	Development Services, Fire and Police Services	Development fee, Environmental impact mitigation	By the end of 2004;  Ongoing					
CD-7.2	Require development proposals, particularly in sphere of influence and hillside areas to preserve, restore, and enhance existing wildlife corridors, habitat, and roadway crossings.	Development Services	Development fee, Environmental impact mitigation	Ongoing			CR-10.3 CR-10.5		
CD-7.5	Work with the Economic Development to assist in attracting local-serving businesses in Olinda Village and ensure the businesses are consistent with the Neighborhood Commercial designation of the Land Use Policy Map.	Development Services, Economic Development	General fund	Ongoing	CD-21.1 CD-21.3 CD-21.6 CD-22.1 CD-22.2 CD-22.4 CD-22.5 CD-23.1				
CD-7.7 CD-8.1	Investigate creative methods or programs aimed at acquiring and/or purchasing open space lands such as: <ul style="list-style-type: none"> <li>▪ Conservation easements;</li> <li>▪ Purchase of development rights;</li> <li>▪ Transfer of development rights;</li> <li>▪ Mitigation and Land banking;</li> <li>▪ Development rights agreement; and</li> <li>▪ Open space preservation fund;</li> </ul>	Development Services,	Development fees, Bonds, Special assessment districts, Grants, Open space conservation funds,	By the end of 2004	CD-1.7 CD-1.9 CD-1.10 CD-6.2 CD-7.1 CD-7.5 CD-8.6		CR-4.1 CR-5.1 CR-8.1 CR-8.3 CR-9.1 CR-9.2 CR-9.3 CR-9.5 CR-10.6		
CD-7.8	Investigate alternatives to the Carbon Canyon Specific Plan for regulating land use within this area. Determine whether the Plan is feasible to implement with respect to biological, infrastructure, circulation, and topographical constraints.	Development Services,	General fund	By the end of 2003					
<b>Hillsides and Unincorporated Brea</b>									
CD-8.4	Work with Integrated Waste Management Department of Orange County (IRWD) to ensure the proposed end use of the landfill is a county regional park.								

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CD-8.5	Collaborate with Federal, state, regional, local, non-profit, environmental and conservation organizations, and private entities to conserve, protect, and enhance open spaces and natural resources throughout Brea's Planning Area.	Development Services	General fund	Ongoing	CD-1.7		CR-4.3 CR-5.3		
CD-8.6	Review development proposals to ensure projects are integrated into City's circulation system.	Development Services	Development fees	Ongoing					
CD-8.8	Work closely with the County of Orange to emphasize the City's need to participate in development review process of projects within Brea's sphere of influence and surrounding unincorporated	Development Services	General fund	Ongoing					
<b>Southeast Brea</b>									
CD-9.1 CD-9.4	Work with the Economic Development to assist in attracting new commercial businesses in Southeast Brea that complement rather than compete with businesses along Imperial Highway and Downtown. Also assist in attracting an established quality, educational institution in this area such as a community college or a unique small-scale school such as an art college.	Development Services, Economic Development	General fund	Ongoing					
CD-9.2	Require development proposals to include improvements for an urban and walkway trail system including, but not limited to access points, landscaping, proper lighting, intersection improvement, decorated accent paving materials, and other pedestrian improvements. Ensure easy linkages to important activity areas such as schools, commercial areas, cultural and social centers and other high pedestrian activity areas.	Development Services, Recreation Services	Development fees	Ongoing	CD-2.2 CD-2.3 CD-2.4 CD-2.5 CD-6.2		CR-7.1 CR-7.2 CR-7.5		PS-2.2 PS-2.3 PS-3.1 PS-3.2 PS-3.3
CD-9.3 CD-9.6	Assess the existing park and recreation system according to the standards established in the Parks and Open Space Section. Require dedication of park facilities, a fee in lieu thereof, or a combination of both, as a condition of new development pursuant to the Quimby Act. Also encourage the development of parks and recreational facilities by the private sector, but available for use by the public. Annually review park facility dedication and development fee requirements to ensure they reflect changing needs of the community, population growth, and current land and construction costs.	Recreation Services, Development Services	Development fees, General fund, Park dedication	Annually	CD-1.7 CD-1.8		CR-6.2 CR-6.3 CR-6.4 CR-6.6 CR-6.7	CS-2.1 CS-2.2 CS-2.3 CS-2.4	PS-2.3 PS-3.1 PS-3.2 PS-3.3
CD-9.5	See Housing Element								
CD-9.7	Have the Community Services Department to explore different cultural art options for the reuse of the Unocal center theatre.	Community Services	General fund	By the end of 2005 or after development of the Unocal Research Center					
<b>SECTION II: CIRCULATION</b>									
<b>Regional Transportation Facilities</b>									
CD-10.1 CD-10.2 CD-10.3 CD-10.4	Coordinate planned development in the planning area with needed improvements to the regional circulation system by:  1) Supporting improvement of all regional highways and freeways in the area with Caltrans; 2) Cooperate with surrounding jurisdictions to ensure efficient operation of the arterial network; and 3) Coordinating with Orange County Transit Authority (OCTA) regarding the planned regional network improvements and transit services.	Community Development, OCTA, Caltrans	General fund	Ongoing	CD-26.1				

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CD-10.4 CD-10.6	Create a special study area for Carbon Canyon Road to analyze local and regional impacts and alternatives to improve congestion on this roadway.	Community Development	General fund	Complete 2005					
<b>Local Circulation System</b>									
CD-10.5	<p>Work closely with OCTA to amend the Master Plan of Arterial Highways as follows:</p> <ul style="list-style-type: none"> <li>▪ <b>Reclassify Puente Avenue and Whittier Boulevard as a Collector Arterial.</b> This modification accurately represents the function of this roadway.</li> <li>▪ <b>Reclassify Brea Canyon Boulevard (north of Lambert Road) as a Collector.</b> The Collector classification best reflects the function Brea Canyon Boulevard serves during most travel hours. Only during the evening commute does the roadway experience heavy use, typically as an alternative to the crowded SR-57 freeway. The City will preserve right-of-way adequate for a Minor Arterial, but the Collector designation indicates the anticipated roadway function.</li> <li>▪ <b>Reclassify South Brea Boulevard (south of Imperial Highway) as Primary Arterial.</b> This modification preserves right-of-way consistent with a Major Arterial standard but indicates the City's intention to maintain no more than two travel lanes in each direction. The land use plan provides for South Brea Boulevard to be a mixed-use, pedestrian-friendly district. The wide right-of-way will provide space for on-street parking, pedestrian enhancements, ample landscaping, and street-oriented development.</li> <li>▪ <b>Eliminate Tonner/Valencia Avenue (north of Lambert Road) as Proposed Primary Arterial.</b> Land use policy north of Lambert Road does not support the MPAH alignment, nor does planned densities require a roadway of this size.</li> </ul>	Community Development , OCTA	General fund	Complete MPAH by 2004					
CD-11.1 CD-11.2 CD-11.4 CD-11.7 CD-6.3	Implement the circulation system in the planning area in concert with land development to ensure adequate levels of service and monitor the operation of major streets. Future roadways must meet roadway classification specifications and performance criteria. As traffic approaches the Level of Service standards established in the Circulation Section, roadway capacity will be improved by adding through and turn lanes and other transportation measures according to the Arterial Highway Plan.	Community Development	General fund, Measure M revenue, Development fees, Other State and Federal sources, Gas tax revenue	Ongoing					
CD-11.1 CD-13.1	Establish a capital improvement program to address phasing and construction of traffic improvements and capital facilities throughout the planning area. Utilize the Capital Improvement Program (CIP) process to finance and complete the roadway and bikeway improvements specified in the Circulation Section. Update the Capital Improvement Program annually to respond to changes in local priorities and available funding sources.	Community Development	Capital Improvement Program (CIP) funds, Gas tax revenues, Federal Transportation funds	Ongoing					
CD-11.5 CD-11.9 CD-6.4	Revise and/or adopt street design standards, focused on pedestrian and bicycle safety, landscaping, traffic calming, and neighborhood character. If requested by the OCTA, consider requiring (and require developers to provide) bus loading areas or turnouts for buses.	Community Development, OCTA	General fund	Ongoing					

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies					
					Community Development	Housing	Community Resources	Community Services	Public Safety	
CD-11.6	To reduce expenditures, improve design, and minimize traffic disruption, coordinate local street improvements with major transportation system improvement projects, such as widening the State Route 57 on and off ramps at Lambert Road.	Community Development, Caltrans	Capital Improvement Program (CIP) funds, Development fees, Gas tax revenue	Ongoing	CD-12.1					
CD-11.10	Coordinate with the school districts and other entities to develop "Suggested Route to School Plans" for all public and private schools in the City and for schools serving students living in Brea. Plans shall identify all pedestrians and bicycle facilities, and traffic control devices for residents to determine the most appropriate travel route. The plans shall also identify existing easements for sidewalks.	Community Development, Brea Olinda School District, La Habra City School District	General fund, Grants	Ongoing						
CD-11.10	Increase priority of pedestrian safety projects (i.e., pedestrian street crossings, sidewalks, or pathways) as part of the Capital Improvement Program. Review the need to install sidewalks or paths and crosswalks on all City streets within one-half mile of all public schools within the City.	Community Development	Capital Improvement Program (CIP) funds	Ongoing					PS-3.1 PS-3.2 PS-3.3	
<b>Public Transportation System</b>										
CD-12.1 CD-12.2 CD-12.3 CD-12.4	Work with OCTA to improve transit service and encourage ridership through the following actions: <ul style="list-style-type: none"> <li>▪ Encourage provision of transit facilities in major new development and major rehabilitation projects;</li> <li>▪ Support OCTA demand-responsive (ACCESS) service and other paratransit operations ;</li> <li>▪ Work with OCTA to provide information to the public on available alternative transportation choices and routes; and</li> <li>▪ Incorporate design features into public improvement projects that promote and support the use of public transportation.</li> </ul>	Community Development, OCTA	General fund, Development fees	Ongoing			CR-6.3 CR-7.1 CR-7.5			
CD-12.3	Encourage local businesses to provide employees with transit passes or other financial incentives to use transit to commute to and from the workplace.	Community Development	General fund, Private investment	Ongoing						
CD-12.5	Identify bicycle and pedestrian projects in the CIP and through development fees that help to complete or enhance connections to bus stops.	Community Development	General fund, Development fees	Ongoing			CR-6.3 CR-7.1 CR-7.5			
CD-12.6	Work with OCTA and/or require development projects to provide transit stop enhancements including benches, shelters, schedule information, and real-time bus location data. Stop locations should be located near building entrances to encourage bus ridership.	Community Development, OCTA	General fund, Development fees	Ongoing						
<b>Bicycles and Pedestrians</b>										
CD-13.1 CD-13.3 CD-13.4	Implement the Circulation Plan to ensure Development of bicycles facilities and amenities as follows: <ol style="list-style-type: none"> <li>1) Upgrade existing bikeways and develop new bicycle facilities in accordance with the standards and locations in the Circulation Section;</li> <li>2) Require the provision of secure bicycle parking (e.g., racks, lockers) as part of all future development projects for non-single family residential development;</li> <li>3) Encourage non-residential development projects to include amenities such as showers and lockers for employees to further bicycling as an alternative to automobile travel; and</li> <li>4) Prohibit motorized vehicular traffic on trails, pathways, parks, and dedicated open space areas except for maintenance and emergency purposes.</li> </ol>	Community Development	Capital Improvement Program (CIP) funds, Development fees	Ongoing			CR-6.4 CR-7.5			

**Community Development**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies					
					Community Development	Housing	Community Resources	Community Services	Public Safety	
CD-13.2	Require development proposals to include improvements for an urban and walkway trail system including, but not limited to access points, landscaping, proper lighting, intersection improvement, decorated accent paving materials, and other pedestrian improvements. Ensure easy linkages to important activity areas such as schools, commercial areas, cultural and social centers and other high pedestrian activity areas.	Development Services, Recreation Services	Development fees	Ongoing	CD-2.2 CD-2.3 CD-2.4 CD-2.5 CD-6.2					PS-2.2 PS-2.3 PS-3.1 PS-3.2 PS-3.3
CD-13.1	Develop a community awareness program to encourage local use of bicycles paths, lanes, and routes including posting maps on the City's website. Include bicycle and pedestrian safety and enforcement when developing community awareness programs.	Community Development	General fund	Ongoing			CR-6.4 CR-7.5			PS-3.1 PS-3.2 PS-3.3
CD-13.4	Make bikeway improvements a funding priority by: <ul style="list-style-type: none"> <li>1) Continuing to consider financing bikeway design and construction as part of the City's annual construction and improvement fund;</li> <li>2) Incorporating bikeway improvements as part of the Capital Improvement Program; and</li> <li>3) Aggressively pursue regional funding and other Federal and State sources for new bikeways.</li> </ul>	Development Services	Capital Improvement Program (CIP) funds, Federal and State funding sources	Ongoing			CR-6.4 CR-7.5			
<b>SECTION III: INFRASTRUCTURE</b>										
CD-14.1 CD-14.2 CD-14.4	Review development proposals for consistency water, power and solid waste disposal infrastructure requirements.	Development Services, Maintenance Services	General Fund, development fees	Ongoing						
CD-14.2	Implement the City's Water Master Plan recommendations for replacement, maintenance, and improvement of water services.	Maintenance Services	General Fund, development fees, state and federal funds	Ongoing						
CD-14.2	Implement the City's Sewer Master Plan recommendations for replacement, maintenance, and improvement of sewer services.	Maintenance Services	General Fund, development fees, state and federal funds	Ongoing						
CD-14.1	Review development proposals for consistency water, power and solid waste disposal infrastructure requirements.	Development Services	General Fund, development fees	Ongoing						
CD-14.3	Continue to balance infrastructure fees with the cost of providing infrastructure services to new development.	Maintenance Services	General Fund, development fees	Ongoing						
CD-15.1 CD-15.2	Continue to participate in the Orange County Storm Water Management Plan.	Maintenance Services	General Fund	Ongoing	CD-26.1					
CD-15.1 CD-15.2	Provide information on storm water hazards, correct disposal techniques, and recycling options within the Brea Line on a quarterly basis.	Maintenance Services	General Fund	End of 2005	CD-26.1					
CD-15.3	Consider implementing parking restrictions for all streets in Brea on days in which street sweeping occurs.	Maintenance Services	General Fund	End of 2005						
CD-16.1	Provide funding or development incentives to encourage the installation of on-line services or learning opportunities.	Development Services, Maintenance Services	General Fund	End of 2006						
CD-16.2	Examine the feasibility of developing an on-line network or City-intranet for development of on-line building permits, registration, or payment of bills	Development Services, Maintenance Services	General Fund	By end of 2006						

**Community Development**  
Brea General Plan Implementation Guide

**SECTION IV: ECONOMIC DEVELOPMENT**

<b>Tax Base Revenue Growth</b>									
CD-23.1 CD-23.3 CD-23.6 CD-24.1 CD-24.2 CD-24.4 CD-24.5 CD-25.1	Continue to implement and update, as needed, the Business Retention and Attraction Program, which includes a regional marketing plan, the Brea Business Beat Program, coordination of the Economic Development Committee, the North Orange County Partnership (NOCP), and participation in the Orange County Consortium, and the Orange County Business Council activities.  Continue to coordinate with the North Orange Partnership (NOCP), Chamber of Commerce, and the Orange County Business Council to address the needs of existing and future businesses in the community. The City will work with these agencies to obtain input from the business community as to the best means for assisting local businesses and maintaining a healthy business environment.  Continue to work with the Brea Chamber of Commerce in providing services and programs that meet the needs of Brea's business community.	Economic Development  North Orange County Partnership  Brea Chamber of Commerce				CD-4.1 CD-6.5			
CD-23.2	Revise the zoning ordinance and map to reflect the adopted Land Use Policy Map, and implement the update ordinance and map over time.	Development Services	General Fund	By the end of 2004					
CD-23.5 CD-26.1	Continue to implement the Redevelopment Agency's 5-year Implementation Plan, Agency adopted goals, and Project Area Plans.	Economic Development Department	General Fund	Ongoing	CD-6.5				
CD-23.7	Create a program (updated regularly) that identifies and advertises childcare services in the City of Brea to residents, employers, and employees in Brea.	Community Services	General fund	Complete by 2004, Ongoing					

**Diversity**

CD-24.3	Work with Family Resource Center to offer educational and training opportunities for Breans.	Community Services	General fund	Ongoing					
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**Expansion of Downtown**

CD-25.2	Review development proposals to ensure proper site design consistent with the Zoning Ordinance Development Standards. Reconfigure parcels, consistent with the Subdivision Map Act, to ensure greater street visibility and adequate automobile and pedestrian access.	Development Services	General fund	Ongoing					
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**Revitalization of Aging Business Centers**

CD-26.2	Continue to provide low-interest loans to Brea businesses through the commercial Rehabilitation Loan Program that matches loans to businesses of exterior remodeling, common space upgrading, parking improvements, landscape and hardscape upgrades, and signage improvements.								
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**SECTION V: GROWTH MANAGEMENT**

CD-28.1	Participate in inter-jurisdictional planning forums such as the County of Orange, the Orange County Transportation Authority (OCTA) and other local jurisdictions or other appointed bodies to implement Measure M and the development of future revisions.	Development Services	General fund, Measure M	Ongoing	CD-9.1 CD-9.2 CD-9.3 CD-9.4				
CD-28.1	Participate in the inter-jurisdictional planning forums to discuss traffic improvements, cooperative land use planning and appropriate mitigation measures for development projects with multi-jurisdictional impacts.	Development Services	General fund, Measure M	Ongoing					
CD-27.5 CD-28.3	Work with inter-jurisdictional planning forums to improve the sub regional balance of jobs and housing	Development Services	General fund, Measure M	Ongoing					
CD-26.3 CD-28.2	Cooperate with state, county, and local governments in planning and implementing the City's Circulation Element, and coordinating efforts to ensure orderly development.	Development Services	General fund, Measure M	Ongoing			CR-13.1		
CD-27.1	Coordinate population, housing, employment, and land use projections with the state Department of Finance, Southern California Association of Governments (SCAG), the County of Orange Development Monitoring Program, and local school and water districts.	Development Services	General fund, Measure M	Ongoing					
G-28.1	Continue to cooperate with the County of Orange in annually updating the Congestion Management Plan (CMP). Participating in the annual CMP update will ensure that the City receives its share of state gasoline sales tax revenue for transportation improvements.	Development Services	General fund, Measure M	Ongoing					
G-27.4 G-27.5	Provide incentives for infill development through development and parking standards in the City's zoning code.	Development Services	General fund	By 2003					

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies														
					Community Development	Housing	Community Resources	Community Services	Public Safety										
<b>SECTION VI: PARKS AND OPEN SPACE SECTION</b>																			
<b>Parks</b>																			
CR-1.1 CR-1.2 CR-1.4 CR-1.5 CR-1.6 CR-1.7 CR-3.3 CR-3.6	Assess the existing park and recreation system according to the standards established in the Parks and Open Space Section. Require dedication of park facilities, a fee in lieu thereof, or a combination of both, as a condition of new development pursuant to the Quimby Act. Also encourage the development of parks and recreational facilities by the private sector, but available for use by the public. Annually review park facility dedication and development fee requirements to ensure they reflect changing needs of the community, population growth, and current land and construction costs.  Properly maintain and operate the grounds and facilities of existing parks to minimize vandalism and crime, affirm safe and pleasant environments, and maximize public use and enjoyment.	Community Services, Development Services	Development fees, General fund, Park dedication funds	Annually	CD-1.7 CD-1.8 CD-8.3 CD-8.6		CR-6.2 CR-6.3 CR-6.4 CR-6.6 CR-6.7	CS-2.1 CS-2.2 CS-2.3 CS-2.4	PS-2.3 PS-3.1 PS-3.2 PS-3.3										
CR-1.3	Create new park standards of park development as follows:  <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">Type of Park</th> <th style="text-align: left;">Size and Service Area</th> </tr> </thead> <tbody> <tr> <td>Smaller green spaces in urban areas (mini, tot lots, and/or pocket parks)</td> <td>0.5 to 5 acres, with a 1/4 mile service area radius in residential setting</td> </tr> <tr> <td>Neighborhood Park</td> <td>5-10 acres, with a 1/4- to 1/2-mile service area radius</td> </tr> <tr> <td>Community or Sports Park</td> <td>20-50+ acres, with a 1/2- to three- mile service area radius</td> </tr> <tr> <td>Regional Park</td> <td>50 acres or larger</td> </tr> </tbody> </table>	Type of Park	Size and Service Area	Smaller green spaces in urban areas (mini, tot lots, and/or pocket parks)	0.5 to 5 acres, with a 1/4 mile service area radius in residential setting	Neighborhood Park	5-10 acres, with a 1/4- to 1/2-mile service area radius	Community or Sports Park	20-50+ acres, with a 1/2- to three- mile service area radius	Regional Park	50 acres or larger	Community Services, Development Services	Development fees, General fund, Park dedication funds	Establish new standards by end of 2004;	CD-8.3				
Type of Park	Size and Service Area																		
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Regional Park	50 acres or larger																		
CR-2.1 CR-2.2 CR-3.7	Review development proposals for potential impacts to existing public parks and open space from non-recreational uses. Ensure that new uses or facilities, including new recreation facilities, do not displace existing casual use facilities and parks.	Development Services, Community Services	Development fees	Ongoing	CD-5.4														
CR-3.1	Create a scheduling program that maximizes use of available recreational programs and facilities.	Community Services	General fund	Establish program by end of 2004;				CS-2.2 CS-2.3 CS-2.4											
CR-3.2	Continue to engage in joint use agreements with the Brea-Olinda School District to further expand recreational facilities.	Community Services, Development Services, Brea-Olinda School District	General fund	Ongoing				CS-3.1 CS-3.2 CS-3.3 CS-6.2											
CR-3.4	Research "Rails to Trails" program and related resources for potential funding sources. Create strategy and proposal to convert abandoned segments of the Union Pacific railroad right-of-way to an additional recreational use, trail, and/or greenway.	Development Services, Community Services	General fund	Complete analysis by 2006			CR-6.4 CR-7.3												
CR-3.5	Collaborate with Federal, state, regional, and local agencies regarding the integration with existing and potential trail systems, recreational facilities, and recreation programs.	Development Services	Development fees, General fund	Ongoing			CR-4.3 CR-5.3 CR-6.1 CR-6.2 CR-6.3												
<b>Open Space</b>																			
CR-4.1 CR-5.1	Investigate creative methods or programs aimed at acquiring and/or purchasing open space lands such as: <ul style="list-style-type: none"> <li>▪ Conservation easements;</li> <li>▪ Purchase of development rights;</li> <li>▪ Transfer of development rights;</li> <li>▪ Mitigation and Land banking;</li> <li>▪ Development rights agreement; and</li> <li>▪ Open space preservation fund;</li> </ul>	Development Services	Development fees, Bonds, Special assessment districts, Grants, Open space conservation funds,	By the end of 2004;	CD-1.7 CD-1.9 CD-1.10 CD-6.2 CD-6.7 CD-7.1 CD-7.5 CD-8.6		CR-8.1 CR-8.3 CR-9.1 CR-9.2 CR-9.3 CR-9.5 CR-10.6												

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CR-4.2 CR-5.2	Assess development proposals for potential impacts to important geologic and scenic landform features pursuant to the CEQA. Require appropriate mitigation measures, including environmentally sensitive site planning and grading, revegetation and open space dedication, for significant impacts. Consider important topographical features, watershed areas, ridgelines, soils, and potential erosion problems in the assessments. Ensure the new development is compatible to adjacent open space lands.	Development Services	Development fees, Grading permit fees,	Ongoing			CR-9.6 CR-10.3 CR-10.5 CR-10.5 CR-11.5 CR-11.6		PS-8.3 PS-8.4
CR-4.3 CR-5.3	Collaborate with Federal, state, regional, local, non-profit, environmental and conservation organizations, and private entities to conserve, protect, and enhance open spaces and natural resources throughout Brea's Planning Area.	Development Services	General fund	Ongoing	CD-1.7 CD-7.5				
<b>SECTION VII: TRAILS</b>									
CR-6.1 CR-6.2 CR-6.3 CR-6.6 CR-6.7 CR-7.4	<p>Develop the multi-use trail system illustrated in Trail Section (Figure CR-2) with the following standards:</p> <ul style="list-style-type: none"> <li>▪ Provide multi-purpose trails, where possible, to serve hikers, bicyclers, and horseback riders.</li> <li>▪ Link trails with adjacent City, County, and State (Chino Hills State Park) trail systems.</li> <li>▪ Maintain trail areas in good condition, and free of litter and debris</li> <li>▪ Design trails with coordination with landowners and conservation biologist to develop trails that will be flexible and site-specific to minimize the impact on adjacent property and fragile habitats.</li> <li>▪ Provide a trail system with both short and long hikes/rides and serve the needs of both beginning and advanced hikers/riders.</li> <li>▪ Utilize citizen volunteers to assist in the development, maintenance and operation of trails and facilities.</li> <li>▪ Keep citizens aware of the trail system through publication of a trails map which also notes safety and courtesy tips.</li> <li>▪ Separate trails from automobile traffic when possible in order to provide safe conditions for riders and walkers.</li> <li>▪ Provide appropriate signs to mark all trails.</li> <li>▪ Design trails entrances to prevent unwanted trail usage by motorized vehicles.</li> <li>▪ Locate trails to provide linkages between open space and the City greenway system.</li> </ul> <p>Adopt and implement a capital improvement program for trail acquisition and improvements to the trail system.</p>	Development Services, Community Services	Development fees, General fund, CIP funds	Ongoing	CD-1.7 CD-1.8 CD-1.9 CD-4.2 CD-7.6 CD-8.2			PS-2.2 PS-2.3 PS-3.1 PS-3.3	
CR-6.4 CR-7.3	<p>Research and acquire Federal, state, local, and non-profit resources to fund and develop trails throughout Brea.</p> <p>Research "Rails to Trails" program and related resources to convert abandoned Union Pacific railroad right-of-way to a community trail as described in the Trails Section of the General Plan.</p>	Development Services, Community Services	General fund	Ongoing			CR-3.4		
CR-6.5	Coordinate with other State and local agencies in creating an integrated regional trail system that connects to important recreational activity areas within Northern Orange County.	Development Services, Community Services	General fund	Ongoing					
CR-7.1 CR-7.2 CR-7.5	Require development proposals to include improvements for an urban and walkway trail system including, but not limited to access points, landscaping, proper lighting, intersection improvement, decorated accent paving materials, and other pedestrian improvements. Ensure easy linkages to important activity areas such as schools, commercial areas, cultural and social centers and other high pedestrian activity areas.	Development Services, Community Services	Development fees	Ongoing	CD-2.2 CD-2.3 CD-2.4 CD-2.5 CD-6.2		CR-7.1 CR-7.2 CR-7.5		PS-2.2 PS-2.3 PS-3.1 PS-3.2 PS-3.3

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
<b>SECTION VIII: WILDLIFE HABITAT</b>									
CR-8.1	<p>Require development proposals, particularly in sphere of influence and hillside areas to preserve, restore, and enhance existing wildlife corridors, habitat, and roadway crossings.</p> <p>Require development proposals to include the assessment of potential migratory birds and raptor nests (in compliance with the Migratory Bird Treaty Act and the California Fish and Game Code.) Mitigation for the presence of active nests may be conducted in the following ways:</p> <ul style="list-style-type: none"> <li>▪ Prior to the commencement of tree removal during the nesting season (February – July), all suitable habitats should be thoroughly surveyed for the presence of nesting birds by a qualified biologist. If any active nests are detected, the area shall be flagged and avoided until the nesting cycle is complete; or</li> <li>▪ Tree removal and grading may be delayed until after the breeding season (August-January) to ensure that no active nests will be disturbed.</li> </ul>	Development Services, United States Fish and Wildlife Service (USFWS), California Department of Fish and Game (CDFG)	Development fee, Environmental impact mitigation	Ongoing	CD-6.2		CR-10.3 CR-10.5		
CR-8.2 CR-9.2 CR-9.3 CR-9.4	<p>Assess development proposals for potential impacts to significant natural resources pursuant to the California Environmental Quality Act (CEQA) and associated state and federal regulations. Require appropriate mitigation for all significant impacts if impact avoidance is not possible. Mitigation measures for habitat and species may include, but are not limited to, avoidance, enhancement, restoration, or a combination of any of the three. Address the following subjects in impact assessment as applicable to individual development proposals:</p> <ul style="list-style-type: none"> <li>▪ Natural communities;</li> <li>▪ Riparian and wetland habitat (including potential jurisdictional areas);</li> <li>▪ Coastal sage scrub habitat;</li> <li>▪ Coast live oak habitat;</li> <li>▪ Rare and endangered plant and animal species;</li> <li>▪ Habitat fragmentation (including disruption of wildlife corridors); and</li> <li>▪ Creeks, blue line streams, and associated tributaries;</li> </ul>	Development Services	Development fee, Environmental impact mitigation	Ongoing	CD-6.3		CR-10.3 CR-10.5		
CR-8.3	<p>Require development proposals to consult with regulatory agencies, including the United States Fish and Wildlife Service (USFWS), California Department of Fish and Game (CDFG), United States Army Corp of Engineers (USACE), about natural resources if the following occurs on the project site:</p> <ul style="list-style-type: none"> <li>▪ Potential take or substantial loss of occupied habitat for any threatened or endangered species; or</li> <li>▪ Existing drainage features (jurisdictional area) are located within the project site.</li> </ul>	Development Services, United States Fish and Wildlife Service (USFWS), California Department of Fish and Game (CDFG)	Development fees	Ongoing					
CR-8.4	Work with resource agencies, universities, and other groups that help monitor wildlife and determine effectiveness of wildlife corridors.	Development Services	General fund, Development fees	Ongoing					
CR-9.1	Consult with Wildlife Corridor Conservation Authority (WCCA) to provide the proper planning, conservation, environmental protection, and maintenance of habitat and wildlife corridors.	Development Services, Wildlife Corridor Conservation Authority (WCCA)	General fund	Ongoing	CD-6.3				

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CR-9.5 CR-9.6	Investigate and create programs to manage wildlife habitat and natural resources using sound ecological principles and professionally accepted methods to protect and restore sensitive animal populations and their habitats and therefore preventing major disruptions or destruction.	Development Services, Non-profit resource groups	General fund	Establish program by end of 2006; Implement as needed					
<b>SECTION IX: SCENIC RESOURCES</b>									
CR-10.1 CR-10.2	Investigate creating program with standards and regulations for scenic routes and highways within the Brea Planning Area. Create program thereafter if deemed feasible.	Development Services	General fund	Investigate program by end of 2005					
CR-10.3 CR-10.5	Assess development proposals for potential impacts to significant natural resources pursuant to the California Environmental Quality Act (CEQA) and associated state and federal regulations. Require appropriate mitigation for all significant impacts if impact avoidance is not possible. Mitigation measures for habitat and species may include, but are not limited to, avoidance, enhancement, restoration, or a combination of any of the three. Address the following subjects in impact assessment as applicable to individual development proposals: <ul style="list-style-type: none"> <li>▪ Natural communities;</li> <li>▪ Riparian and wetland habitat (including potential jurisdictional areas);</li> <li>▪ Coastal sage scrub habitat;</li> <li>▪ Coast live oak habitat;</li> <li>▪ Rare and endangered plant and animal species;</li> <li>▪ Habitat fragmentation;</li> <li>▪ Creeks, blue line streams, and associated tributaries;</li> <li>▪ Significant tree stands.</li> </ul>	Development Services	Development fees	Ongoing					
CR-10.4	Assess development proposals, including those in Brea's Sphere of Influence, for potential impacts to important visual resources identified in the Scenic Resources Section pursuant to the California Environmental Quality Act. Require appropriate mitigation measures for all significant impacts. Consider important ridgelines, creeks, open space, views of the surrounding hillsides, and light and glare affects.	Development Services	Development fees	Ongoing			CR-4.1 CR-5.1		
CR-10.6	Work with Federal, state, regional, and other appropriate public agencies, private entities, and landowners to conserve, protect and enhance natural resources throughout Brea's Planning Area.	Development Services	General fund	Ongoing					
<b>SECTION X: WATER CONSERVATION AND QUALITY</b>									
CR-11.1 CR-11.4 CR-11.5	Encourage water conservation throughout Brea in the following ways: <ul style="list-style-type: none"> <li>▪ Encourage the Maintenance Services Department to expand the production of recycled water and work with the districts to develop new uses for recycled water;</li> <li>▪ Require new projects to install and maintain a dual system when reclaimed water is available;</li> <li>▪ Require the use of drought resistant plant species (native species where possible) in landscaping for private and public areas, including parks;</li> <li>▪ Work with the Maintenance Services Department on the development of new water sources to serve the Planning Area;</li> <li>▪ Work with the Brea-Olinda School District to establish water conservation programs;</li> <li>▪ Require the incorporation of water conservation devices (including low-flush toilets, flow restriction devices and water conserving appliances) in new residential and non-residential development, public projects and rehabilitation projects; and</li> <li>▪ Continue to implement where applicable, the regulations and requirements in the Water-Conserving Landscape Ordinance.</li> </ul>	Development Services, Maintenance Services	General fund	Ongoing	CD-9.1 CD-9.2 CD-9.3 CD-9.4				

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CR-11.2	Where practical, incorporate Federal, state, and other agency policies and standards for water efficiency (e.g.: reclaimed and recycled water for landscape irrigation) into City codes, regulations, and procedures	Maintenance Services	General fund	Ongoing					
CR-11.3	Regularly review the water rate structure to ensure that it encourages efficiency and is competitive.	Maintenance Services	General fund	Ongoing	CD-9.1 CD-9.2 CD-9.3				
CR-11.4 CR-11.6 CR-12.1 CR-12.2	To reduce pollutants in urban runoff, require new development projects and substantial rehabilitation projects to incorporate Best Management Practices pursuant to the National Pollutant System Discharge Elimination System Permit (NPDES), Santa Ana Regional Quality Water Control Board, and the County of Orange Drainage Area Master Plan to ensure that the City complies with applicable State and federal regulations.  Adopt and enforce regulations in educational efforts to eliminate pollution from urban runoff.	Development Services, Maintenance Services, Santa Ana Regional Quality Water Control Board, County of Orange,	Development fees, Drainage fees, General fund, state and Federal funding sources	Ongoing					
<b>SECTION XI: AIR QUALITY</b>									
CR-13.1	Continue to implement the Transportation Demand Management (TDM) ordinance that implements the provisions of the 1997 Air Quality Management Plan. The ordinance will specify various TDM methods to reduce trips and influence travel modes such as: <ul style="list-style-type: none"> <li>▪ Trip reduction programs for City staff;</li> <li>▪ Van pool programs for private employers;</li> <li>▪ Employee incentives for public transit use;</li> <li>▪ Preferential parking for carpools;</li> <li>▪ Trip reduction programs for major commercial centers; and</li> <li>▪ Alternative transportation modes for major events.</li> </ul> Implement the programs described in the Circulation and Land Use Sections related to transportation system management (TSM) to avoid traffic congestion and reduce related emissions levels and urban runoff pollutants resulting from excess vehicle travel miles.	Development Services	General fund	By the end of 2006	CD-11.1 CD-11.2 CD-11.3 CD-11.4 CD-11.5 CD-11.6 CD-12.1 CD-12.2 CD-12.4				
CR-13.2	Promote retrofit programs by the City to reduce energy usage and consequently reduce emissions from energy consumption. Provide informational literature about available retrofit programs at City offices. Inform property owners of retrofit programs when plans for remodeling and rehabilitation projects are submitted.	Development Services, Southern California Gas Company, Southern California Edison Company	General Fund, development fees, Southern California Gas Company, Southern California Edison Company	Ongoing					
CR-13.3 CR-13.4 CR-13.5	Review development proposals for potential air and water quality impacts pursuant to the California Environmental Quality Act (CEQA), Orange County Air Pollution Control District, and the South Coast Air Quality Management District CEQA Air Quality Handbook. Construction impacts can be reduced by enforcing SCAQMD Rule 403. Reduce long-term impacts using available land use and transportation planning techniques such as: <ul style="list-style-type: none"> <li>▪ Incorporation of public transit stops;</li> <li>▪ Pedestrian and bicycle linkage to commercial centers, employment centers, schools and parks;</li> <li>▪ Preferential parking for carpools;</li> <li>▪ Traffic flow improvements; and</li> <li>▪ Employer trip reduction programs.</li> </ul>	Development Services	General fund	Ongoing	CD-11.1 CD-11.2 CD-11.3 CD-11.4 CD-12.1 CD-12.3 CD-12.4		CR-7.5 CR-13.4 CR-13.5		

**Community Resources**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CR-13.6 CR-13.7 CR-13.8	<p>Work with the South Coast Air Quality Management District (SCAQMD) and the Southern California Association of Governments (SCAG) to implement the Air Quality Management Plan (AQMP) and meet federal and state air quality standards for all pollutants. To ensure that new measures can be practically enforced in the region, participate in future amendments and updates of the AQMP.</p> <p>To reduce dust and particulate matter levels, implement SCAQMD's fugitive dust control measures such as:</p> <ul style="list-style-type: none"> <li>▪ Restricting outdoor storage of fine particulate matter;</li> <li>▪ Controlling construction activities and emissions from unpaved areas; and</li> <li>▪ Paving areas used for vehicle maneuvering.</li> </ul> <p>Cooperate with the Integrated Waste Management Department's (IWMD) Regulatory Compliance group and South Coast Air Quality Management District (SCAQMD) to enforce air quality regulations at the Olinda Alpha Landfill.</p>	Development Services, IWMD, and SCAQMD	General fund	Ongoing			CR-13.7 CR-13.8		
<b>SECTION XII: HISTORIC RESOURCES</b>									
CR-14.1 CR-14.3	Assess development proposals for potential impacts to significant historic and cultural resources pursuant to 15064.5 of the California Environmental Quality Act.	Development Services	Development fees	Ongoing	CD-5.4				
CR-14.2	Investigate the potential of creating historic districts, conservation districts, and/or preservation overlay zones within the City. Amend the Historic Preservation Ordinance in developing criteria for district designation and include appropriate regulations to safeguard historic resources within the delineated district.	Development Services	General fund,	Investigate by end of 2005;	CD-5.7				
CR-14.4 CR-14.5	Research all types of possible funding sources and financial assistance (such as California Mills Act) to help promote, preserve, and restore historical resources in Brea. Such funding sources can include, tax incentives, Federal and state funds, grants, awards, loans, and other financial sources.	Development Services	General fund, Tax credits, grants, state and Federal funding sources	Ongoing	CD-4.7				
CR-14.6	<p>List historic resources and structures to various Federal, state, and local historic registers, including Brea's Historic Resources Register, to prevent and discourage demolition.</p> <p>Update the City's Historic Resources Register to further increase level of awareness of Brea's historical resources. Continue to add resources that meet City's Historic Preservation Ordinance.</p>								
CR-14.7 CR-14.8 CR-14.9 CR-15.3 CR-15.5 CR-15.6	<p>Work with Community Services and Recreation Services Department to promote Brea's heritage through artwork, signs, preservation, recreation programs, events, and historical structures throughout the City and in City park lands.</p> <p>Use Federal, state, non-profit, and private programs and resources to promote Brea's historic resources.</p>	Development Services, Community Services	General fund	Ongoing	CD-4.7 CD-5.6			CS-5.1 CS-5.2 CS-5.4 CS-5.6	
CR-15.1	Identify and implement programs to assist and encourage private property owners to preserve historic, cultural, and archaeological resources within the City.	Development Services	General fund	Ongoing					
CR-15.2	Continue to work with the City's Historical Committee and Brea Historical Society to accomplish the goal of increasing awareness of Brea's historic resources and the importance of their preservation. Work with Historical Society in preserving and recognizing City's historical materials and resources. Continue to distinguish resident's hard work for renovating and restoring historic structures with the Brea Historic Preservation Award honored by both the Brea Historical Society and City Council.	Development Services	General fund	Ongoing					
CR-15.4	Continue to work with the Brea Olinda Unified School District to provide access to the City's information and structures that reflect the community's history.	Development Services, Community Services	General fund	Ongoing				CS-3.1 CS-3.5	

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
<b>SECTION XIII: HUMAN SERVICES</b>									
CS-1.1	Continue to use the Community Center as a resource for family classes.	Community Services Department	General Fund	On-going					
	Continue to use volunteer coordinators and committees to coordinate family classes and activities.	Community Services Department	General Fund	On-going					
	Conduct an annual survey to assess the supply and demand of childcare in Brea (by a volunteer).	Community Services Department	Not applicable	Annually					
	Maintain and make available pertinent information on child care providers through referral information.	Community Services Department	General Fund	On-going					
	Encourage full integration of individuals with disabilities in all phases of community life.	Community Services Department, Development Services	General Fund	On-going					
	Advocate, enhance and maintain the delivery of meals to the homes of low income, frail seniors, through the City, County and Feedback.	Senior Citizen Advisory	General Fund	On-going					
	Maintain (in partnership) an adult day program in Brea	Community Services Department	General Fund	On-going					
	Refine and expand the existing provider inventory for disabled residents services	Community Services Department	General Fund	On-going					
	Formalize a commitment to support the Brea Family Resource Center through adopting of the Human Services Master Plan.	Community Services Department	General Fund	2003					
	Maintain support for community volunteer coordinator program	Community Services Department	Not applicable	On-going					
	Continue to offer family friendly arts experiences highlighting value and fun.	Community Services Department	General Fund	On-going				CS-5.3	
CS-1.1 CS-1.2	Maintain a Human Services Coalition to facilitate the establishment of community based agencies and organizations that provide human services.	Community Services Department	Not applicable	On-going					
	Work with the School District to collaborate on after school and summertime programs.	Community Services Department , Brea-Olinda School District,	General Fund	On-going					
	Continue to expand business involvement to support youth and families.	Community Services Department	General Fund	Develop annually					
	Evaluate parent education programs and resources for effectiveness	Community Services Department	General Fund	On-going					
	Update and evaluate community awareness campaign regarding well-being of youth, families, and available resources.	Community Services Department	General Fund	By 2004					

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CS-1.1 CS-1.4	Continue to establish various partnerships with law enforcement. Identify needed teen prevention programs for particular teen age groups.  Continue to market the volunteer program for the older population to tap into their skills and life experiences.  Encourage the development of long term care facilities in Brea that will serve the needs of low-income seniors.  Provide programs and activities that focus on socialization, recreation, health, wellness and education needs at the Community Center. Continue vigorous effort to attract the active senior.  Annually provide documented information on services and programs available in the community for older population.  Continue to support and expand public transportation for seniors.  Expand the supply of quality licensed home childcare providers as need is determined through survey data. Explore the feasibility of including child care impact mitigation in development agreements. Continue to be available for cooperative daycare programs with Brea Olinda School District.  Continue to provide free meeting space for senior citizens' groups at the Brea Senior Center  Support and sustain the activities of the Brea Senior Center including transportation and the lunch program  Encourage quality services targeted to Brea residents with disabilities, such as Brea Express  Expand and support youth volunteer programs to encourage community involvement.  Support and enhance the Case Management Program, the Homemaker Program, and Respite Service program delivered from the Brea Senior Center for "frail elderly"	Brea Police Department, Community Services Department Community Services Department Development Services Community Services Department Community Services Department Community Services Department, Development Services Department Brea, Olinda School District, Development Services Community Services Department Community Services Department Community Services Department Senior Citizen Advisory	Not applicable General Fund General Fund General Fund General Fund General Fund General Fund General Fund General Fund General Fund General Fund	On-going On-going 2005 On-going Annually On-going Conduct survey by end of 2005 On-going On-going On-going On-going On-going					
CS-1.1 CS-1.2 CS-1.4	Develop and expand hotline for teens with updated programs and services.  Investigate the feasibility of Youth Council and annually evaluate the level of participation of Student Advisory Board (7 <sup>th</sup> & 8 <sup>th</sup> graders).  Evaluate effectiveness of youth employment program for teens.	Community Services Department Community Services Department, Brea-Olinda School District Community Services Department	General Fund General Fund General Fund	Implement hotline and other programs by late 2006 Begin investigation by 2005 Annually					
CS-1.1 CS-1.2 CS-1.4	Encourage the development of local recreational alternatives for older teens (17-19)	Community Services Department	General Fund	On-going					

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CS-1.1 CS-1.3 CP-1.4	Continue to strive to establish sports programs to meet the developmental skills/ differences of youth.	Community Services Department	General Fund	On-going					
CS-1.1 CS-1.6	Annually evaluate After School Program at the Community Center. Involve participants and their parents in determining future programs and activities.  Host a regular “convening” of service group stakeholders every two years. Inform service providers of the volunteer coordinator’s role.  Create a formal community education system and conduct personal outreach to senior citizens.  Increase collaboration between schools, the community center and parents through exchanging activity calendars, and having a contact person at each school, community center, the City and the police department.	Community Services Department  Community Services Department, Development Services Department Community Services Department  Community Services Department, Brea Police Department	General Fund  General Fund  General Fund	Annual  Every 2 years  End of 2006  On-going					
CS-1.2 CS-1.3	Encourage bilingual families/parents to volunteer at the community center	Community Services Department	Not applicable	On-going					
CS-1.2 CS-1.4	Offer a shuttle bus from the community center to the junior high school.	Community Services Department	General Fund	2004					
CS-1.3	Offer free/low cost child care at the site of ESL classes.  Encourage the continuation and development of after school and evening programs for children and youth. Maximize participation by increasing the number of locations where the programs are provided and supporting transportation options to these locations. Develop a focused implementation plan that will examine the needs of youth, senior, and cultural integration programs in Brea. The Plan will examine existing needs, resources, and service gaps.	Community Services Department  Development Services Department  Community Services Department	General Fund  General Fund, Redevelopment, Impact fees General Fund	2003  On-going  2004					
CS-1.4	Create developer incentives (such as increased floor area ratio or reduced parking requirements) for employers or developers who include child and/or senior care facilities in the design of new housing and commercial buildings. Provide an internet link and description of community clubs on the City’s website.	Development Services Department  Development Services Department	Impact fees  General Fund	2004  2003					
CS-1.4 CS-1.5	Establish/designate a series of days which focus on specific charitable events/programs that service clubs could jointly support to the level of capability.	Community Services Department	Not applicable	2004					
CS-1.4 CS-1.5 CS-1.6 CS-1.7	Look into volunteers at the senior center, cultural organizations and high school students to translate at schools and school events such as during Parent Teacher nights.	Community Services Department, Brea Olinda Unified School District	Not applicable	On-going					
CS-1.5	Establish “volunteer time” as a community service credit for city employees working for community organizations or at events.  The City shall work with faith based organizations in the community to expand children-oriented services to the youth of Brea.	Community Services Department  Community Services Department	Not applicable  Not applicable	2003  On-going					

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CS-1.5 CS-1.6	Develop a coordination director or liaison between the city and community groups. This position would coordinate the assistance of service clubs to assist/perform charitable programs such as "taste of Brea", "Fourth of July" and other similar events. This would also help in informing clubs when any city assets/equipment is available/planned for redistribution.	Brea Community Services Department	General Fund	2003					
CS-1.6	Establish tangible incentives and recognition programs to high school volunteers for community service.	Brea Olinda Unified School District	Not applicable	2003					
<b>SECTION XIV: RECREATIONAL SERVICES</b>									
CS-2.1	The City shall strive to meet the growing recreational needs of residents through the acquisition of new programs and recreational facilities.  Support community and senior services agencies with developing recreational outreach programs.	Community Services Department	General Fund	On-going  Development implementation program by 2005.					
CS-2.2	Work with the Brea Olinda School District to develop after school recreational opportunities for teens and youth.	Community Services Department, Brea Olinda School District	General Fund,	On-going					
CS-2.3	Partner with private agencies and employment centers to develop recreational programs such as work-related sports leagues and fitness days.	Community Services Department	General Fund	2003					
CS-2.4	Continue to engage in joint use agreements with the Brea-Olinda School District to further expand recreational facilities.	Community Services Department , Brea Olinda School District	General Fund	On-going			CR-3.2 CR-1.5		
CS-2.5	Continue to support the role of the Parks, Recreation, and Human Services Commission.	Community Services Department	General Fund	On-going					
<b>SECTION XV: EDUCATION SERVICES</b>									
CS-3.1	Work with the Brea Olinda Unified School District to determine ways that schools can be made more available to the community for weekend and evening use.  Increase collaboration between schools, the community center and parents through exchanging activity calendars, and having a contact person at each school, community center, the City and the police department  Cooperate with existing public and non-profit service agencies to develop a service program that will coordinate the efforts of providing education to children and youth in Brea.	Brea Olinda Unified School District, Development Services Department	General Fund	On-going					
CS-3.2	Provide regular status reports to the Brea Olinda Unified School District on potential and approved development projects. Review development proposals and amendments for consistency with school facility requirements.	Brea Olinda Unified School District, Development Services Department	General Fund	Implement by 2005.					PS ES-1.3
CS-3.1 CS-3.2	Explore creating a part-time school and city liaison position.	Brea Olinda Unified School District, Development Services Department	General Fund	Investigate feasibility by end of 2005.					
CS-3.3	Work with the Brea Olinda school district to assess development impact fees and other potential funding sources for the development of new school facilities.	Brea Olinda Unified School District, Planning Department	General fund	On-going		ES-1.3			
CS-3.2 CS-3.3	Require an assessment of school impacts prior to the approval of development projects that require legislative acts, including general plan amendments and zoning changes.	Brea Olinda Unified School District, Development Services Department	General Fund	Investigate feasibility by end of 2005.					

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CS-3.4 CS-3.5	Offer a shuttle bus from the community center to the junior high school.	Brea Olinda Unified School District, Development Services	General Fund	Investigate feasibility by end of 2004.					
CS-3.6	Encourage the establishment of a post-secondary educational institution such as an art school, satellite university campus, or vocation school. Provide funding and regulatory incentives to facilitate development.	Economic Development, Development Services	Impact fees and outside funding sources	Commence investigation by 2005.					
<b>SECTION XVI: LIBRARY SERVICES</b>									
CS-4.1	Encourage adequate library services not only for general users of the facility, but also those involved with children's programming and English as a second language users.	Orange County Library	General Fund	On-going					
CS-4.1 CS-4.3	Establish an advisory network composed of representatives of local public facility and service provider, City officials, the Brea Olinda School District and the Orange County Public Library. This network would not be a standing board of the City, but rather, a group that periodically meets and advises the Orange County Library in order to ensure that the community's needs for library services are being met.  Explore possible joint marketing Library programs with City and Schools.	Orange County Library, City of Brea, Brea Olinda Unified School District	General Fund	2004  Investigate feasibility by 2004.					
CS-4.3	Identify and continually monitor library service needs in the community through surveys and outreach.  Provide advanced communication opportunities for the public at City libraries.  Explore need for citizens support group for the Library.  Continue effort to expand existing Library programs through the use of teen and adult volunteers.	Orange County Library, Community Services Department	General Fund	2003  Ongoing  Investigate program by 2005.  On-going					
CS-4.4	Cooperate with the Orange County Public Library system, in exploring the development of maintaining a new or expanded library facility in the community.	Orange County Library	General Fund	Investigate feasibility by 2005.					
<b>SECTION XVII: CULTURAL ARTS</b>									
CS-5.1	Continue implementation of the Arts in Public Places Program.	Community Services Department, Development Services	General Fund, Development fees	On-going					
CS-5.1 CS-5.11	Prepare a Cultural Arts Master Plan.	Community Services Department	General Fund	Complete Plan by end of 2005					
CS-5.2	Work with the Brea Historical Society to establish criteria to include more representation of Brea's history and historical resources in arts programs and public art.	Community Services Department,	General fund, Development fees	On-going					
CS-5.2 CS-5.9	Explore new and creative methods for capturing and preserving the cultural heritage of the community, such as oral history programs and videotapes.	Community Services Department	General Fund	2005					
CS-5.3	Continue to integrate the fine arts into the After School Program.  Continue to establish art internships.	Community Services Department	General Fund	On-going					
CS-5.3 CS-5.7	Cooperate with the Brea Olinda Unified School District, local private schools and colleges to increase art education in Brea.	Community Services Department, Brea Olinda School District	General fund, School District funds	On-going					
CS-5.4	Develop an art recognition or awards program to recognize local artists.	Community Services Department	General Fund	Establish program by end of 2005					

**Community Services**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
CS-5.5	Develop opportunities for live/work "artist loft" housing for artist through zoning, regulatory incentives and funding. Restrict these units for use by local artists.	Community Services Department, Development Services	General Fund	Explore development of program by end of 2005. Implement as needed.					
CS-5.6	Adopt a plaque program to recognize culturally important and heritage sites.  Continue implementation of the Art Shelter program for public bus stops.  Develop program objectives and provide detailed standards and guidelines for the installation of public art, with an emphasis on the relationship between local history and public art. Address issues of artist selection, project siting, funding, etc. There should be close coordination with the Streetscape Standards & Guidelines and the Environmental Graphics Program.	Community Services Department	General Fund	Develop of program by 2006  On-going  Develop program by 2006					
CS-5.8	Examine possible private/public partnerships to further cultural arts awareness and programs.	Community Services Department	General Fund	On-going					
CS -5.11	Consider developing an 'arts shuttle' similar to the arts shuttle in Laguna Beach and Pasadena that provides transportation to different venues in Brea.	Community Services Department, Development Services	General Fund	Explore feasibility of shuttle by 2005.					
CS-5.12	On a periodic basis, poll the community to identify demand for cultural arts programs and to determine whether needs/desires are being met.	Community Services Department	General Fund	Develop survey by end of 2004. Poll community every two years.					
CS -6.1	Continue to utilize and promote the Curtis Theater, Brea Gallery, and the Community Center for cultural art venues.	Community Services Department	General Fund	On-going					
CS-6.2	Develop joint-use agreements with the Brea Olinda School District for the sharing of performance and exhibit spaces.	Community Services Department, Brea Olinda School District	General Fund	Begin development of agreements by 2004					
CS-6.3 CS-6.3	Examine opportunities within existing structures and new development for the exhibit spaces or performance venues.	Community Services Department, Development Services	General Fund	2005					
CS- 7.2	Encourage Brea talent to apply for grants for public art creation and programs.	Community Services Department	General Fund	On-going					
CS-7.1 CS- 8.1	Apply for state, federal, local and private grants to increase participation and promote Brea's cultural arts programs.	Community Services Department	General Fund	2005 and as needed					
CS-7.3	Encourage City staff to volunteer at City-sponsored arts events.	Community Services Department, City of Brea	General Fund	On-going					
CS-7.2 CS--8.1	Work with non-profit, private and community organizations to provide funding, volunteer and exposure for cultural arts activities. Explore the development of a community or privately sponsored plaque or mural program.	Community Services Department	General Fund	On-going					

**Public Safety**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
<b>SECTION XVIII: EMERGENCY SERVICES AND SAFETY</b>									
PS-1.1	Ensure that city staff continues to meet with the Brea Police Department to identify community needs, issues and any impacts that new development might have on police services. Evaluate the need for additional police facilities to serve new development.	Police Services	General fund	Ongoing					
PS-1.2	Coordinate with the Brea Fire and Police Department to determine the need and applicability of new technology and equipment in Brea.	Police Services, Fire Services	General fund	Ongoing					
PS-1.3	Evaluate the number of officers, total population, and crime statistics to ensure that appropriate levels of police protection are provided citywide.	Police Services	General fund	Ongoing					
PS-1.4	Fund appropriate levels of fire personnel, staff, and equipment, to maintain a four- to six-minute emergency response time for fire emergencies.	Police Services, Fire Services	General fund	Ongoing					PS-6.2
	Utilize wildland urban fire hazard mitigation techniques involving vegetation modification including chipping, scattering, multi-cutting, crushing, pruning and other non-fire hazard abatement concepts.	Development Services, Fire Services	General fund	Ongoing					PS-6.2 PS-6.3
PS-1.5	Require new development to pay for increased fire protection as necessitated by a particular development..	Development Services, Fire Services	General fund	Ongoing					
PS-1.6	Require the installation of sprinkler systems in all buildings that are not readily accessible to fire-fighting equipment and apparatus or do not meet minimum fire flow and fire hydrant requirements.	Development Services, Fire Services	General fund	Ongoing					
	Inspect water hydrants and conduct fire-flow tests on an annual basis, with priority given the high-fire-hazard areas.	Fire Services	General fund	Ongoing; Annually					
PS-1.7	Continue active community oriented policing programs such as the Brea Bicycle Patrol program and the Community Action Patrol. Require the participation of the Police Department in the development review process relative to building and site plan vulnerabilities to criminal activity and suggested design criteria to mitigate such vulnerabilities.	Police Services	General fund	Ongoing					
PS-1.8 PS-1.2	Coordinate with the Brea Fire and Police Department to determine the need and applicability of new technology and equipment in Brea.	Police Services, Fire Services	General fund	Ongoing					
PS-1.9	Biannually evaluate the City's emergency preparedness program to ensure that the City has adequate police, fire and protection services in event of an emergency.	Police Services, Fire Services, Development Services	General fund	Biannually					
PS-1.10	Conduct annual mailings to Brea residents through utility bills that explain the City's Emergency Response Plan, Brea Emergency Operations Center, and appropriate procedures and numbers to call in the event of a disaster.	Police Services, Fire Services, Development Services	General fund	Annually					
PS-1.11	Coordinate with the Brea Police Department to increase public awareness about criminal activity and crime prevention. Maximize the use of after school programs, volunteer programs, Neighborhood Watch programs and other community oriented policing programs.	Police Services	General fund	Ongoing					
PS-1.12	Develop and maintain crime prevention guidelines for building construction. Provide these guidelines to police and code enforcement personnel.	Police Services, Development Services	General fund						
<b>Crime Prevention Through Environmental Design (CPTED)</b>			General fund						
PS-2.1	Implement crime prevention through environment (CPTED) design, based upon the concept of defensible space with the establishment of specific design criteria, and the application of that criteria to proposed projects through the development review process. Establish funding sources and priorities, and set forth a phased improvement program.	Police Services, Development Services	General fund						
PS-2.2	Enforce natural surveillance techniques for existing projects and new residential and commercial projects.	Police Services, Development Services	General fund	Ongoing					

**Public Safety**

Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
PS-2.3	Require the participation of the Police Department in the development review process for public areas relative to building and site plan vulnerabilities to criminal activity and suggested design criteria to mitigate such vulnerabilities.	Police Services, Development Services	General fund	Ongoing					
PS-2.4	Examine and maintain a database of structures within the City that are at risk of incidence of or crime. Develop an implementation strategy based upon CPTED design techniques.	Police Services, Development Services	General fund						
<b>Pedestrian Safety</b>									
PS-3.1	Develop objectives, and detailed standards and guidelines for the treatment of public streetscapes to improve safety and walkability. Recommendations should address street trees, street lighting, street furniture, traffic calming, and other pertinent issues. Establish funding sources and priorities, and set forth a phased improvement program.	Development Services	General fund		CD-12.2		CR-7.1 CR-7.2 CR-7.5		
PS-3.2	Require that adequate safety lighting in pedestrian and parking lots is addressed for all new projects through the development review process.	Development Services	General fund	Ongoing	CD-12.2		CR-7.1 CR-7.2 CR-7.5		
PS-3.3	Develop appropriately designed shielding safety lighting for pedestrian walkways and trails. Establish an implementation plan for development of lighting installation.	Development Services	General fund		CD-12.2		CR-7.1 CR-7.2 CR-7.5		
<b>SECTION XIX: HAZARDS MANAGEMENT</b>									
PS-4.1	Minimize public health and environmental risks from the use, transport, storage and disposal of hazardous materials with the following approaches: <ul style="list-style-type: none"> <li>Cooperate with federal, state and local agencies to effectively regulate the management of hazardous materials and hazardous waste;</li> <li>Cooperate with the County of Orange to implement applicable portions of the County Hazardous Waste Management Plan and the Hazardous Materials Area Plan;</li> <li>Identify roadway transportation routes for conveyance of hazardous materials;</li> <li>Implement the emergency response plan for accidents involving hazardous materials; and</li> <li>Cooperate with the Certified Unified Program Agency (CUPA) for the City of Brea (the Orange County Environmental Health Department) and the Orange County Fire Authority to administer risk management plans for businesses within the City.</li> </ul>	Development Services, Maintenance Services, County of Orange, Federal and State Agencies	General fund, federal, state and local regulatory agencies	Ongoing					
PS-4.2	Monitor businesses that transport, handle, and generate hazardous materials to ensure proper disposal.	Development Services, Maintenance Services	General fund	Ongoing					
PS-4.3	Cooperate with the County Household Hazardous Waste Program to protect residents from dangers resulting from the use, transport and disposal of hazardous materials used in the home. The program includes public education about health and environmental hazards of household hazardous materials and periodic collection campaigns at established sites. Educate residents of the nearest location of a hazardous waste disposal site.	Development Services, County of Orange, Environmental Health Department	General fund	Ongoing					
PS-4.4	Provide education and information to the community about: <ul style="list-style-type: none"> <li>Commonly used hazardous materials;</li> <li>More environmentally friendly alternatives;</li> <li>Safe recycling and disposal methods; and</li> <li>Location of nearest household hazardous waste collection center.</li> </ul>	Development Services, County of Orange	General fund	Ongoing					
PS-5.1 PS-5.2	Cooperate with the California Department of Conservation, Division of Oil, Gas, and Geothermal Resources to protect residents from dangers resulting from the drilling, operation, maintenance, and abandonment of oil, gas, and geothermal wells and support regulations regarding abandoned oil facilities including both idle and orphan wells.	Development Services, California Department of Conservation-Division of Oil, Gas, and Geothermal Resources	General fund	Ongoing					PS-5.2
PS-5.3	During review of development proposals, require comprehensive investigation, studies, disclosures, and remediation of any former oil field property proposed for an alternative use.	Development Services	Development fee	Ongoing					
<b>SECTION XX: WILDLAND FIRES</b>									

**Public Safety**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
PS-6.1	Continue to implement the Very High Fire Hazard Area Requirements and the Fuel Modification Plan Guidelines for High Fire Hazards Areas as a preventative measure to avert wildland fires.	Development Services, Fire Services	General fund	Investigate by end of 2005; implement as needed					
PS-6.2 PS-6.3	Promote fire prevention in Brea in the following ways: <ul style="list-style-type: none"> <li>Work closely with the Orange County Fire Authority to implement fire hazard education and fire prevention programs, including fuel modification programs;</li> <li>Utilize wildland urban fire hazard mitigation techniques involving vegetation/fuel modification including chipping, scattering, multi-cutting, crushing, pruning and other non-fire hazard abatement concepts.</li> <li>Coordinate with the local water districts and Orange County Fire Authority to ensure that water pressure for urban and hillside areas and sites to be developed is adequate for fire fighting purposes; and</li> <li>Adopt and implement the Uniform Fire Code provisions and appropriate amendments to reflect the unique Brea topography, climate, vegetation and urban form.</li> </ul>	Development Services, Fire Services	Development fees	Ongoing					PS-6.3
PS-6.2	Review development proposals to ensure that the City's four to six-minute fire response time is undertaken.	Development Services, Fire Department	Development Fees	Ongoing					
<b>Flood Concerns</b>									
PS-7.1 PS-7.2 PS-7.6	Coordinate with the Orange County Flood Control District (OCFCD) to ensure regularly scheduled maintenance of flood control channels and completion of necessary repairs. Coordinate with the OCFCD, Cal Domestic Water Company and Metropolitan Water District (MWD) regarding any needed improvements to existing aboveground water tanks. In addition, work with the District to identify new flood control improvements for existing flood hazards and potential hazards from new development, and establish installation programs for improvements. Require detention basins for new development, where necessary, to reduce flooding risks.  Continue to participate in the National Flood Insurance Program (NFIP) administered through the Federal Emergency Management Agency (FEMA). The NFIP program provides federal flood insurance subsidies and federally financed loans for property owners in flood-prone areas.	Development Services, Orange County Flood Control District, Cal Domestic Water Company, Metropolitan Water District (MWD)	Development fees, General fund	Ongoing					PS-7.2 PS-7.6
PS-7.3 PS-7.5	Create an active storm drain inspection program in order to prevent flooding, avoid property damage, and protect surface water quality. Allow the program to evaluate and monitor water storage facilities that might pose an inundation hazard to downstream properties.	Development Services	General fund	Establish program by end of 2007					PS-7.5
<b>SECTION XXII: GEOLOGIC AND SEISMIC CONSIDERATIONS</b>									
PS-8.1 PS-8.2	To minimize damage from earthquakes and other geologic activity, implement the most recent state and seismic requirements for structural design of new development and redevelopment.  Where geologic instability can be identified and cannot be mitigated, or presence of faulting is identified, use open space easements and other regulatory techniques to prohibit development and avoid public safety hazards.	Development Services	Development Fees, General fund	Ongoing					PS-8.2

**Public Safety**  
Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
PS-8.3	During review of development and redevelopment proposals, require surveys of soil and geologic conditions by state licensed Engineering Geologists and Civil Engineers where appropriate. Examples of when these surveys are required are (1) for projects within a State-delineated earthquake fault regulatory zones for liquefaction, fault ruptures, and seismically induced landsliding, in accordance with the California Geologic Survey and (2) prior to the development of any area with slopes more than 10 feet high at a gradient equal to or steeper than 2:1.	Development Services	Development fees	Ongoing					
PS-8.4	Develop informational overlays that augment the City's zoning ordinance to identify those areas where more detailed geotechnical studies should be carried out as part of liquefaction-susceptibility investigation, restrict development in liquefaction-prone areas, or to establish specific building design standards aimed at reducing the risk of liquefaction.	Development Services	Development fees, General fund	By the end of 2004					
PS-8.5 PS-8.6	Promote earthquake preparedness in the community with periodic earthquake awareness programs. The program could include mailing brochures to residences and businesses, press releases, school education programs and presentations to homeowner groups and property managers. Coordinate programs with Federal, State, and local emergency service providers and the school districts to maximize public participation.	Development Services, Brea-Olinda Unified School District	General fund	Ongoing					
<b>SECTION XXII: NOISE</b>									
PS-9.1 PS-9.4 PS-9.5, PS-11.4	Ensure that new development is exposed to acceptable noise levels. Require acoustical analyses for all proposed development within the 60 dB CNEL contour as shown on Figure N-1 in the Noise Element. Also require acoustical analyses for all proposed residential projects in the vicinity of existing and proposed commercial and industrial areas. Where the noise analyses indicate that the noise standards in Table N-2 will be exceeded, require noise control measures to be incorporated into the proposed development to reduce noise to acceptable levels. Noise control measures may include berms, walls, and sound attenuating architectural design and construction methods. Only permit new development if the noise standards and the City noise regulations can be met.	Development Services	Development fees	Ongoing					PS-9.4 PS-9.5 PS-11.4
PS-9.2	Continue to enforce the City noise regulations to protect residents from excessive noise levels from stationary sources. The regulations provide protection from non-transportation related noise sources such as music, machinery, pumps, and air conditioners on private property. Periodically evaluate regulations for adequacy and revise, as needed, to address community needs and changes in legislation and technology.	Development Services	General fund	Ongoing					
PS-9.3 PS-9.5	Use noise and land use compatibility standards to guide future planning and development decisions. Table PS-6 in the Noise Element summarizes the standards for acceptable noise levels by land use types. Table PS-6 in the Noise Element provides criteria for assessing the compatibility of future development with the noise environment. Review development proposals to ensure that the noise standards and compatibility criteria are met. Require mitigation measures, where necessary, to reduce noise levels to meet the noise standards and compatibility criteria.	Development Services	Development fees, Mitigation payment	Ongoing					PS-9.5
PS-9.4	All City departments and agencies will comply with local, state and federal noise standards, including state and federal OSHA standards. All new equipment or vehicle purchases will comply with local, state and federal noise standards. Such equipment includes compressors, air conditioners, leaf blowers and other fixed and mobile machinery. Purchase noise-producing equipment with the necessary noise abating equipment installed.  Reduce the noise impacts associated with City services acquired through contracts with other public and private entities, such as street sweeping, solid waste removal and transport, landscaping maintenance, construction projects and emergency services.	Development Services	General fund	Ongoing					
PS-9.5 PS-11.5	Limit delivery or service hours for stores with loading areas, docks or trash bins that front, side, border, or gain access on driveways next to residential and other noise sensitive areas. Only approve exceptions if full compliance with the nighttime limits of the noise regulations are achieved.	Development Services	General fund	Ongoing					PS 11.5

**Public Safety**

Brea General Plan Implementation Guide

Policy	Implementation Program Description	Responsible Agency	Potential Funding Source	Time Frame	Related General Plan Element Policies				
					Community Development	Housing	Community Resources	Community Services	Public Safety
PS-10.1 PS-10.2	Mitigate noise impacts to acceptable levels from transportation activity to enhance the quality of the community. Incorporate noise control measures, such as sound walls and berms, into roadway improvement projects to mitigate impacts to adjacent development. Request Caltrans and the Orange County Transportation Corridor Agencies (OCTA), as well Los Angeles Metropolitan Transit Authority (MTA) to provide noise control for roadway projects within the City and Sphere of Influence. Particularly, advocate reducing noise impacts from the SR-57.	Development Services	General fund, OCTA, Federal grants	Ongoing					PS-10.2
PS-10.1	Coordinate with the Orange County Sheriff's Department and the California Highway Patrol to enforce the California Vehicle Code pertaining to noise standards for cars, trucks, and motorcycles.	Development Services	General fund	Ongoing					
PS-11.1	Provide written information describing methods of retrofitting existing structures and properties to reduce noise impacts, including sound insulation, double-pane glass windows, sound walls, berming and other measures.	Development Services	General fund	Ongoing					
PS-11.2	Enforce the provisions of the State of California Noise Insulation Standards (Title 24) that specify that indoor noise levels for multi-family residential living spaces shall not exceed 45 dB CNEL. The Title 24 noise standard is defined as the combined effect of all noise sources and is implemented when existing or future exterior noise levels exceed 60 dB CNEL. Title 24 requires that the standard be applied to all new hotels, motels, apartment houses and dwellings other than single-family dwellings. Also apply the standard to single-family dwellings and condominium conversion projects as official policy.	Development Services	Development fees, Mitigation payments	Ongoing					
PS-11.3	Require all construction activity and special events to comply with the limits established in the Noise Control Ordinance. Review the Noise Control Ordinance for adequacy and amend as needed to address community needs and development patterns.	Development Services	Development fees, General fund	Ongoing, Revise ordinance by end of 2004					

# URBAN DESIGN IMPLEMENTATION STRATEGIES

## Public Realm Strategies

1. **Urban Design Principles:** Adopt the Urban Design Principles, and require that all public improvements and development projects conform to its principles and guidelines. Subsequent planning documents, including specific plans, public improvement plans and design guidelines, shall also be consistent with the Urban Design Principles.
2. **Public Realm Urban Design Palette:** Adopt the Public Realm Urban Design Palette, and require that all subsequent planning documents and public improvement projects are consistent with its recommendations. Allow deviations where they are shown to fulfill the palette's intent.
3. **Public Improvement Plans:** Prepare and adopt the following public improvement plans. The development of these plans shall include appropriate City staff and officials, community residents and key stakeholders; plans shall exhibit sensitivity to the quality and character of districts and neighborhoods. Coordinate proposed funding sources and phasing strategies with the City's Capital Improvement Program.
  - **Citywide Parks & Trails Master Plan:** Study and identify opportunities for new parks and trails throughout the community, as well as the improvement of existing parks and open spaces. Establish policies, development standards and guidelines, funding sources and priorities, as well as a phased improvement program. Conceptual studies for the design and improvement of particular parks and trails may be included.
  - **Citywide Streetscape Standards & Guidelines:** Develop objectives, and detailed standards and guidelines for the treatment of public streetscapes; as appropriate conceptual design studies for the design and improvement of particular streets may be included. Recommendations should address street trees, street lighting, street furniture, traffic calming, and other pertinent issues. Establish funding sources and priorities, and set forth a phased improvement program.
  - **Citywide Environmental Graphics Program:** Prepare program objectives, detailed design standards and guidelines, and identify funding opportunities for a coordinated system of public signage; as appropriate, specific design studies may be included. Recommendations should address community and district identification signs and gateways, directional signage, pedestrian-oriented directories, street banners, etc. This program should supplement the Streetscape Standards & Guidelines.

- **Citywide Public Art Program:** Develop program objectives and provide detailed standards and guidelines for the installation of public art, with an emphasis on the relationship between local history and public art. Address issues of artist selection, project siting, funding, etc. There should be close coordination with the Streetscape Standards & Guidelines and the Environmental Graphics Program.
  - **Cultural Resources Inventory & Conservation Plan:** Maintain a current inventory of the City's historic and cultural resources. Include significant building and landscape resources, and provide for periodic review and update. Develop objectives and strategies for the conservation of these resources.
4. **Public Improvement Programs:** Implement public improvement plans through a coordinated program of public works. Improvements should cover parks, trails and streetscapes, and funding should encompass a variety of public and private sources.
- **Parks & Trails Expansion:** Pursue funding and undertake expansion of the community's parks and trail systems, based on the priorities and recommendations of the Parks & Trails Master Plan.
  - **Parks Improvements:** Pursue funding and undertake improvements to existing parks in accordance with the priorities and recommendations of the Parks & Trails Master Plan.
  - **Streetscape Improvements:** Pursue funding and undertake streetscape improvements in accordance with the priorities and recommendations of the Citywide Streetscape Standards and Guidelines, as well as the Citywide Environmental Graphics Program.
  - **Neighborhood Street Trees Program:** Develop a program that involves residents and neighborhood groups in the funding, planting, replacement, and maintenance of street trees within residential neighborhoods. Organizational and technical assistance will be provided by City staff, and to ensure compliance with overall urban design goals and objectives.
5. **Maintenance Programs:** Ensure that plans and program provide for the funding of on-going maintenance of public improvements, including parks, trails, streetscapes, and other components of the open space network. Funding and maintenance activities may encompass a variety of public and private sources.

## **Private Realm Strategies**

1. **Urban Design Principles:** Adopt the Urban Design Principles, and require that all private development projects conform to its principles and guidelines. Subsequent planning documents, including specific plans and design guidelines, shall also be consistent with the Urban Design Principles.
2. **Private Realm Design Palette:** Adopt the Private Realm Urban Design Palette, and require that all subsequent planning documents and private development projects are consistent with its recommendations. However, allow deviations where they are shown to fulfill the palette's intent.
3. **Development Standards:** Review existing development standards for consistency with the Urban Design Principles and Plan, and adopt new and/or revised standards as appropriate. Give particular attention to preparation and adoption of the following:
  - Standards that support mixed-use and urban village development types.
  - Standards that prevent inappropriate and excessive development on single family residential lots (i.e. anti-mansionization requirements).
  - Standards for large-lot residential development that supports environmental protection of hillside areas.
4. **Design Guidelines:** Use design guidelines to not only achieve minimum standards of design, but also promote high quality and creative design solutions. In addition, rely on design guidelines to establish criteria for the review of private development proposals.
  - **Citywide Design Guidelines:** Prepare and adopt a Citywide Design Guidelines document applicable to private sector development throughout the community. These Guidelines should build upon the Urban Design Principles and Private Realm Urban Design Palette.
  - **Design District Guidelines:** Prepare and adopt design guidelines applicable to particular districts and neighborhoods. Focus on those areas with a unique character and/or in need of special attention. This work should supplement the Citywide Design Guidelines. As appropriate, design guidelines applicable to a particular area may be incorporated within a Specific Plan.
  - **Downtown Architectural Control Criteria:** The existing document shall continue to apply to all development within the area designated by that plan.
5. **Public Assistance Programs:** Initiate programs whereby the City takes an active role in assisting private property owners to upgrade their properties, and achieve conformance with the community's urban design goals and policies.

- **Rehabilitation Assistance:** Develop a program and identify funding sources that will encourage and assist business and commercial property owners in the improvement, rehabilitation, and reuse of buildings and building facades. Give priority to historic buildings and buildings that otherwise contribute to the identity of the community and its districts.
  - **Design Assistance:** Develop a program that works in conjunction with the rehabilitation assistance program to aid business and property owners in preparing appropriate designs for buildings and signage. Assistance may be provided directly by assigned Staff and/or facilitate the hiring of recommended design consultants.
- 6. Enforcement Programs:** Initiate programs that ensure adherence with adopted urban design policy, development standards and design guidelines, including design review and active code enforcement.
- **Design Review:** Require Staff level design review of all private development projects, including new buildings and building additions, rehabilitations and remodels, as well as signage. This may exclude minor additions to single-family residences that do not face or visually impact the public right-of-way.
  - **Code Enforcement:** Enforce the City's zoning, building, and property maintenance standards, with priority given to those residential neighborhoods where there is an especially strong interest in protecting and maintaining the established character.

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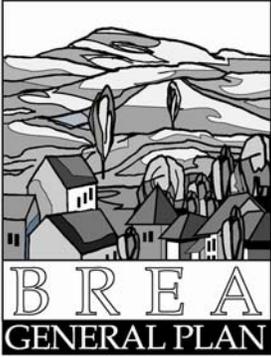
Appendix B

# URBAN DESIGN PRINCIPLES

THE CITY OF  
BREAA

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GENERAL PLAN



# Appendix B

## URBAN DESIGN PRINCIPLES



This appendix supplements the Urban Design section of the Community Development Chapter Element by setting forth eleven principles of urban design that underpin the more specific design strategies contained in the chapter. These principles emphasize the development of a visually appealing and functionally integrated community. They encourage the creation of vital activity centers, connections from residential to commercial and retail areas, and high quality residential areas. The following issues are addressed:

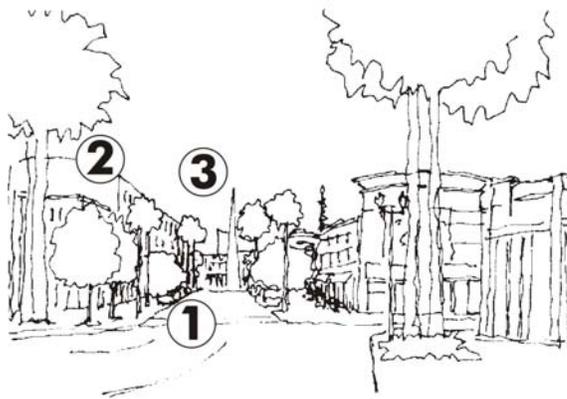
- Community Character
- Street Environment
- Site Planning
- Building Design
- Sign Appearance
- Residential Street Environment
- Residential Site Planning
- Residential Building Design
- Hillside Residential Site Planning
- Hillside Residential Building Design

## COMMUNITY CHARACTER

Community character is largely affected by the built and natural environment. Specific attention to urban design can provide guidance to enhance community character by creating a sense of identity, time and place. The following design principles seek to enhance Brea's unique qualities while creating a stronger sense of community identity and cohesiveness.



**Design Principle 1:** Establish a strong community identity and unique sense of place that includes the presence of identifiable districts and neighborhoods, gateways, landmarks, and natural features.



1. **Spatially defined street**
2. **Compact development**
3. **Landmark feature**

CC 1-1 Mark community, district and neighborhood gateways, and identify and enhance important visual and pedestrian linkages; street trees and special landscape design are recommended for key gateways and linkages.

CC 1-2) Create identifiable places and nodes of activity; commercial and mixed-use areas should emphasize compact, street-oriented development patterns that reinforce social vitality.

CC 1-3) Pursue a "main street" urban design concept along streets where pedestrian activity is most desired; these streets should feature buildings of sufficient height to create a comfortable sense of enclosure, as well as ground floor uses that animate the sidewalk.

- CC 1-4) Encourage infill development of vacant and underutilized property to minimize spatial gaps along the street; in addition, buildings that define the street and contribute to local identity should be retained.

**Design Principle 2:** Build strong visual and physical connections throughout the community, for instance, through the protection of view corridors, and the provision of a comprehensive network of public spaces and paths.



1. **Pedestrian paseo**
2. **Pedestrian scaled light fixtures**
3. **Connection to sidewalk**
4. **Secondary shop entrances**

**Design Guidelines:**

CC 2-1) Maintain and extend the existing street pattern; multi-block developments that close existing streets and alleys are discouraged.

CC 2-2) Provide safe and accessible paths that promote physical continuity and connection; continuous sidewalks, as well as mid-block and open air passages are encouraged.

CC 2-3) Protect and frame important views of natural and built features and landmarks; developments that obscure existing views along public rights-of-way are discouraged.

CC 2-4) Enhance important visual and physical connections; for instance, use streetscape and landscape improvements to communicate street hierarchy and function, and identify key pedestrian connections.



**Design Principle 3):** Express local history and culture, relying on the preservation and reuse of architecturally and historically significant structures, a respect for adjacent architectural character, as well as the provision of public art.



1. **Facade improvements**
2. **Compatible infill development**
3. **Visually unified street**

#### **Design Guidelines:**

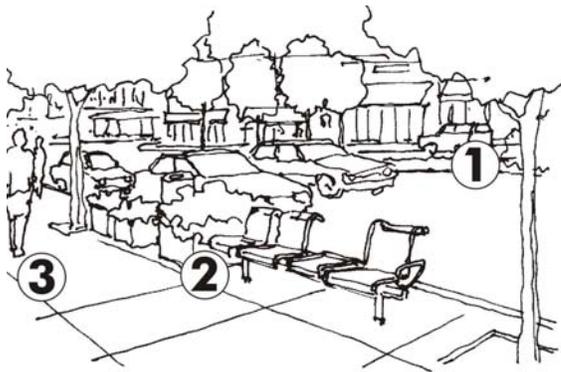
- CC 3-1) Promote sensitive infill development that visually unifies the street; historically and architecturally significant structures should be retained.
- CC 3-2) Design harmonious transitions between new and older buildings; the scale, massing and articulation of nearby historically and architecturally significant structures should be respected.
- CC 3-3) Design harmonious transitions between non-residential and mixed-use developments and residential neighborhoods; consider adjacent residential character in the design of non-residential and mixed-use projects.
- CC 3-4) Integrate publicly accessible works of art into infrastructure and new development projects; civic art that celebrates local history and culture is encouraged.

## STREET ENVIRONMENT

Brea's street environment includes roadways, bicycle and pedestrian paths and trails. Street environments are not just access routes but represent means of experiencing Brea. The community's streets offer means of welcoming visitors, presenting a visual image of the community, and offer a visual and sensory experience for those driving, riding transit, walking, and bicycling throughout the community.



**Design Principle 1:** *Communicate through design that streets are public space intended for a diversity of users, including pedestrians, bicyclists, motorists, and transit users.*



1. **Planted median**
2. **Coordinated streetscape elements**
3. **Comfortable sidewalk width**

### Design Guidelines:

- SE 1-1) Incorporate streetscape amenities such as street trees, street furniture, street lighting, way-finding signage, special paving, etc; a high-level of amenity is critical where pedestrian activity is most desired.
- SE 1-2) Include well-designed transit stops, especially at or near important pedestrian crossings; seats and shelter should be incorporated for safety and comfort, with consideration given to additional amenities such as waste receptacles, maps, etc.
- SE 1-3) Use signs and environmental graphics to assist way-finding; street banners, community and district identification signs, directional signage and pedestrian-oriented directories should contribute to a coordinated way-finding system.

## STREET ENVIRONMENT

**Design Principle 2:** *Contribute to the physical safety and comfort of pedestrians along streets and sidewalks, incorporating such features as traffic calming devices and streetscape amenities, including street trees and street furniture.*



1. **Clear pedestrian zone**
2. **Curb-side parking**
3. **Trees and furniture sited adjacent curb**
4. **Pedestrian-scaled street lights**
5. **Overhead cover (awnings)**

### Design Guidelines:

- SE 2-1) Minimize curb-cuts to reduce conflicts between pedestrians and autos; where there is no alley access, curb-cuts should be consolidated near mid-block.
- SE 2-2) Provide sidewalks with ample width, and where feasible, reduce pedestrian crossing distance at intersections; in general, on-street parking is encouraged, and street furniture should be sited adjacent the curb as a barrier to automobile traffic.
- SE 2-3) Plant and maintain street trees, emphasizing mature canopy trees that allow access to sun and shade; in general, consistent tree species, size, and spacing are encouraged.
- SE 2-4) Provide ample street furniture, and safe and attractive street lighting; pedestrian-scaled light fixtures, benches, trash receptacles, and planters should be provided at frequent intervals in active, pedestrian-oriented activity nodes.

## STREET ENVIRONMENT

**Design Principle 3:** *Make streets active and engaging places that contribute to local identity, with special emphasis on a creating strong relationship between the building and street.*



1. **Shopfronts**
2. **Benches**
3. **Tree canopy**
4. **Sidewalk dining**

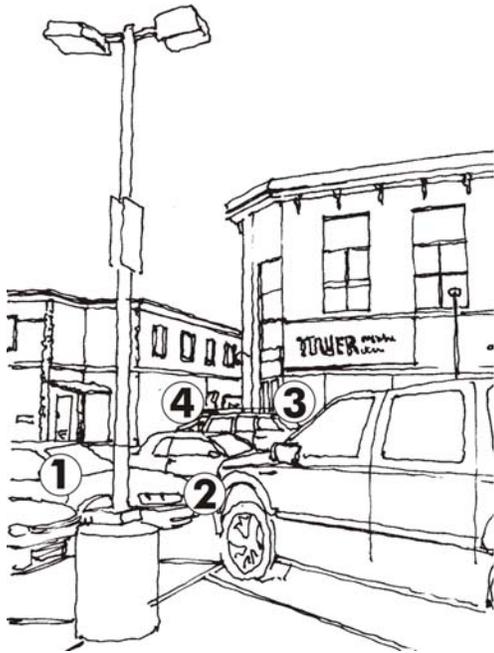
### Design Guidelines:

- SE 3-1) Define streets at or near their edge with buildings and/or canopy street trees; for instance, a continuous building street wall and street trees should be maintained where pedestrian activity is encouraged.
- SE 3-2) Orient buildings and uses toward the street; in particular, shops and active commercial uses with generous street level windows should be located along streets where pedestrian activity is encouraged.
- SE 3-3) Emphasize quality and detail at street level; streetscape enhancements should be coordinated to promote local identity and street level facades should exhibit a high standard of design that satisfies the pedestrian.
- SE 3-4) Make provisions for site and building elements that contribute to street life; canopies, awnings, and architectural lighting are encouraged, and outdoor dining should be allowed along pedestrian-oriented shopping streets.

## SITE PLANNING

The relationship between structures, open space, cars and pedestrian has a large impact upon street cohesiveness, accessibility, and comfort. Too often the design of a site, from all angles is not given adequate consideration resulting in ‘piecemeal’ orientation of structures, inadequate parking and pedestrian access, and inadequate buffering from incompatible land uses. The following principles work to provide visual cohesiveness, pedestrian comfort, access and activity.

**Design Principle 1:** Encourage site planning that functionally and visually integrates on-site facilities and uses, including buildings, services, vehicular access and parking, pedestrian access and circulation, and outdoor spaces.



1. Consolidated parking
2. Parking located to rear
3. Secondary entrance at rear of building
4. Pedestrian paseo leads to sidewalk

### Design Guidelines:

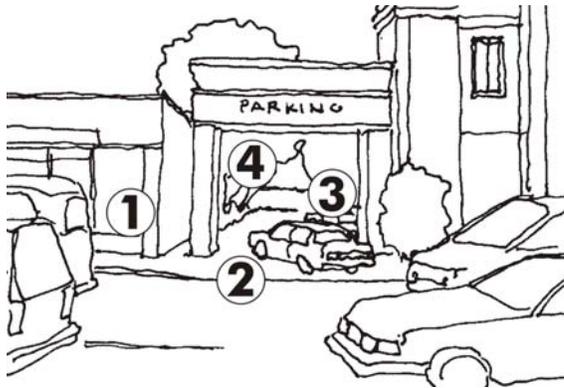
- SP 1-1) Discourage developments that excessively turn inward or result in “left-over” space; site planning should reinforce context, place emphasis on the street, and support active public space.
- SP 1-2) Establish a comfortable transition between interior and exterior space; clear connections and gracious paths should direct pedestrians between sidewalks, parking, outdoor spaces and building entrances.

# *Imagine Brea*

- SP 1-3) Buffer adjacent sensitive land uses from undesirable impacts that originate from a site; dense landscape and/or cohesive architectural treatment are desirable methods of screening service and parking facilities.

## SITE PLANNING

**Design Principle 2:** Control vehicular access, on-site parking, and service facilities to reduce their visual impact along streets, promote sidewalk interest and activity, and protect adjacent sensitive land uses.



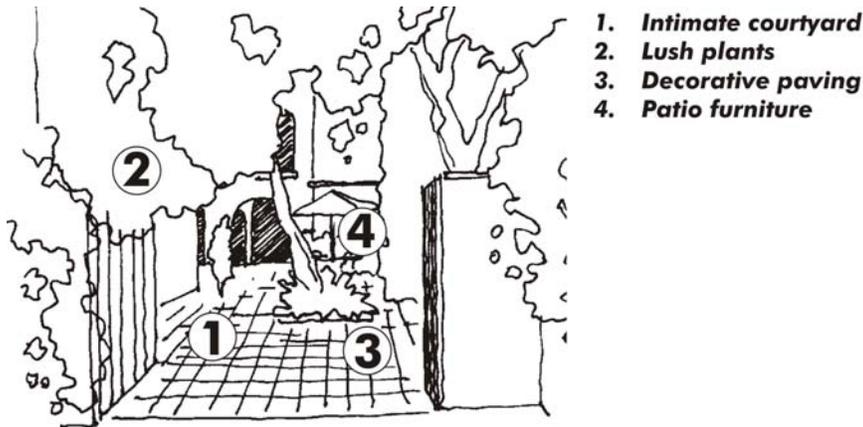
1. **Buildings maintain street wall**
2. **Limited access from street to parking**
3. **Parking located to rear of lot**
4. **Dense plants screen and shade parking**

### Design Guidelines:

- SP 2-1) Site parking behind buildings or interior to a block, especially along pedestrian-oriented streets; most importantly, surface parking should be restricted from street corners.
- SP 2-2) Minimize the expansiveness of surface parking lots; shade trees and other plants should be provided within parking areas to enhance their appearance and reduce heat build-up.
- SP 2-2) Locate service, loading and storage areas away from public streets and outdoor gathering places; service facilities should be unobtrusive and should not be sited as an afterthought.
- SP 2-2) Screen roof mounted equipment so that it is not visible from streets and public areas; screening should be integral to the building form.

**SITE PLANNING**

**Design Principle 3:** Incorporate accessible outdoor spaces that are amenable to outdoor activity and comfortable social interaction.



**Design Guidelines:**

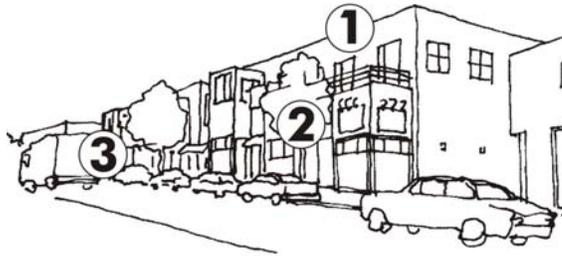
- SP 3-1) Provide well-defined and comfortable outdoor rooms, such as plazas and courtyards, in conjunction with new development projects; outdoor spaces should establish a comfortable transition between the exterior and interior of a building.
- SP 3-2) Define and contain outdoor rooms through a combination of building and landscape; oversized outdoor spaces that lack containment are discouraged.
- SP 3-3) Design outdoor rooms for comfortable human activity and social interaction, including a pleasant microclimate; seating and deciduous trees should offer opportunities for shade from summer sun and access to winter sunlight.
- SP 3-4) Use plants, furniture, and lighting to shape, embellish, enliven and give purpose to outdoor space; a rich, yet coordinated palette of landscape materials should provide scale, texture and color.

## BUILDING DESIGN

The location, scale, and orientation of a structure have a tremendous impact on the streetscape and neighborhood character. Front yards or entryways are similar in depth and scale results in a uniform alignment of building fronts which creates a sense of visual uniformity. By varying setback patterns, and design features, streets are more interesting to walk, are desirable to shop and visit and contribute to a distinct neighborhood character.



**Design Principle 1:** *Promote architectural design that fits with and enhances its surroundings, emphasizing visually attractive buildings that bring interest to the street, and add richness and variety to the community.*



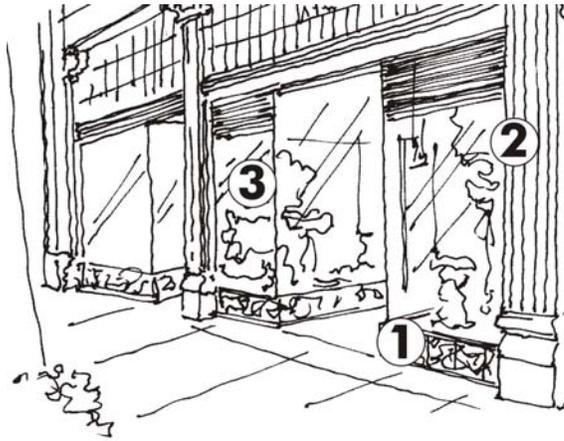
1. **Strong massing**
2. **Articulated sub-volumes**
3. **Visual emphasis on street**

### Design Guidelines:

- BD 1-1) Employ simple and strong, yet varied massing, and feature deep wall openings that create shadow lines and provide visual relief; discourage large, monolithic buildings and blank wall surfaces.
- BD 1-2) Use articulated sub-volumes as a transition in size to adjacent structures; building volumes should maintain a compatible scale with their surroundings.
- BD 1-3) Unify and articulate street facades; building facades should be based upon a pleasing set of proportions and a clear pattern of building openings.
- BD 1-4) Express roofs in a visually interesting manner that complements the composition of the building and the surrounding area; a strong and attractively detailed cornice should be provided in conjunction with a flat roof, although sculpted roof forms are encouraged.

**BUILDING DESIGN**

**Design Principle 2:** Encourage architectural design that contributes to a more pleasant and humane living environment, emphasizing high-quality construction and human-scale design.



1. **High quality, durable materials**
2. **Light modulating details**
3. **Window displays**

**Design Guidelines:**

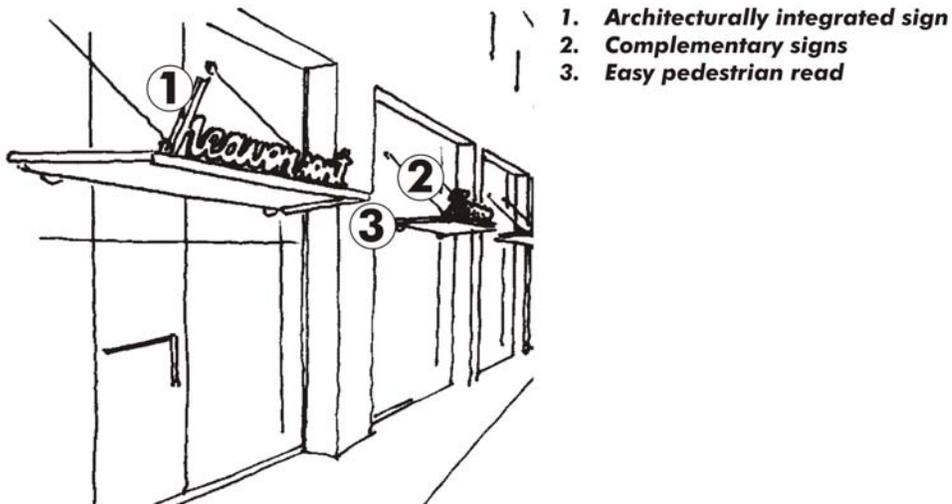
- BD 2-1) Accentuate the ground floor of a building and distinguish it from the upper levels; street level facades should receive the most attention to detail, and pedestrian-oriented streets should include separate storefronts, frequent shop entrances, and window displays.
- BD 2-2) Demarcate and accentuate building entrances; well-marked, articulated building entrances oriented to the street and public gathering places are encouraged.
- BD 2-3) Provide a high-degree of street level transparency, especially along streets where pedestrian activity is encouraged; the use of reflective, opaque, and darkly tinted glass should be restricted along the ground floor.
- BD 2-4) Use compatible materials that exhibit permanence and quality, and unify a building's appearance on all sides; in general, the number of materials and colors on the exterior of a building should be limited to promote visual simplicity and harmony.
- BD 2-5) Design architectural features and details as an integral part of the building; architectural features, including canopies and awnings, should not appear "tacked on."

## SIGN APPEARANCE

Signs are an important architectural element for any business. However, since signs influence the overall character and appearance of the streetscape, they should complement the building's style and street character. Sign fonts should be clear and legible for pedestrians and motorists, and be consistent in style and color.



**Design Principle 1:** *Require legible signs that make a positive contribution to the street.*

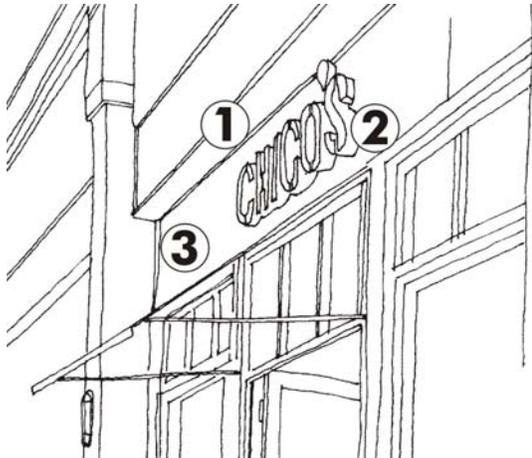


### Design Guidelines:

- SA 1-1) Use simple, direct signage that is unique to a particular business; high-quality, innovative and expressive designs are encouraged.
- SA 1-2) Offer a clear, simple message; the number of lettering styles should be limited, and the use of identifiable symbols and logos is encouraged.
- SA 1-2) Construct signs with durable materials; internally illuminated cabinet signs, as well as paper and cloth signs are discouraged.
- SA 1-3) Encourage pedestrian-scaled signs that are easily and comfortably read by passers-by on foot; for instance, projecting blade signs are especially appropriate along pedestrian-oriented streets and outdoor passages.

## SIGN APPEARANCE

**Design Principle 2:** Encourage signs that fit comfortably with their sites, buildings and surroundings, and that are designed to minimize impacts on adjacent sensitive uses.



1. **Tenant wall sign**
2. **Individual letters**
3. **Margins**

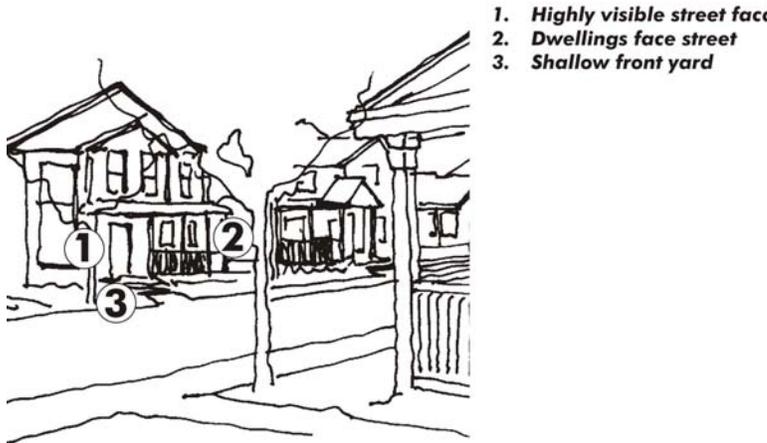
### Design Guidelines:

- SA 2-1) Require signs that are in balance and scaled to the building, site and surrounding area; avoid repetitive signage on the same building frontage.
- SA 2-2) Conceive of signs as an integral part of the building façade; tenant identification signs should fit comfortably into storefront architecture and should not obscure architectural features.
- SA 2-3) Orient signs away from neighboring residences, and limit the scale and size of signs adjacent residential neighborhoods; blinking and flashing lights should be discouraged near dwellings.

## RESIDENTIAL NEIGHBORHOOD IDENTITY

Diverse neighborhoods and active citizens are a community's greatest asset. Neighborhoods are commonly identified by distinct geographic features; a business district, and a school, park, or other public amenity. Neighborhoods are also defined by the style of the buildings and the characteristics of the people who inhabit them. Enhancing these features help to define a neighborhood, and create a sense of identity.

**Design Principle 1:** *Integrate residential developments with their built and natural surroundings; in particular, encourage a strong relationship between dwellings and the street.*



1. **Highly visible street facade**
2. **Dwellings face street**
3. **Shallow front yard**

### Design Guidelines:

- RI 1-1) Support conservation of established residential neighborhoods; new construction should be sensitive to the character of existing homes, and the rehabilitation and remodeling of existing dwellings is encouraged.
- RI 1-2) Integrate new residential development with its surroundings, both functionally and aesthetically; discourage isolated residential developments and restrict the backing of residential lots onto a street.
- RI 1-3) Provide transitions between residential scales; setbacks and building heights should respond to adjacent structures, for instance, through articulated sub-volumes and horizontal regulating lines that establish a visual relationship.

## *Imagine Brea*

- RI 1-4) Site residential buildings to create a sense of continuity along the street; proportions, patterns, and materials should also relate to the surrounding area.

## RESIDENTIAL NEIGHBORHOOD IDENTITY

**Design Principle 2:** Establish direct links between residential developments and important destinations, including transit stops, local shopping, parks and public facilities.



1. **Continuous sidewalk**
2. **Sidewalk coincident w/ street**
3. **Trees define and shade sidewalk**

### Design Guidelines:

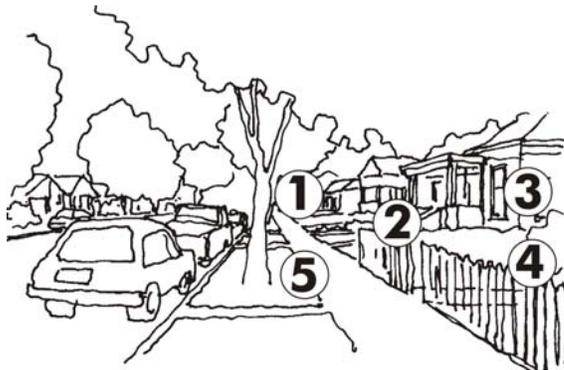
- RI 2-1) Plan new developments as an extension of the street grid, and discourage gated projects; where cul-de-sacs are provided, pedestrian connections should be introduced.
- RI 2-2) Provide interconnected paths and continuous sidewalks throughout residential neighborhoods; at a minimum, sidewalks should be coincident with the street system and are encouraged along both sides of the street.
- RI 2-3) Distinguish paths, walks, and crossings with enriched paving material, where appropriate; for instance, special paving may delineate pedestrian crossings to parks, schools and public facilities located in or near residential neighborhoods.

## RESIDENTIAL STREET ENVIRONMENT

A well-designed residential street environment should give residents and visitors a sense of comfort, and enjoyment while being accessible to all. Particularly important in residential areas is the creation of a sense of neighborhood identity, intimacy, and safety. A key aspect of this is to ensure that streets are walkable, well lit, and protected from traffic. Streets are also made more pedestrian comfortable through the provision of porches, street lawns, wide sidewalks, canopy trees and pocket parks.



**Design Principle 1:** Support neighborliness and offer a sense of security along residential streets, encouraging dwellings, front entrances and windows that orient to the street.



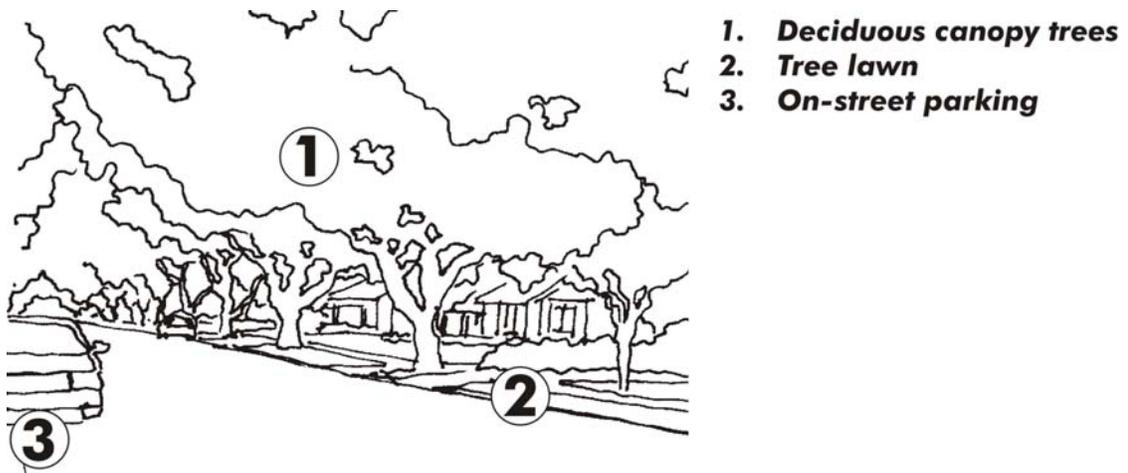
1. **Differentiated dwellings oriented to street**
2. **Front porches**
3. **Doors and windows face street**
4. **Front yards with lawn, plants and walks**
5. **Sidewalks and street trees**

### Design Guidelines:

- RSE 1-1) Orient dwellings to the street to help create attractive residential streets; monotonous street scenes dominated by garage doors should be avoided.
- RSE 1-2) Encourage entry porches and stoops; these will provide a transition between the public space of the sidewalk and the private dwelling.
- RSE 1-3) Discourage excessively deep front yard setbacks; front yard depth should balance privacy needs with a consistent street-oriented development pattern.

## RESIDENTIAL STREET ENVIRONMENT

**Design Principle 2:** Make walking safe and appealing along residential streets, for instance, through a reduction in the width of travel lanes and the provision of mature street trees.

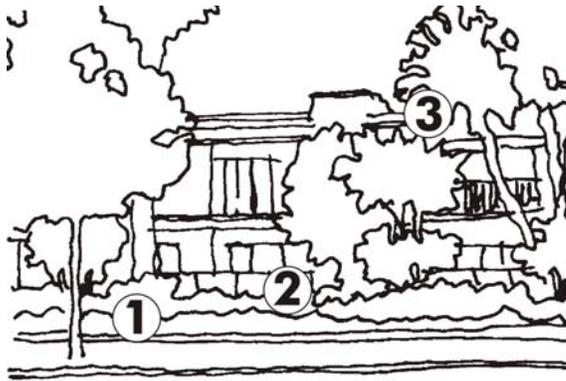


### Design Guidelines:

- RSE 2-1) Minimize the actual and perceived width of residential streets; for instance, on-street parking should be accommodated, and street trees and tree lawns should be incorporated.
- RSE 2-2) Create ambiance with mature street trees; street trees should unify the street and provide a spreading, shady canopy that encloses the street.
- RSE 2-3) Encourage tree lawns between the street and sidewalk; new developments that do not provide this amenity are generally discouraged, and at a minimum, trees should be planted adjacent or near the sidewalk.

## RESIDENTIAL SITE PLANNING

**Design Principle 1:** Reduce the visual impact of residential parking facilities, and encourage developments that offer greenery and shade the site with mature canopy trees.



1. **Planted front yard**
2. **Mature trees**
3. **Layered transition from street to dwelling**

### Design Guidelines:

- RSP 1-1) Locate parking facilities to reduce their visibility from the street; for instance, place parking below or interior to the block of a multi-family residential development, or setback the garage from the front elevation of a single-family residential dwelling.
- RSP 1-2) Minimize the amount of lot frontage devoted to vehicular access and drives, and locate points of vehicular access so as to not dominate street elevations; access should be taken off of an alley, where feasible.
- RSP 1-3) Reinforce domestic character with abundant landscape; as far as feasible, existing mature trees should be retained or replaced.
- RSP 1-4) Plant required yard areas with a combination of trees, shrubs and groundcover; front yard landscape should contribute to a pleasing transition from the sidewalk to the dwelling.

## RESIDENTIAL SITE PLANNING

**Design Principle 2:** Provide functional outdoor space in conjunction with residential development, including private outdoor spaces, as well as communal gathering areas, recreational facilities, and outdoor amenities.



1. Residential courtyard
2. Seating
3. Shade trees



### Design Guidelines:

- RSP 2-1) Incorporate communal and private outdoor space in conjunction with residential developments; communal space may include communal gathering areas and recreational facilities, and should be clearly distinguished from private outdoor spaces
- RSP 2-2) Site communal open space so that it is centrally located and functional in size; in general, a large contiguous space is preferable to a series of small isolated spaces.
- RSP 2-3) Include mature canopy trees, attractive plantings and outdoor furnishings with communal outdoor space; these spaces should afford residents comfortable interaction.

## *Imagine Brea*

- RSP 2-4) Provide each dwelling with suitable private outdoor space; this space should be located so that it is readily accessed by and adjacent to the dwelling it serves.

## RESIDENTIAL BUILDING DESIGN

**Design Principle 1:** Encourage the design of visually appealing residential dwellings that feature varied facades with pleasing compositions.



1. **Modulation of wall plane**
2. **Change of roof form**
3. **Architectural detailing**
4. **Differentiated dwelling facades**

### Design Guidelines:

- RBD 1-1) Vary residential building plans and facades to add interest, reduce monotony, and create identity from building to building; for instance, facades should be differentiated from one dwelling to another within low-density residential developments.
- RBD 1-2) Modulate the plane of exterior walls in height, depth, and direction to create visual interest; roof form and height should complement the building's mass and articulation.
- RBD 1-3) Create a simple and pleasing composition that uses a common vocabulary of forms, architectural elements, and materials; collectively, dwellings should contribute to an attractive street scene.

## RESIDENTIAL BUILDING DESIGN

**Design Principle 2:** *Communicate the residential function of a building with an emphasis on domestic details and intimate scale.*

### Design Guidelines:



1. **Front porches and steps**
2. **Articulated entries**
3. **Street facing windows w/ decorative trim**
4. **Detailing on all elevations**

RBD 2-1) Include single story building forms that break down mass and promote visual interest; this is especially applicable to detached residential developments where a single story floor plan should be included within the unit mix.

RBD 2-2) Use windows and doors to articulate facades, establish scale and give expression to residential dwellings; three-dimensional features such as front porches, bays, and balconies, and trim details are especially encouraged.

RBD 2-3) Emphasize high-quality design and construction; design elements and detailing should be continued around the entire structure.

## HILLSIDE RESIDENTIAL SITE PLANNING

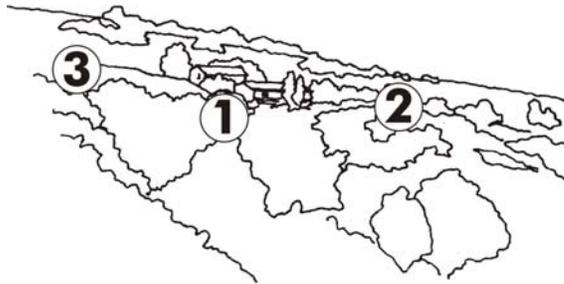
The history of Brea lies in its hillsides: in the oil industry, production camps and early settlements that predate the city's founding. Today, Brea's rugged hills provide a scenic backdrop to the community and insulate the community geographically and emotionally from Los Angeles and San Bernardino counties.



While development in some areas is inevitable, it is important to preserve the unique topography of the hillsides and limit the impact of new development. This can be achieved through a variety of design techniques such as cluster housing, preservation of ridgelines, and maintaining the natural contours of the land whenever possible.

***Design Principle 1:*** Site hillside residential dwellings to minimize disturbance to hillside areas and protect significant topographic features, emphasizing large lots with extensive acreage left unbuilt.

- 1. Dwelling located below ridge line**
- 2. Native vegetation preserved**
- 3. Significant undeveloped land**



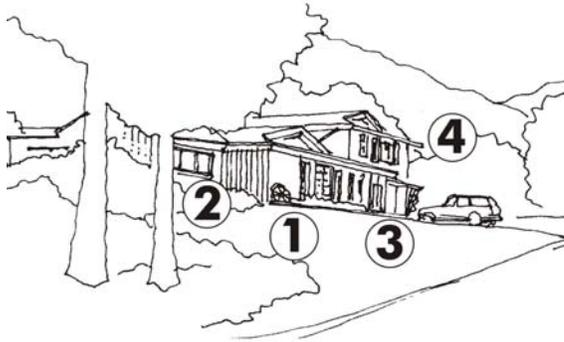
**Design Guidelines:**

- HRP 1-1) Minimize disturbance to the natural surroundings; in particular, grading should be limited to roadways and building envelopes and naturalistic grading techniques should be employed.
- HRP 1-2) Protect environmentally sensitive areas; a significant percentage of each lot should remain in its undeveloped state, and preferably, the most sensitive hillside areas will be maintained as public open space.
- HRP 1-3) Design roads and driveways that conform to the natural contour of the land and limit visual disruption of the landscape; roads that run perpendicular to the site contours are generally discouraged.
- HRP 1-4) Locate dwellings and their accessory structures away from visually prominent locations; as far as feasible, hillside construction should be unobtrusive and retain a generally open and undeveloped character.



## HILLSIDE RESIDENTIAL BUILDING DESIGN

**Design Principle 1:** *Design unobtrusive hillside residential dwellings that are visually compatible with the undeveloped terrain.*



1. **Low profile dwelling**
2. **Single story element**
3. **Articulated facade**
4. **Native vegetation as backdrop**

### Design Guidelines:

- HRD 1-1) Use building forms and materials that contribute to the integration of building and site; dwellings should generally exhibit a rural character.
- HRD 1-2) Reduce the bulk of hillside residential dwellings and emphasize building forms that conform to the topography; the profile of a hillside dwelling should remain close to the land.
- HRD 1-3) Design visually attractive dwellings that vary from one another; facades should be articulated and roof forms should be consistent with the overall composition and hillside setting.

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